

City of Dunedin
Presents the Comprehensive Plan

**STRIVE FOR
2035**

ADOPTED ON
OCTOBER 7, 2021

AMENDED THROUGH
JANUARY 26, 2023

DUNEDIN
Home of Honeymoon Island

Adoption and Amendment Chronology

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 Public Services Division
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 Water Division
 Wastewater Division
Parks & Recreation Department
Finance Department
Economic Development & Housing Department
Community Relations Department



INTRODUCTION

PURPOSE OF THE COMPREHENSIVE PLAN

A comprehensive plan is a blueprint to guide public and private investment in the physical development of the City. It is a plan that provides long-range goals, objectives, and policies that aim to implement the community's vision for future growth and development, and when feasible, align them with other local government, regional, State, and Federal agencies. Chapter 163 of the Florida Statutes (F.S.) requires all local governments to adopt and maintain a comprehensive plan.

The City of Dunedin aims to utilize this plan in order to implement the community's vision for growth and development, coordinate with other government agencies, and support community engagement and participation. While this is not an implementation plan, it is an integral part of aligning community-oriented goals with strategic planning items that will arise throughout the lifespan of this long-range document. This plan will act as a foundational document that guides other City plans, capital projects, and programs and will be referenced throughout the plan to respond how certain goals, objectives, and policies will be achieved.

This plan will also set a policy framework that establishes the legal authority for implementation of the City's land development regulations through its Land Development Code. The Land Development Code sets forth community-wide performance standards that are intended to protect the health, safety, and welfare of the citizens of Dunedin by ensuring that neighborhoods are protected from potential negative impacts of development and use of land.

The community-oriented goals are organized into several elements of this document. There are nine elements within this plan:

- Future Land Use;
- Transportation;
- Housing;
- Conservation;
- Coastal Management;
- Recreation and Open Space;
- Support Services;
- Capital Improvements; and
- Property Rights.

Each element sets not only community-oriented goals, but also establishes benchmarks for attaining the goals and provides policies to be implemented to meet the stated benchmarks. Each element will also establish meaningful and predictable guidelines for the use and redevelopment of land as well as set level of service standards.



A NEW PLAN TO STRIVE FOR 2035

The City of Dunedin's first comprehensive plan was adopted in 1989. The first substantial update to the plan was adopted in 1997. The current edition of the comprehensive plan was last updated and adopted in 2008.

The 2008 plan incorporated a major update involving a multi-year process of analysis, review, and public input, known as the Evaluation and Appraisal Review (EAR) at that time. It was adopted in accordance with Florida's Growth Management Act and regulations, and Rule 9J-5, Florida Administrative Code, the current legislation at that time. Rule 9J-5 required and set the minimum criteria for the local governments' comprehensive plans which were overseen by the Florida Department of Community Affairs (DCA).

In 2011, a significant legislative change by the State was adopted, affecting the way comprehensive plans are evaluated and reduced state oversight of planning and land development activity. The State reorganized the DCA and established the new Department of Economic Opportunity (DEO) to oversee local government comprehensive planning. As part of the Community Planning Act, Rule 9J-5 was repealed and replaced by amended Chapter 163 F.S. Intergovernmental Programs; and specifically §163.3177 with required and optional elements of a comprehensive plan; studies and surveys. While this section continues to outline specific element topics that must be included in a comprehensive plan, local government is afforded more control over the creation and evaluation of their community's comprehensive plan.

The structure and format of comprehensive plans have significantly evolved over the past decade. Communities strive to create a plan that is useful and engaging for the public. As more citizens are able to access information and conduct business transactions through the internet, maintaining an online presence has become the norm for community engagement and information sharing. As a result, the City is proactively working towards creating an interactive web-based comprehensive plan platform to ensure the document and its goals, objectives, and policies remain relevant.

The Comprehensive Plan, *Strive for 2035*, is a result of an extensive review and discussion of existing City, County, and State documents. Additionally, this plan is in response to the work done since the 2008 update; which included improvements to the development process, updates to the Land Development Code, and completion of several corridor studies, citizen engagement through a visioning exercise in 2017, and the City Commission's Strategic Planning Engagement session in 2018.



City of Dunedin 2017 Community Visioning Report Update

After implementing many of the recommendations from the 2005 Community Visioning Report, the City of Dunedin conducted a five-day visioning process in 2017 to update the report. The report focuses on the following key areas that the City has targeted for potential future redevelopment:

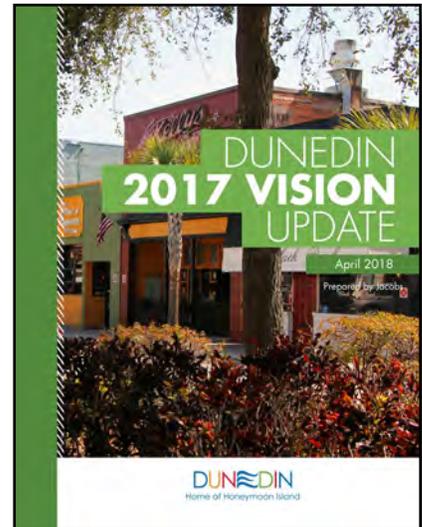
- Downtown Dunedin,
- Douglas Avenue Corridor,
- Patricia Avenue Corridor,
- State Road 580 Corridor, and
- Causeway Boulevard Corridor.

The update was intended to provide the current growth and development trends in the City and outlines how the City will address future growth and development. The City gathered input through a series of workshops and an online survey, allowing flexibility and convenience for citizens to provide feedback for the update. A substantial amount of feedback was provided related to the growth of the City, particularly the redevelopment potential of the five key areas mentioned above. Participants voiced their concerns to the City regarding the existing traffic conditions, safety for non-motorized transportation, and potential development that may negatively impact the aesthetics and characteristics of the key areas. In response to community feedback, the City is taking steps to address the concerns through code amendments and planning initiatives through the Municipal Business Plan update as well as the Comprehensive Plan update.

City of Dunedin Municipal Business Plan

In 2018, the City Commission identified five EPIC! Goals for the City at their Strategic Planning Engagement session:

1. Create a vibrant, cultural experience that touches the lives of our community and visitors.
2. Create a visual sense of place throughout Dunedin.
3. Promote Dunedin as the premier coastal community, protecting and improving our natural resources for the enjoyment of all.
4. Be the Statewide model for environmental sustainability stewardship.
5. Embrace community and employee relationship strategies that strengthen inclusiveness, respect, transparency and collaborative engagement.



[Image I-1] 2017 Community Visioning Report Update



[Image I-2] 2017 Community Visioning Workshop



[Image I-3] Municipal Business Plan

In an effort to align with the City Commission's strategic planning process, the City developed a Municipal Business Plan that provides short-term initiatives and projects that either support or enhance the City's core services as outlined in the Comprehensive Plan. The Municipal Business Plan communicates to the City Commission how staff will implement the initiatives or execute projects over the course of the fiscal year in order to bring the City closer to attaining its EPIC! Goals; inform citizens how the City will enhance its services; and enable city staff to understand how their work contributes to the success of the City.

The Municipal Business Plan is an important part of the City's annual planning cycle, designed to ensure that city funds, as well as City Commission and city staff efforts are allocated effectively and efficiently in response to the top public priorities. The initiatives and projects contained within the Municipal Business Plan are reported to the City Commission on a quarterly basis in order to ensure transparency and measure progress. By aligning with the goals, objectives, and policies of the Comprehensive Plan, this document plays an important role in the City's performance management program.



What's New?

The City of Dunedin, in compliance with new Statewide changes in community planning and growth management, has updated the Comprehensive Plan to reflect new goals for sustainability, resilience, and Smart Growth principles. Some new policies have been developed as a result of emerging practices in climate science and global sustainability research. The City of Dunedin has been recognized in many publications for its walkability and small-town feel and plans to continue to support these features through various planning initiatives, such as Complete Streets. Although the City has maintained steady population growth since 2008, Dunedin has embraced smart growth principles in response to recent increases in development and redevelopment activity. Updating the Comprehensive Plan to reflect new development goals in the context of historical development patterns in the City of Dunedin is an important step in ensuring compatibility and sustainable growth practices. In addition to including these updated items in their text format, many of these concepts will be represented as diagrams and visuals in order to clearly communicate planning practices. The accessible nature of the Comprehensive Plan is rooted in an extensive planning process that embraces public input and community-wide visioning.

Intergovernmental Coordination Element

Strategies within the Comprehensive Plan elements involve State, regional, County, and local provided programs and services. These coordination efforts with the City are discussed in the introduction section as well as stated in certain goals, objectives, and policies of each element.

Coastal Management Element

One important change to the City of Dunedin Comprehensive Plan is the addition of strategies to address the Peril of Flood Act (Florida SB 1094), which was passed in 2015. The Peril of Flood Act specifies six redevelopment components that must be addressed in the Coastal Management Element of local government comprehensive plans, as applicable. The State of Florida made major changes to the Florida Building Code after Hurricane Andrew and has continued to develop initiatives and policies whereby coastal communities can improve resilience in the face of both storm-related wind, as well as flooding. With the addition of sea-level rise as one of the causes of flood risk, coastal communities are required to update their coastal elements to incorporate redevelopment principles, strategies, and engineering solutions to reduce flood risk and are strongly encouraged to strictly adhere to flood-resistant construction requirements in the Florida Building Code. On a local level, the addition of this new language in the City of Dunedin Comprehensive Plan was fortified by a study conducted by the University of South Florida.



[Image I-4]
Downtown Dunedin



[Image I-5] Peril of Flood
Source: Florida Adaptation
Planning Guidebook

[Image I-6] Walk to School Day



Public Schools Element

In April 2007, the City of Dunedin entered into an interlocal agreement with 12 other municipalities, Pinellas County and the Pinellas County School Board to establish jointly the specific ways to coordinate the plans and processes of the district school board and local governments. One purpose of this agreement is to establish a uniform Public School Facilities Element and land development regulations in each local government to ensure that sufficient capacity is available for new and existing students in school facilities. Based on this agreement, Dunedin will be utilizing the Public School Facilities Element created from this agreement.

Concurrency

Following the 2011 changes to Chapter 163 F.S., specifically §163.3180, the City of Dunedin revised its concurrency requirements to reflect those public facilities and services subject to the concurrency requirements of state statutes. The following elements have adopted level of service standards:

- Sanitary Sewer,
- Solid Waste,
- Stormwater (Drainage), and
- Potable Water.

The City also adopted Pinellas County's Mobility Management System in lieu of transportation concurrency.

[Image I-7] City of Dunedin
Public Works





Plan General Provisions

Legal Status

The legal status of the Comprehensive Plan requires that all policies will be considered in making legal determinations of consistency with the Comprehensive Plan, pursuant to State Laws and Statutes.

Vested Rights

Consistent with Chapter 163.3167, F.S., nothing in this Comprehensive Plan shall limit or modify the rights of any person to complete any development that has been authorized by the City. See the City of Dunedin Land Development Code for the City's vested rights determination process.

Graphics and Images

The use of graphics, images, photographs or similar throughout the *Strive for 2035* are for illustrative purposes only and not intended to convey or require a specific element(s).

Use of Terms: "Shall", "Should", "Will" and "May"

The terms "shall", "should", "will" and "may" are included within this Comprehensive Plan and are understood to be used in the following manner:

Shall: provisions or items that are mandatory;

Will: declares purpose to accomplish an item in the future;

Should/May: items that are more flexible in nature and are non-mandatory, but encouraged.

Plan Interpretation

- A. The Community Development Director or his or her designee shall interpret the Comprehensive Plan and its application to public and private land and to activities permitted thereon. Plan interpretations are to be based on the applicable text, maps, figures, and tables within the adopted Comprehensive Plan along with the support documents to support the Comprehensive Plan.
- B. The support documents are not adopted as part of the Comprehensive Plan, but may be used in any review, interpretation, and update of the adopted Plan.
- C. Interpretation of Conflicts- Conflicts shall be judged under the following guidelines:
 - C-1. In the event of any difference of meaning or implication between the text of the Comprehensive Plan and any caption, illustration, graphic, summary table or illustrative table, the text shall control.
 - C-2. In the event of an apparent conflict between the Future Land Use Map and any other map or figure in the adopted Comprehensive Plan, the Future Land Use Map shall control.
- D. Interpretation of Undefined Terms- Terms not otherwise defined in the Appendix- Definitions/ Acronyms of the Comprehensive Plan, shall be interpreted first by reference to the relevant provisions of the Community Planning Act Part II, Chapter 163 F.S., or other relevant and appropriate State Statutes if specifically defined therein; secondly, by reference to terminology adopted by City Ordinance; thirdly, by reference to terminology generally accepted by Federal or State agencies; fourthly, by reference to terminology generally accepted by the planning profession; and otherwise according to the latest edition of Webster's New Collegiate Dictionary.

- E. Administrative Interpretations- It is the responsibility of the Community Development Director to make Comprehensive Plan administrative interpretations. Applications for an administrative interpretation together with the appropriate fee as may be required by resolution of the City Commission shall be filed with the Director. Administrative interpretations may be required as a result of one of the following actions:
- E-1. *Failure to obtain a Comprehensive Plan consistency finding based on submission of a development order application:* In the event a development order application is found inconsistent with the Plan, the application may be forwarded to the Director for an administrative interpretation. The building permit, subdivision, or design review application, or conditional use application shall serve as the application for an administrative interpretation.
 - E-2. *Request for interpretation of land use designation boundaries:* Written requests are required for administrative interpretations related to land use designation boundaries. The request shall include a legal description of the parcel(s) and depending on the circumstance a current survey may be required.
 - E-3. *Any other action that would require a Comprehensive Plan interpretation:* Any affected party may make application for an administrative interpretation of the Comprehensive Plan. Specifically, the application shall include a written statement which clearly identifies the section or sections of the Plan for which an interpretation is required along with references to text, maps, figures, and support document(s) which the applicant feels supports the application.
- F. Administrative Interpretation of Plan Consistency and its Relationship to the Concurrency Provisions of the Support Services Element- An administrative interpretation resulting in a determination of consistency does not constitute a determination of concurrency.
- G. Appeal of an Administrative Interpretation- Any administrative interpretation made by the Community Development Director under the provisions of this Section may be appealed to the Local Planning Agency.

Plan Amendments

All amendments to the Comprehensive Plan shall be reviewed and adopted in accordance with Chapter 163.3184, Florida Statutes.



BACKGROUND

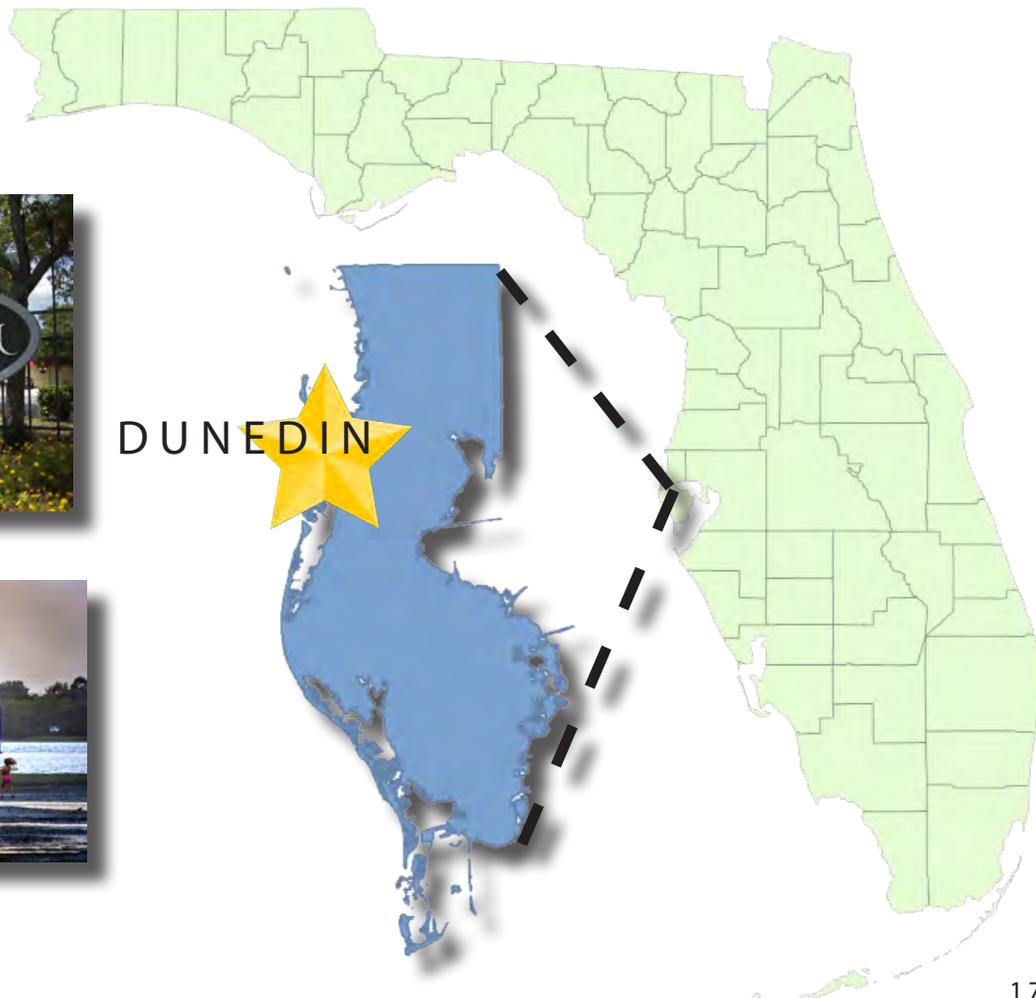
Located in Pinellas County, the most densely populated county in Florida, Dunedin lies on the west coast of Florida, 25 miles west of Tampa. It is bordered by the unincorporated community of Palm Harbor to the north, the City of Clearwater to the east and south, and the Gulf of Mexico to the west. Dunedin enjoys four miles of picturesque waterfront overlooking the Gulf of Mexico, St. Joseph's Sound and the barrier islands including Clearwater Beach, Caladesi Island and Honeymoon Island.

Dunedin has a relaxed lifestyle that has lured people for decades from around the world. It offers a uniquely vibrant arts and culture scene, a lively downtown, and waterfront views. It is one of the few open waterfront communities from Sarasota to Cedar Key where buildings do not obscure the view.

Dunedin provides access to Honeymoon Island State Park and St. Joseph's Sound, offering one-of-a-kind recreational opportunities. Dunedin is also the Spring Training home to the Toronto Blue Jays and hosts 25 festivals annually, such as Mardi Gras, Dunedin Wines the Blues and the Arts and Crafts Festival. Dunedin is recognized as a vibrant Main Street City, offering quaint retail stores, art galleries, restaurants, and microbreweries. The Fred Marquis Pinellas Trail, a 38.2 mile recreational trail extending from St. Petersburg to Tarpon Springs, runs through the heart of downtown.



[Image B-1]Downtown Dunedin



[Image B-2]Dunedin Causeway

Land Use History of Dunedin

Origins

Prior to 1800, Native Americans, Cuban fishing outposts and a few Spanish expeditions were known to have left the first human imprints on the area. Early permanent settlers were a mix of post-Civil War southerners looking for a new start and immigrants of English and Scottish decent. In the mid 1870s, two Scottish merchants, John Douglas and James Somerville, opened a general store on the waterfront. In 1878, the two Scots succeeded in establishing a Post Office which gave them naming rights for the town. They chose Dunedin, which is Gaelic for their home town of Edinburgh, Scotland. Thanks to a deep water dock, early Dunedin became one of Florida's chief seaports and trading centers. At one time Dunedin had the largest fleet of sailing vessels in Florida.



[Image B-3] Dunedin Marina

Source: Dunedin

Industrial Land Use

The southern settlers brought with them their cotton farming experience following the Civil War. By the early 1870s, cotton became an export crop as it provided currency, could be transported by boat and sustainable. Until the arrival of the railroad in 1888, Downtown Dunedin, as it is known today, was a large cotton field.



[Image B-4] Orange Belt Railroad

Source: Dunedin

The first major land use change to the economy was a shift away from cotton and toward citrus production. By 1880 citrus groves dominated the area. More lucrative than cotton, oranges could also be exported by ship and later by train. The key figure in Dunedin's citrus industry was Lee Bronson (L.B.) Skinner. In 1883, Skinner left Wisconsin and came to Florida to buy land suitable for orange groves. He purchased land north and east of the present day Coca Cola plant.

The citrus industry did very well until the "Great Freeze" of 1894-95, when temperatures dropped into the teens, killing almost every orange tree in Dunedin. Many of the grove owners lost everything and left the area. The ones that stayed, like L.B. Skinner, replanted their groves and were back to producing fruit in five years. With the help of an investor, L.B. Skinner purchased many of the abandoned groves while also inventing machinery to wash, sort, and grade citrus. Incorporated in 1913, the Skinner Machinery Company gradually became the largest manufacturer of citrus packing house equipment in the world.

Championed by Russian immigrant Peter Demens, the Orange Belt Railroad brought trains from Sanford (Florida) to St. Petersburg, opening the door to tourism and creating new markets for local citrus.

The addition of a Dunedin rail station, the Orange Belt Railroad Depot on Main Street catalyzed the development of a true downtown consisting of a mix of retail, services, and residential land uses. Even with the beginnings of a town center, Dunedin's growth was slow as land remained difficult to acquire. Downtown parcels were overpriced, while the larger outlying holdings were kept as seasonal hunting preserves for wealthy northerners. Dunedin population in 1898 was estimated at 100.

Residential Land Use

The 1920s brought Dunedin its first real estate boom and bust:

- Prior to 1924, Fenway on the Bay was Dunedin's only subdivision;
- In October 1924, three new residential developments were announced;
- By 1925, new subdivision plans were being announced almost every month;
- But on November 19, 1925 came the announcement of a project that would dwarf all of the City's previous residential development combined.

The new project, known as the Dunedin Isles Subdivision, was the brainchild of Edward Frischkorn, a successful Michigan home builder, looking to expand to Florida. The Orange Belt Railroad brought well-to-do Northerners to see the new development. A fleet of buses and sales people were used to show land parcels to prospective clients. The Frischkorn-Florida master plan consisted of the following:

- 3,000 acres in size;
- A cost of 63 million dollars;
- Five man-made islands using the dredge and fill technique, each with their own bridge to the mainland and one island with its own golf course;
- A total of 6,200 building lots with a projected population increase of 24,000.

By the end of the decade, the dream was over. An unrealistic plan combined with the Great Depression, forced Frischkorn-Florida into eventual foreclosure, but Dunedin Isles did leave behind a championship golf course and a handful of beautiful Spanish mansions. It would take the next 50 years to gradually populate an adjusted master plan minus the five islands.



[Image B-5] Dunedin Isles
Source: Dunedin



As World War II approached and the Great Depression began to ease, Dunedin's land uses evolved again. L.B. Skinner's industrial operations on the north side of Main Street changed and grew significantly. His citrus machinery plant switched to manufacturing amphibious vehicles for the Marine Corps and with government funding, he built a large citrus concentrate plant to supply frozen orange juice to the war effort. Despite being rebuilt following a devastating 1945 fire and multiple changes in ownership, the plant continues to make Coca Cola soft drink concentrates until 2021.

Following World War II, Dunedin's residential and retail land development followed the same pattern as hundreds of cities and towns across the United States. The ability of every family to own an automobile changed the landscape for the foreseeable future. The new era of auto-oriented development separated land uses into specific residential, retail, institutional, and industrial districts.

In Dunedin, the vacant platted lots left over from the late 1920s boom began to fill with modest single-family homes with one-car garages. At the end of the War, the population of the City was below 2,000. Over the next thirty years, residential development replaced orange groves, yielding a 1978 population of 30,000 residents.



[Image B-6]
Amphibious "Gator" Vehicle
Source: Dunedin



Recreation & Open Space Land Use

Prior to being split by a hurricane in 1921, Caladesi and Honeymoon Islands were formerly known as Hog Island. Henry Scharrer homesteaded the southern end of Hog Island in 1888. Although his wife passed away at a very young age, Henry and his daughter lived comfortably off the food provided by land and sea. After Scharrer died in 1934, Caladesi returned to its natural state with no human habitation. Over time there would be twenty-one, absentee land owners, but the final fate of Caladesi Island would not be determined for another thirty years.

By 1963, the demand for recreation by both residents and tourists moved the State of Florida to develop a long range plan for State parks. Utilizing State funding in the form of a 2.85 million dollar grant, the City Commission was given the opportunity to make Caladesi Island a State park if they could convince all twenty-one private land owners to sell. Through the hard work of Dunedin's Mayor, Jerry Rehm, all twenty-one property owners sold their land, officially creating Caladesi Island State Park in 1967.

While Caladesi Island languished in its uninhabited natural state, Honeymoon Island was experiencing a completely different development plan. In 1900, much like the southern end of Hog Island, only one family lived on the northern end of the Island. By 1919, Ed Haley, a wealthy Clearwater hotel operator became the sole owner of what is now Honeymoon Island, as a base for his alleged Prohibition-era rum-running operations. In 1933, when Prohibition ended, Haley sold the Island to a group of Tampa businessmen.

In 1938, the Island was sold to Clinton Washburn for \$25,000. During his tenure, the Island received its trademark name by allowing newlyweds to honeymoon free in small huts on the beach. In 1958, Washburn sold the Island for \$625,000 to a true development group intent on making Honeymoon Island "Miami Beach – West". The "Miami Beach – West" plan consisted of the following:

- A 2 million dollar toll-free causeway to be constructed from Curlew Road to Honeymoon Island with 100 foot beaches on each side;
- A 3,000 acre dredge and fill project (including the filling of Hurricane Pass) to increase the area suitable for development;
- A 17 acre public beach;
- 4,500 residential lots with additional land set aside for commercial establishments, police and fire stations, schools, churches, playgrounds, and a wastewater plant; and
- A proposed Island population of 16,750.



[Image B-7] Scharrer Homestead
Source: Dunedin



[Image B-8] Caladesi Island
Source: Dunedin



[Image B-9] Honeymoon Island
Source: Dunedin



[Image B-10] Royal Stewart Arms
Source: Dunedin



[Image B-11] Former Publix Supermarket
Location, Now Present Site of Dunedin
Library
Source: Dunedin

Much like the story of Dunedin Isles, “Miami Beach – West” never happened. Multiple development groups through the 1960s misspent the project financing, defaulted on the causeway construction project, and failed to renew the dredge and fill permit that expired in 1969. Only a small part of the original plan was ever built – namely the causeway (completed by a bonding company) and Royal Stewart Arms, a series of multifamily high rise buildings at the southeastern tip of the Island.

By the early 1970s, environmental protection efforts were gaining strength on a national scale and the days of allowing dredge and fill permitting were over. The City, County, and State of Florida eventually purchased Honeymoon Island for 25.5 million dollars. As the 1970s came to a close, Dunedin was fortunate to have two large barrier island recreation areas within the City limits preserved forever.

Retail Land Use

During this same period, like many cities and towns across the United States, retail was chasing residential development along Dunedin’s commercial corridors. As new residential development moved away from the Downtown, auto-oriented strip retail centers sprang to life on arterial roadways.

In Dunedin, three retail corridors evolved to support new subdivisions – namely Douglas Avenue, Patricia Avenue and State Route 580. Single-lot depth retail centers were very popular in the 1950s and 1960s. While it worked well for a while, the evolution of malls and subsequently big box retail have put the City’s commercial corridors in decline.



Downtown Revitalization

As shops, restaurants and services followed residential development to the corridors, the Downtown suffered severely. By the early 1980s, the hundred-year-old quaint Main Street was a dilapidated group of small bars and vacant buildings, devoid of landscaping and trees. The railroad still went through Downtown, but the days of bringing winter tourists or hauling fresh fruit were long gone. It was not until 1988, when a group of visionary Downtown stakeholders initiated the Community Redevelopment Agency (CRA), that the Downtown would begin to change for the better.

Working together, the Downtown stakeholders focused on remaking the downtown into a walkable, pedestrian-centered, village environment:

- State Route 580 was rerouted to Skinner Boulevard, which significantly reduced traffic volume in the Downtown;
- Pioneer Park was refurbished with a performance stage;
- Overhead power lines were buried; and
- Brick paver sidewalks, trees, flowers and shrubs replaced the aging concrete jungle as well as pedestrian scale lighting.

The combination of these CRA funded improvements created a beautiful new ambiance for the Downtown. The private sector embraced the new approach with owners and tenants investing in new retail and restaurants. In 1990, the transformation of the railroad right-of-way to a multi-purpose walking, running and cycling trail began across the region. These changes, combined with the City improvements, created one of the best downtown areas in Florida.

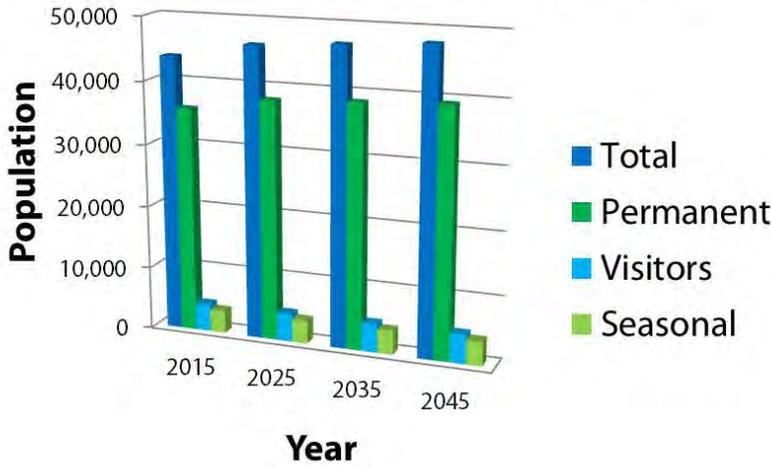


[Image B-12]
Downtown Dunedin-
Douglas Avenue and Main Street, 1980s
Source: Dunedin



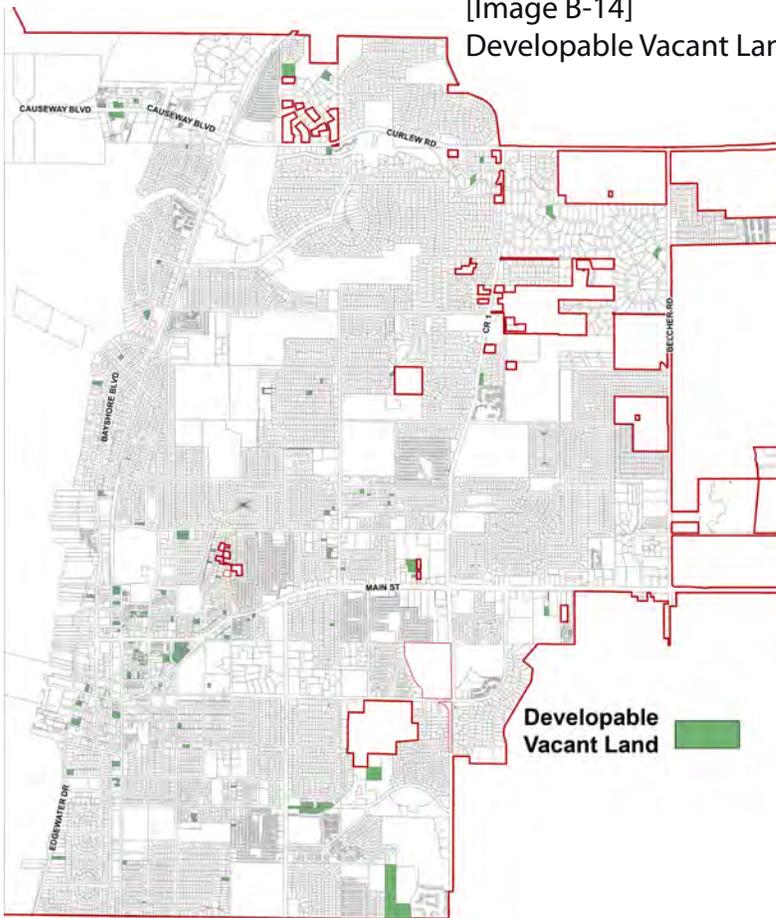
[Image B-13]
Downtown Dunedin-
Pinellas Trail and Main Street, Present Day
Source: Dunedin

Dunedin Population Projection



	2015	2025	2035	2045
Permanent	35,783	38,015	38,668	39,321
Seasonal	3,591	3,706	3,821	3,936
Visitors	4,391	4,484	4,578	4,672
Total	43,765	46,205	47,067	47,929

[Image B-14]
Developable Vacant Land

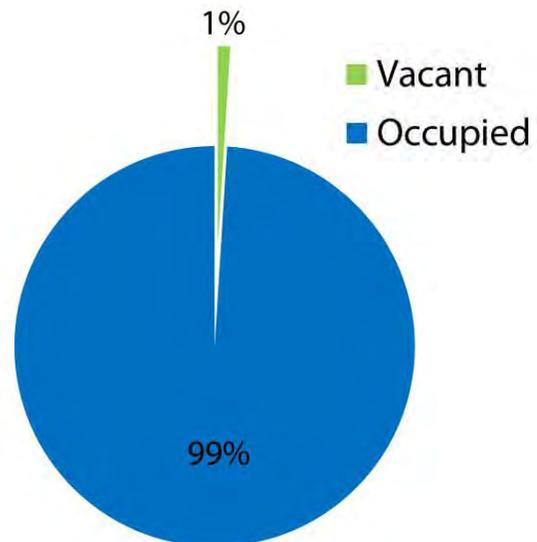


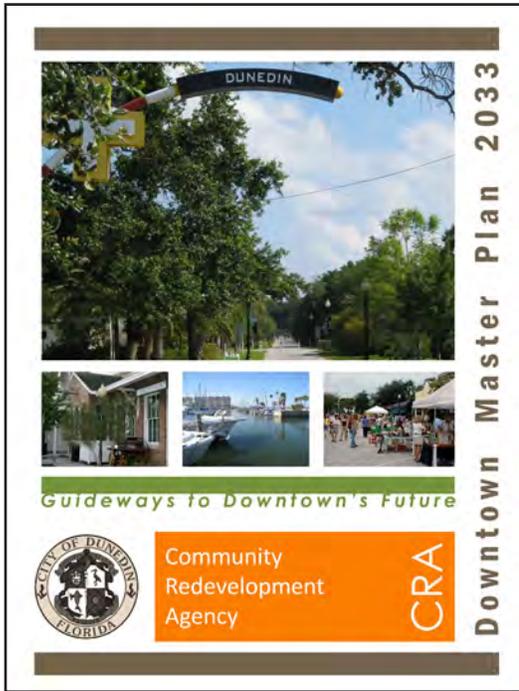
Current Conditions

As of 2019, the City's incorporated area covers 10.39 square miles and has an estimated permanent residential population of 36,580. Dunedin is anticipated to have a steady growth in population over the next 25 years. These population estimates are taken into account for making decisions regarding the amount and type of infrastructure, public facilities and services required to meet future demands. The high population density of 3,520 persons per square mile and predominately built-out land area increases the demand for mixed-use and higher density infill on remaining undeveloped lands and redevelopment of underdeveloped lands to provide new housing options, employment opportunities, and services.

Vacant Land

Currently, a GIS analysis indicates that there are approximately 56.4 acres (about 1 percent) of developable vacant land within the city limits as referenced in Image B-14. While the development pattern has been consistently auto-oriented residential for over sixty years, more recent projects have begun to shape a more urban development pattern focused on centers and corridors where increased density/intensity and mix of uses are encouraged. There is currently one major activity center, four primary multimodal corridors, and several neighborhood activity centers.





[Image B-15]
Downtown Master Plan 2033

Downtown Dunedin

In May 1988, the City of Dunedin adopted a resolution creating the Community Redevelopment Agency (CRA) and established the District comprised of 217 acres and approximately 550 parcels. In December 1988, the City finalized its first CRA plan, "Guideways to Downtown's Future". Since then, the City has enjoyed tremendous success in implementing the strategies and capital projects described in the 1988 plan. As a result, Downtown Dunedin has become a prime end-destination for residents and tourists locally and internationally.

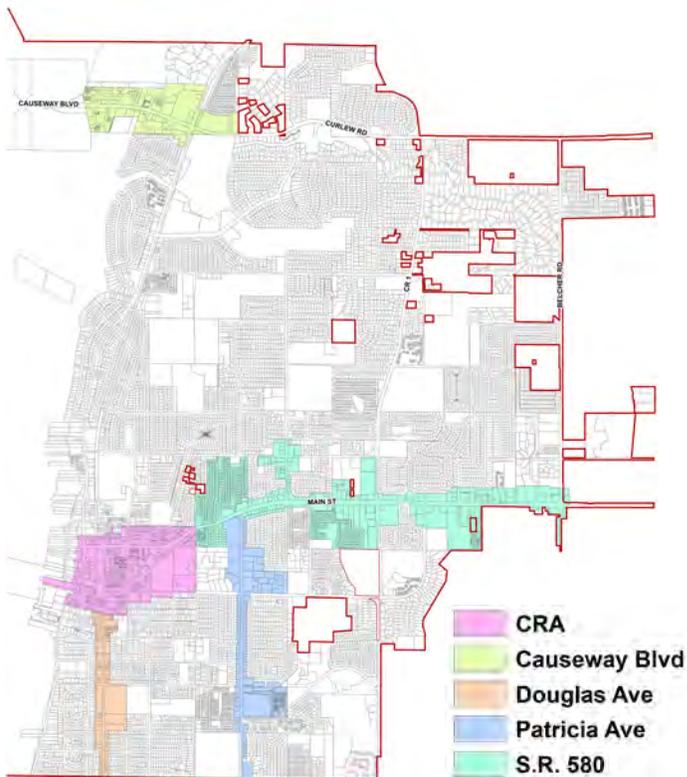
After more than two decades, the City recognized the need to update the CRA Master Plan in order take its quaint Downtown to the next level in response to new opportunities and changes. With the assistance of a consultant, the City initiated its first major update to the 1988 plan in 2009. The CRA Master Plan Update 2033 was adopted in 2011 with the intent to continue the success of the revitalization of the Downtown area as well as planning for a sustainable community.

Multimodal Corridors:

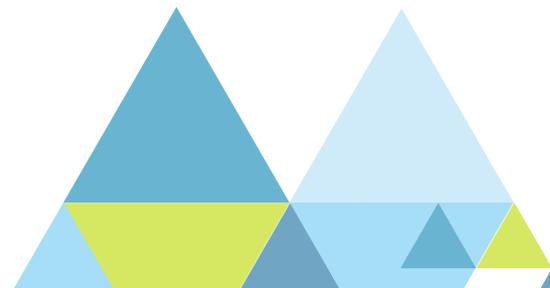
As identified by Visioning Exercises in 2005 and 2017, there are four primary multimodal corridors identified as areas suitable for redevelopment.

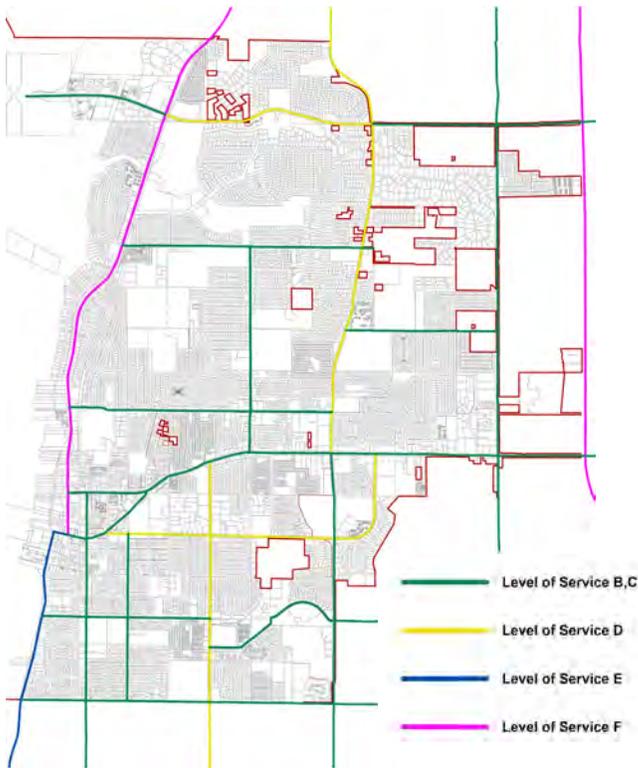
- Main Street (State Road 580)
- Douglas Avenue
- Patricia Avenue
- Causeway Boulevard

These multimodal corridors are arterial roadways that carry both local and regional commuter traffic and are served by Pinellas Suncoast Transit Authority (PSTA) fixed-route buses. Future plans for improvements to these corridors are guided in the respective corridor studies as well as the 2017 Community Visioning Report Update. It is anticipated that with continued intergovernmental collaboration and the creation of the City's Multimodal Transportation Plan, specific projects and initiatives will be carried out to provide infrastructure improvements and beautification to these corridors in the coming years.



[Image B-16]
Multimodal Corridors & CRA





[Image B-17]
Level of Service

Level of Service

Due to long-term proactive planning, Dunedin is well-positioned for any future development or redevelopment. The City's capacity for potable water, waste water, solid waste and parklands meets or exceeds adopted level of service standards to meet the projected permanent and seasonal populations through 2045.

A primary issue for the region is the growing arterial roadway congestion resulting from increased tourism and a lack of transit choices. It is anticipated that continued intergovernmental coordination and the creation of the City's Multimodal Transportation Plan will address possible solutions to roadway congestion with planned roadway improvements and further development of other transportation alternatives.

Level of service standards for transportation, potable water, sanitary sewer, solid waste, and recreation & open space are listed in the goals, objectives, and policies of each respective element of the Plan.

Recent Development

Between 2008 and 2010, a multi-departmental group of City staff members collaborated to accomplish three directives in response to the economic downturn and the lack of development within the City. The three directives were to:

1. Fix the development review process;
2. Change Dunedin's reputation as being a difficult place to build, and;
3. Create a user-friendly Land Development Code.

In 2013 as the City was recovering from the Great Recession, the changes to the development review process, new Land Development Code, and strong real estate market conditions spurred many new development projects in Dunedin.

Despite the success and economic benefits that resulted from changes in development policies and standards, new development projects were not welcomed by all. Many viewed new development projects as a threat to the City's village-like ambiance. In response to community concerns, the City is taking steps to implement the recommendations that resulted from the 2017 Community Visioning Report Update through code amendments and planning initiatives, such as the creation of the Architectural Review Committee which also referenced as a policy in the Future Land Use Element.



[Image B-18]
Courtyard on Main



[Image B-19]
Gramercy Court



1

C H A P T E R O N E

FUTURE LAND USE ELEMENT

Future Land Use

The Future Land Use Element provides a strategic framework for the physical growth and development within the City by determining the use of the land that will influence how people live, work, and play. It determines where residential, commercial, institutional, and industrial uses can be built within the City. It also identifies what land will be preserved and where redevelopment can occur.

The Future Land Use Element establishes the allowable use, intensity, and density of land and provides for consistency between land use categories and zoning districts. This Element also provides guidelines to promote and preserve Dunedin's unique development patterns, while looking to adopt efficient and modern practices of Smart Growth. The goals, objectives, and policies that make up the Future Land Use Element support the preservation of major natural resources while also focusing on a sense of place that values the City's rich history and modern creative pursuits.

Existing Conditions

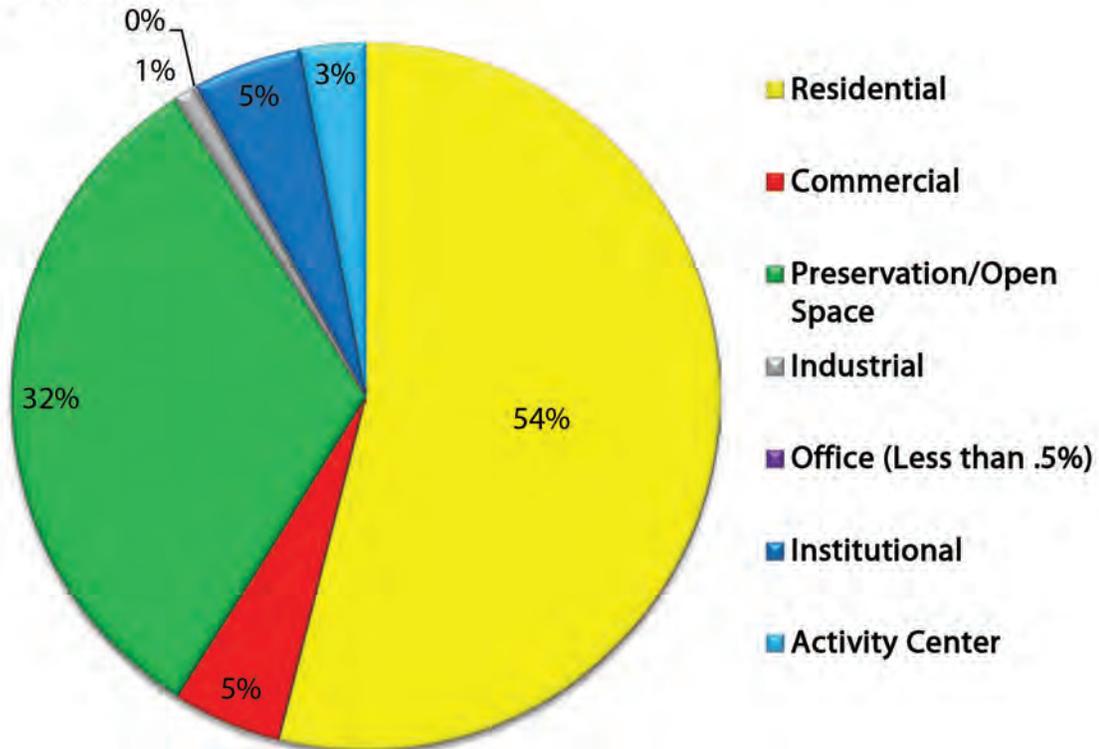
The City of Dunedin is a built-out community with very little vacant land available for development, yet continues to grow in population. Infill development, redevelopment and strategic annexation is essential for steady growth of the City. The predominant development pattern in Dunedin consists of single-story residential uses, one-story office and retail uses, and a small concentration of light industrial uses that lack adequate transit, bicycle, and pedestrian facilities.

Because Dunedin is predominantly built-out, the City has seen an increase in the demand for mixed-use and higher density infill development on the remaining vacant lands as well as redevelopment in the form of new housing options, employment opportunities, and professional services. While the development pattern and character of the City has been historically suburban-dominated residential uses, conditions over the last few decades have shaped a more urban development pattern focused on centers and corridors where increased density, intensity, and mixes of uses are encouraged.



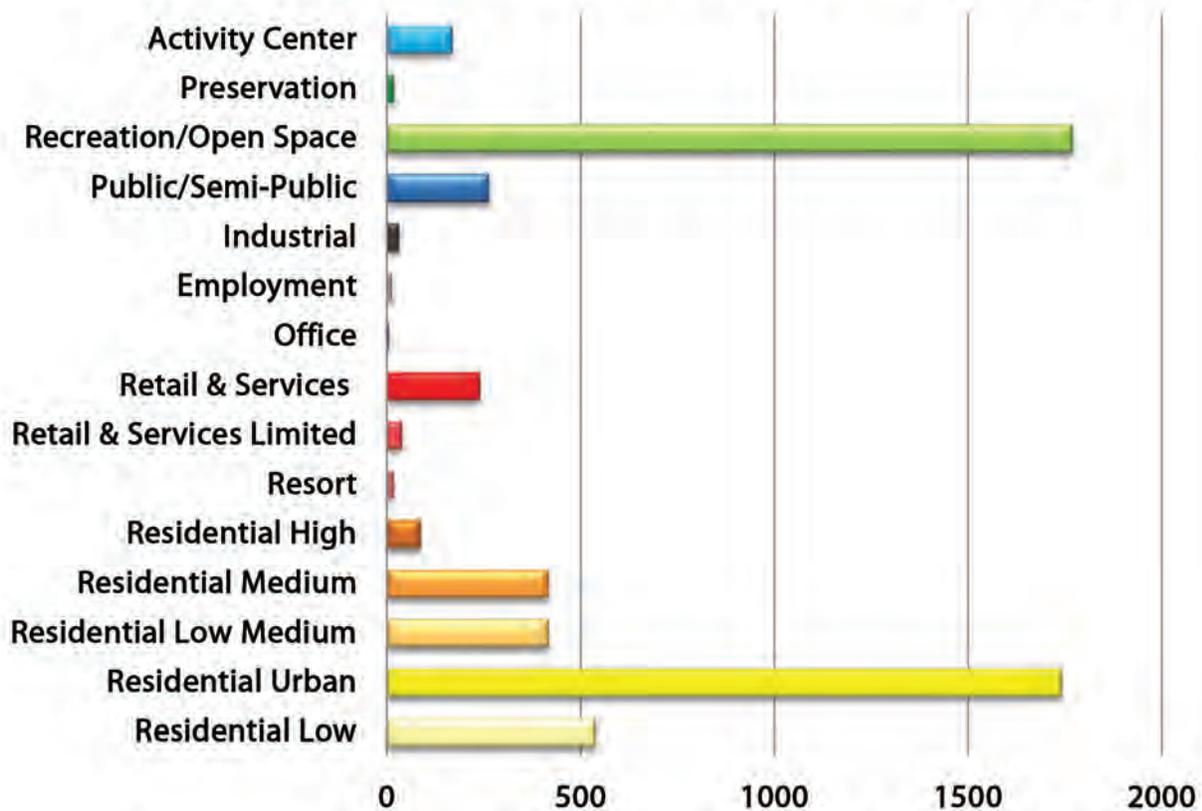
[Figure 1-1] Land Use Distribution by Percentage

Land Use Distribution:



[Figure 1-2] Future Land Use Distribution by Acreage

Future Land Use Classification by Acreage :



Dunedin's Vision

2017 Community Visioning Report Update

The City gathered input from local residents and business owners through a series of workshops and online surveys to determine relevancy to the 2005 visioning plan and assess the community perceptions of growth and development based on the current trend of the City. The feedback from the visioning update resulted in similar elements and goals of the Smart Growth Principles.

Vision for Smart Growth

The Smart Growth Principles provide strategies for providing a land development pattern that uses land efficiently, reinforces community vitality, protects natural resources and helps mitigate the adverse impacts of population growth. Smart Growth is about promoting development that is good for the economy, community and the environment. There are ten Smart Growth principles that can guide the City of Dunedin to a sustainable community and enhance the quality of life of its citizens:

- Mix land uses;
- Take advantage of compact building design;
- Create a range of housing opportunities and choices;
- Create walkable neighborhoods, including the Downtown;
- Foster distinctive, attractive communities with a strong sense of place;
- Preserve open space, natural beauty, and critical environmental areas;
- Strengthen and direct development towards existing communities;
- Provide a variety of transportation choices;
- Make development decisions predictable, fair, and cost effective; and
- Encourage community and stakeholder collaboration in development decisions.

Applying these principles will help the City balance competing demands on the land by supporting development patterns that are orderly and rational, provide the greatest benefit for the community, and help avoid conflicts between land uses.



What does a sustainable community look like?



[Image 1-1] Vision for Smart Growth
Source: Smart Growth Ballston



Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- American Association of Retired Persons (AARP)
- Florida Department of Transportation (FDOT)
- Florida Department of State
- Southwest Florida Water Management District (SWFWMD)
- Tampa Bay Regional Planning Council (TBRPC)
- Forward Pinellas (PPC/ MPO)
- Pinellas County

Plans, Programs/Initiatives:

- AARP Livable Communities
- FDOT Complete Streets Implementation
- Forward Pinellas Countywide Plan (Countywide Rules, Countywide Plan Map, and Countywide Strategies)
- Forward Pinellas 2045 Long Range Transportation Plan (LRTP)
- Forward Pinellas Bicycle Pedestrian Master Plan Facilities Element
- Forward Pinellas Countywide Trends & Conditions Report
- Pinellas by Design
- City of Dunedin Municipal Business Plan and Capital Improvements Plan
- City of Dunedin Land Development Code
- City of Dunedin Code of Ordinances
- City of Dunedin 2017 Visioning Report Update
- City of Dunedin Community Redevelopment Agency Downtown Master Plan 2033
- City of Dunedin Bicycle and Pedestrian Master Plan
- City of Dunedin Douglas Avenue Corridor Study
- City of Dunedin Patricia Avenue Corridor Study
- City of Dunedin Causeway Corridor Report
- City of Dunedin State Road 580 Corridor Study
- City of Dunedin Economic Development Master Plan

Related Maps:

- Map 1-1: Planning Area
- Map 1-2: Future Land Use Designation
- Map 1-3: Visioning Corridors and Areas
- Map 1-4: Vacant Lands

GOAL 1 – *Vision for Smart Growth*- Plan for a healthy and sustainable community by utilizing the Smart Growth principles to mitigate the adverse impacts of population growth and development.

OBJECTIVE 1.1:

Diversify and expand the City through a strategic annexation program focused on compact boundaries, eliminating enclaves, ensuring efficiency of services and infrastructure, providing for consistent land uses, and creating a sustainable community.

Policies:

- 1.1.1. The City will extend the City's municipal limits to the boundaries of its Planning Service Area when financially feasible in accordance with the Interlocal Agreement with Pinellas County.
- 1.1.2. The City will develop informational materials regarding the costs of annexation and the services and amenities offered by the City of Dunedin to inform and educate the public.
- 1.1.3. The City will extend utilities when financially feasible, including the City's sanitary sewer system, potable water system, and reclaimed water system within its service boundaries.
- 1.1.4. The City will continue to process voluntary annexations for single-family residential properties upon request.
- 1.1.5. The City will continue to allow voluntary annexations for noncontiguous properties that are within an enclave as defined by Section 171.031(13)(a), Florida Statutes ("Type A" enclaves) and as authorized by the 2014 Interlocal Service Boundary Agreement (ISBA) between Pinellas County and municipalities, including City of Dunedin.
- 1.1.6. The City will continue to work with property owners and Pinellas County to permit development activities in the County prior to the effective date of annexation provided the proposed development complies with the provisions of the City's Land Development Code.

OBJECTIVE 1.2:

Work toward a land use pattern that can be supported by the available community and public facilities that would be required to serve the development.

Policies:

- 1.2.1 The City shall ensure proposed development is in conformance with existing and planned support facilities' adopted level of service standards. Facilities and services shall be available at the adopted level of service standards and concurrent with the impacts of development.
- 1.2.2. The City shall enforce the level of service standards for parkland, potable water, sanitary sewer, solid waste, and stormwater.
- 1.2.3. When considering requests to amend the Future Land Use Map or the Zoning Map, the City shall review the potential impact on the transportation system by considering the following:
 - 1.2.3.1. Ability of the surrounding existing and planned transportation network to meet the mobility objectives of the Comprehensive Plan;
 - 1.2.3.2. Capacity of the surrounding existing and planned transportation network to accommodate any projected additional demand; and
 - 1.2.3.3. Extent to which the proposed Future Land Use Map amendment or rezoning furthers the intent of the Comprehensive Plan to improve mobility.
- 1.2.4. The City shall recognize the overriding Constitutional principle that private property shall not be taken without due process of law and the payment of just compensation, which principle is restated in Section 163.3194 (4)(a), Florida Statutes.
- 1.2.5. The City hereby designates the land within the corporate limits of the City of Dunedin as an existing urban service area.

OBJECTIVE 1.3:

Coordinate and collaborate with intergovernmental agencies to ensure that the City's goals, objectives, and policies are consistent with the vision for the region.

Policies:

- 1.3.1 The City shall continue to include impacted agencies as members of the City's Development Review Committee (DRC) in the development review process for the purpose of jointly determining constraints and opportunities associated with proposed developments within the City.
- 1.3.2. The City shall continue to participate in regional/County boards, committees and associations.

- 1.3.3. The City shall continue to coordinate with Forward Pinellas and other relevant agencies to engage in joint planning studies, sharing of Countywide data and analysis, and implementation of projects.

OBJECTIVE 1.4:

Plan to provide residents with safe, convenient and interconnected mobility options within neighborhoods, corridors, and other special area districts.

Policies:

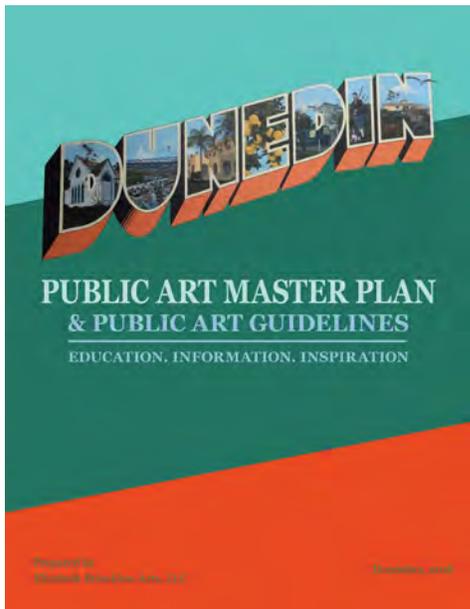
- 1.4.1 The City shall ensure land use regulations remain consistent with City initiatives and programs, as well as Countywide and regional long-range multimodal transit plans that encourage a vibrant mix of uses and street level activity.
- 1.4.2. Where possible, the City shall establish land development patterns, including land use/zoning, density/intensity, urban form and related planning considerations, that facilitate transit-ready conditions in advance of transit service.
- 1.4.3. The City will strive to protect and, if feasible, acquire or gain public access to rights-of-way for urban trail corridors, trails, bicycle and pedestrian paths, future multimodal corridors, and alternative mobility options, as may be identified or needed to create, enhance, connect, or interconnect the needs of each transportation system.
- 1.4.4. The City will develop an active and interconnected Downtown that will create linkages between Downtown Dunedin and the City's Activity Centers, to create a sense of place that offers a destination for residents and visitors alike.
- 1.4.5. The City will work with Pinellas Suncoast Transit Authority (PSTA) to advocate for attractive and ADA compliant bus shelters and similar amenities or other features that can take advantage of vistas, scenic corridors, or natural surroundings.
- 1.4.6. The City will promote transit-oriented development (TOD) principles, where applicable and if possible, through private investment, partnerships, and effective collaboration with relevant agencies.
- 1.4.7. The City shall utilize Complete Streets principles, where applicable and if possible, through private investment, partnerships, and effective collaboration with relevant agencies.

OBJECTIVE 1.5:

Honor the City's history and celebrate its culture, diversity, and character through public art to create a stimulating, rich, and welcoming community for residents and visitors to experience, as referenced in the City's Public Art Master Plan.

Policies:

- 1.5.1. Public art shall be integrated into the architecture, infrastructure, and landscape of the City to enliven spaces, invigorate commerce, and provide an inspiration to people of all ages.
- 1.5.2. The City shall follow the planning framework via guidelines for public art including inventory, maintenance, and conservation.
- 1.5.3. The City will provide education and marketing of the Public Art Master Plan.
- 1.5.4. The City will provide program administration by public arts knowledgeable City staff, public art administrator or via consulting contract.
- 1.5.5. The City shall enforce Chapter 110 of the City's Land Development Code as referenced in the Public Art Master Plan which states 0.5 percent of the cost of each public and private project may be devoted to new public art projects or deposited into the Public Art Fund, if funding sources permit.



[Image 1-2] Dunedin Public Art Master Plan



[Image 1-3] *Bounce*, Robert Ellison



[Image 1-4] Andrews Memorial Chapel
Source: Dunedin Historical Musuem



[Image 1-5] Blatchley House
Source: Dunedin Historical Musuem

OBJECTIVE 1.6:

Preserve and protect historically significant sites and structures in the interest of the health, prosperity, safety, welfare, and economic well-being of the people.

Policies:

- 1.6.1 The City shall recognize the use of historic landmarks as a public necessity to preserve their special historic, architectural, archeological, aesthetic, or cultural interest and value of the City, State, and Nation.
- 1.6.2. In conjunction with Objective 2.8 and Objective 3.2 of the Future Land Use Element, the City shall preserve the rare historic resources that support the City's unique identity.
- 1.6.3. The City will promote the educational, cultural, economic, and general welfare of historically significant sites and structures to foster civic pride in the value of notable accomplishments of the past.
- 1.6.4. The City will implment State law relating to exemption from certain ad valorem taxes for historic properties to provide an incentive for renovation or rehabilitation of historic structures to enhance the visual and aesthetic character, diversity, and interest of the City as well as to support and stimulate the local economy, business, and industry.

GOAL 2 – Future Land Use- Achieve a healthy balance of land uses to develop a sustainable land use pattern to ensure compatibility between residential and non-residential uses while preserving neighborhood character.

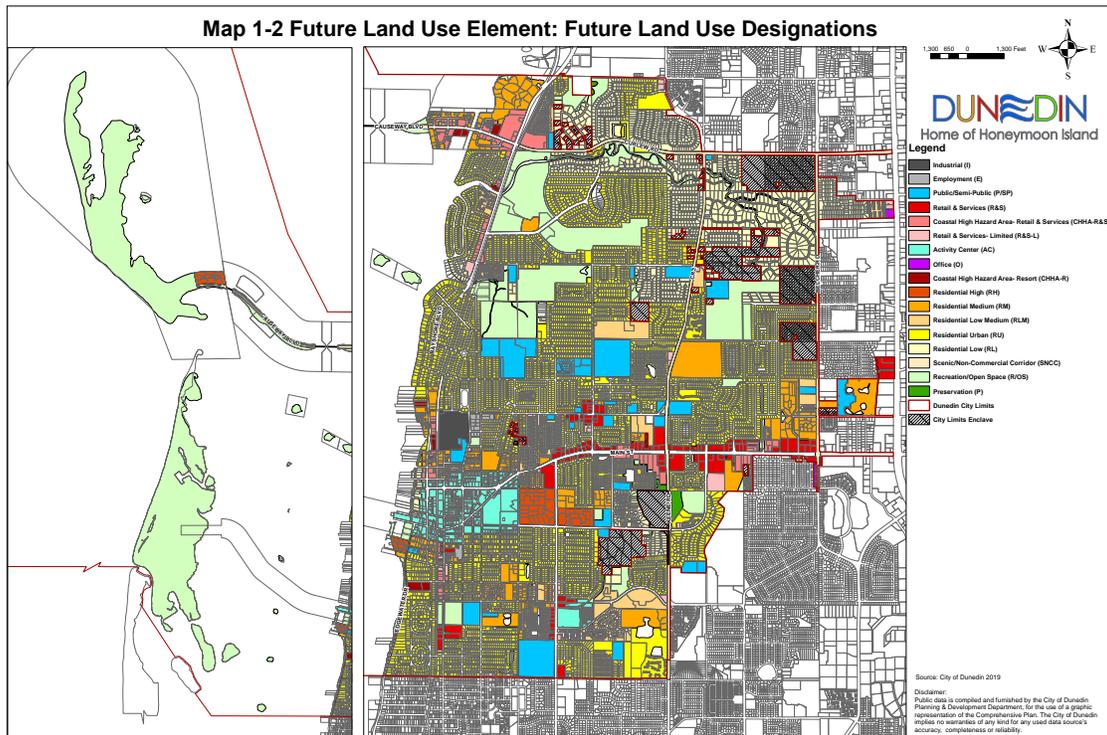
OBJECTIVE 2.1:

Maintain the City’s Future Land Use Map and Future Land Use Designations to ensure consistency with the Countywide Plan for Pinellas County (The Countywide Plan) and the Countywide Plan Map.

Policies:

- 2.1.1. Future Land Use Designations on the City’s Future Land Use Map shall generally be interpreted as indicated in Table 1-1: Future Land Use.
- 2.1.2. The City’s Future Land Use Map and Future Land Use Designations shall be implemented through the City’s Land Development Code.
- 2.1.3. Zoning districts shall be compatible and consistent with the Future Land Use designations as outlined in the Land Development Code.
- 2.1.4. All Future Land Use designations on the Future Land Use Map shall be consistent with the density and intensity standards and other standards contained in the Forward Pinellas Countywide Plan Rules, including criteria and standards for nomenclature, continuum of plan classifications and categories, use and locational characteristics, map delineation, other standards and special rules.
- 2.1.5. Development shall not exceed the densities and intensities specified on the Future Land Use Map and Future Land Use Designation Table, unless otherwise permitted by approved special area or redevelopment plans.
- 2.1.6. Residential equivalent uses shall not exceed 3.0 beds per permitted dwelling unit based on the underlying residential density.





[Image 1-6] Map 1-2: Future Land Use Designations

- 2.1.7. The City will continue to provide density bonuses for affordable and workforce housing as established in the Land Development Code. Density bonuses shall be permitted in all land use designations that allow residential uses except properties located in the Coastal Storm Area as defined in the Coastal Management Element. The bonus shall not exceed 50% of the density permitted by the future land use designations as shown on the City's adopted Future Land Use Map. Qualification for affordable and workforce housing density bonuses shall be based on when the development proposes units affordable to households earning less than 120% of the Pinellas County Median Income and/or an age-restricted senior multifamily development.
- 2.1.8. Temporary lodging uses shall not exceed the density provided by the Land Development Code and the Countywide Plan.
- 2.1.9. Subdivision of land shall be consistent with and governed by the Comprehensive Plan goals, objectives, and policies regarding future land use, transportation, conservation, and support services.
- 2.1.10. The City shall continue to support the adoption and utilization of Future Land Use categories and subcategories made available by the Countywide Plan that are consistent with and promote infill and desired redevelopment along corridors.

Table 1-1: Future Land Use

City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses	Maximum Density (dwelling unit/acre)	Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)	Corresponding Countywide Plan Map Categories/Locational Criteria
<p>Residential Low (RL) In areas where use and development characteristics are low-density residential in nature; and in areas serving as a transition between more suburban and more urban residential areas. <u>Permitted Uses:</u> Primary- Residential Secondary- Residential Equivalent; Institutional; Public Educational Facility; Ancillary Non-Residential; Recreation/Open Space.</p>	<p>5.0 du/a</p>	<p>0.40 FAR 0.65 ISR</p>	<p>Residential Low Medium (RLM) This category is intended to depict areas that are now developed, or appropriate to be developed, in a suburban, low density or moderately dense residential manner; and to recognize such areas as primarily well-suited for residential uses that are consistent with the suburban qualities, transportation facilities, including transit, and natural resources of such areas. Maximum Density (unit/acre): 10 du/a Intensity Floor Area/Impervious Surface Ratio: 0.50 FAR 0.75 ISR</p>
<p>Residential Urban (RU) In areas removed from, but in close proximity to urban activity centers; in areas where use and development characteristics are urban residential in nature; and in areas serving as a transition between more suburban and more urban residential areas. <u>Permitted Uses:</u> Primary- Residential Secondary- Residential Equivalent; Institutional; Public Educational Facility; Ancillary Non-Residential; Recreation/Open Space.</p>	<p>7.5 du/a</p>	<p>0.40 FAR 0.65 ISR</p>	

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Residential Low Medium (RLM) In areas in close proximity to urban activity centers; in areas where use and development characteristics are low-medium residential in nature; and in areas serving as a transition between low density and high density residential areas. <u>Permitted Uses:</u> Primary- Residential Secondary- Residential Equivalent; Institutional; Public Educational Facility; Ancillary Non-Residential; Recreation/Open Space.</p>	<p>10.0 du/a</p>	<p>0.50 FAR 0.75 ISR</p>	<p>Residential Low Medium (RLM) This category is intended to depict areas that are now developed, or appropriate to be developed, in a suburban, low density or moderately dense residential manner; and to recognize such areas as primarily well-suited for residential uses that are consistent with the suburban qualities, transportation facilities, including transit, and natural resources of such areas. Maximum Density (unit/acre): 10 du/a Intensity Floor Area/Impervious Surface Ratio: 0.50 FAR 0.75 ISR</p>
<p>Residential Medium (RM) In areas within or in close proximity to urban activity centers; in areas where use and development characteristics are medium-density residential in nature; and in areas serving as a transition between less urban and more urban residential and mixed use areas. <u>Permitted Uses:</u> Primary- Residential Secondary- Residential Equivalent; Institutional; Public Educational Facility; Ancillary Non-Residential; Recreation/Open Space.</p>	<p>15.0 du/a</p>	<p>0.50 FAR 0.75 ISR</p>	<p>Residential Medium (RM) It is the purpose of this category to depict those areas of the County that are now developed, or appropriate to be developed, in a medium-density residential manner; and to recognize such areas as primarily well-suited for residential uses that are consistent with the urban qualities, transportation facilities, including transit, and natural resources of such areas. Maximum Density (unit/acre): 15 du/a Intensity Floor Area/Impervious Surface Ratio: 0.50 FAR 0.75 ISR</p>

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Residential High (RH) In areas within or in close proximity to urban activity centers; in areas where use and development characteristics are high-density residential in nature; and in areas serving as an urban center. <u>Permitted Uses:</u> Primary- Residential Secondary- Residential Equivalent; Institutional; Public Educational Facility; Ancillary Non-Residential; Recreation/Open Space.</p>	<p>30.0 du/a</p>	<p>0.60 FAR 0.85 ISR</p>	<p>Residential High (RH) It is the purpose of this category to depict those areas of the County that are now developed, or appropriate to be developed, in a high-density residential manner; and to recognize such areas as primarily well-suited for residential uses that are consistent with the urban and intensive qualities, transportation facilities, including transit, and natural resources of such areas. Maximum Density (unit/acre): 30 du/a Intensity Floor Area/Impervious Surface Ratio: 0.60 FAR 0.85 ISR</p>
<p>Office (O) In areas where it would serve as a transition from an urban activity center or more intensive non-residential use to low density residential or public/semipublic use; and in areas where the size and scale of office and residential use is appropriate to free standing office, medium density residential or a combination thereof. <u>Permitted Uses:</u> Primary- Office Secondary- Residential; Residential Equivalent; Institutional.</p>	<p>15.0 du/a</p>	<p>0.50 FAR 0.75 ISR</p>	<p>Office (O) This plan category is intended to accommodate areas developed, or appropriate to be developed, with office uses, low-impact employment uses, and residential uses (subject to an acreage threshold), in areas characterized by a transition between residential and commercial uses and in areas well-suited for community-scale residential/ office mixed-use development. Maximum Density (unit/acre): 15 du/a Intensity Floor Area/Impervious Surface Ratio: 0.50 FAR 0.75 ISR</p>

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Resort (R) In areas where it would identify existing moderately intensive mixed residential and hotel/ motel use; or in locations where unique recreational assets warrant the combination of permanent and temporary accommodations. It is also areas adjacent to activity centers or areas designated for commercial use; in water-dependent locations for marina and boat service use. <u>Permitted Uses:</u> Primary- Transient Accommodation; Residential Secondary- Residential Equivalent; Institutional; Tourist Facilities.</p>	<p>30.0 du/a</p>	<p>0.55 FAR 0.90 ISR</p>	<p>Resort (R) This plan category is intended to depict areas developed, or appropriate to be developed, in high-density residential and resort use; and to recognize such areas as well-suited for the combination of residential and temporary lodging use consistent with their location, surrounding uses, transportation facilities, and natural resources of such areas. Maximum Density (unit/acre): 30 du/a Intensity Floor Area/Impervious Surface Ratio: 1.20 FAR 0.95 ISR</p>
<p>Coastal High Hazard Area- Resort (CHHA-R) In areas where it would identify existing moderately intensive mixed residential and hotel/ motel use; or in locations where unique recreational assets warrant the combination of permanent and temporary accommodations located in the Coastal High Hazard Area. It is also areas adjacent to activity centers or areas designated for commercial use; in water-dependent locations for marina and boat service use located in the Coastal High Hazard Area. <u>Permitted Uses:</u> Primary- Transient Accommodation; Residential Secondary- Residential Equivalent; Institutional; Tourist Facilities.</p>	<p>15.0 du/a</p>	<p>0.65 FAR 0.85 ISR</p>	

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Retail & Services Limited (R&S- L) In areas where it would serve as a transition from an urban activity center or more intensive non-residential use to residential, office or public/semi-public use; and in areas where the size and scale of development will accommodate true mixed residential, office, and retail use. <u>Permitted Uses:</u> Primary- Office; Residential; Retail Commercial; Personal Service Secondary- Residential Equivalent; Institutional.</p>	<p>15.0 du/a</p>	<p>0.45 FAR 0.85 ISR</p>	<p>Retail & Services (R&S) This plan category is intended to depict areas developed with, or appropriate to be developed with, a mix of businesses that provide for the shopping and personal service needs of the community or region, provide for employment opportunities and accommodate target employment uses, and may include residential uses as part of the mix of uses. Maximum Density (unit/acre): 24 du/a Intensity Floor Area/Impervious Surface Ratio: 0.55 FAR 0.90 ISR</p>
<p>Retail & Services (R&S) In areas in and adjacent to activity centers where surrounding and uses support and are compatible with intensive commercial use; in areas supported by arterial or major collector roadways. <u>Permitted Uses:</u> Primary- Retail Commercial; Office; Personal Service; Commercial/Business Service; Transient Accommodation; Restaurant; Financial Institution Secondary- Residential; Residential Equivalent; Institutional.</p>	<p>24.0 du/a</p>	<p>0.55 FAR 0.90 ISR</p>	

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Coastal High Hazard Area- Retail & Services (CHHA- R&S) Located in the Coastal High Hazard Area-areas in and adjacent to activity centers where surrounding uses support and are compatible with intensive commercial use; in areas supported by arterial or major collector roadways. <u>Permitted Uses:</u> Primary- Retail Commercial; Office; Personal Service; Commercial/Business Service; Transient Accommodation; Restaurant; Financial Institution Secondary- Residential; Residential Equivalent; Institutional.</p>	<p>15.0 du/a</p>	<p>0.55 FAR 0.90 ISR</p>	<p>Retail & Services (R&S) This plan category is intended to depict areas developed with, or appropriate to be developed with, a mix of businesses that provide for the shopping and personal service needs of the community or region, provide for employment opportunities and accommodate target employment uses, and may include residential uses as part of the mix of uses. Maximum Density (unit/acre): 24 du/a Intensity Floor Area/Impervious Surface Ratio: 0.55 FAR 0.90 ISR</p>
<p>Employment (E) In areas with sufficient size to encourage an industrial park arrangement, as well as integrated industrial/mixed-use projects, with provision for internal service access in locations suitable for light industrial use with minimal adverse impact on adjoining uses. <u>Permitted Uses:</u> Primary- Light Manufacturing; Research; Office; Wholesale Sales; Office Machine Repairs Secondary- Retail Sales.</p>	<p>N/A</p>	<p>0.65 FAR 0.85 ISR</p>	<p>Employment (E) This plan category is intended to recognize areas developed with, or appropriate to be developed with, a wide range of employment uses, including primary industries (i.e., those with a customer base that extends beyond Pinellas County), allowing for flex space, and for uses that have minimal external impacts. Maximum Density (unit/acre): N/A, for Temporary Lodging Use- 50 du/a, subject to five acre maximum Intensity Floor Area/Impervious Surface Ratio: 0.65 FAR 0.85 ISR</p>

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Industrial (I) In areas with sufficient size to encourage an industrial park type arrangement with provision for internal service access and adequate buffering of adverse noise, odor, or emissions; with minimal adverse impact on adjoining uses. <u>Permitted Uses:</u> Primary- Assembly and Manufacturing; Warehousing; Open Storage; Foundry; Wholesale Sales Secondary- Business Office.</p>	<p>N/A</p>	<p>0.75 FAR 0.95 ISR</p>	<p>Industrial (I) This plan category is intended to depict areas developed, or appropriate to be developed, in a general industrial manner; and so as to encourage the reservation and use of areas for industrial use in a manner consistent with surrounding use, transportation facilities, other necessary infrastructure, and natural resources. Maximum Density (unit/acre): N/A Intensity Floor Area/Impervious Surface Ratio: 0.75 FAR 0.95 ISR</p>
<p>Public/Semi-Public (P/SP) In areas where educational, health, public safety, civic, religious, and like institutional uses are required to serve the community. It is also areas where such utility installations are required to serve the community. <u>Permitted Uses:</u> Primary- Elementary, Middle and High Schools; Hospital; Church; Cemetary; Fraternal or Civic Organization; Municipal Buildings; Water Treatment Plant; Public Works Garage; Wastewater Treatment Plant; Electric Substation Secondary- Residential; Residential Equivalent.</p>	<p>12.5 du/a</p>	<p>Institutional Use 0.65 FAR 0.85 ISR Hospital Use 1.0 FAR 0.85 ISR Transportation/ Utility Use 0.70 FAR 0.90 ISR</p>	<p>Public/Semi-Public (P/SP) This plan category is intended to recognize institutional and transportation/utility uses that serve the community or region, especially larger facilities having acreage exceeding the thresholds established in other plan categories, which are consistent with the need, character, and scale of such uses relative to the surrounding uses, transportation facilities, and natural resource features, and may include residential as part of the mix of uses. Maximum Density (unit/acre): 12.5 du/a Intensity Floor Area/Impervious Surface Ratio: Institutional Use- 0.65 FAR 0.85 ISR Hospital Use- 1.0 FAR 0.85 ISR Transportation/Utility Use- 0.70 FAR 0.90 ISR</p>

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Recreation/Open Space (R/OS) Public and private open spaces and recreational facilities dispersed throughout the City. <u>Permitted Uses:</u> Primary- Open and Undeveloped Areas; Public/Private Open Space; Public/Private Park; Public Recreation Facility; Public Beach/Water Access; Golf Course/Clubhouse.</p>	<p>N/A</p>	<p>0.25 FAR 0.60 ISR</p>	<p>Recreation/Open Space (R/OS) This plan category is intended to recognize recreation/open space uses that serve the community or region. Maximum Density (unit/acre): N/A Intensity Floor Area/Impervious Surface Ratio: 0.25 FAR 0.60 ISR</p>
<p>Preservation (P) In areas that need to recognize natural resource features, wherever they may appear, and at a size significant to the feature being depicted in relationship to its surroundings; will frequently occur in a random and irregular pattern interposed among the other categories. <u>Permitted Uses:</u> Primary- Open and Undeveloped Areas; Swamps; Streams; Drainage Areas; Estuary.</p>	<p>N/A</p>	<p>0.10 FAR 0.10 ISR</p>	<p>Preservation (P) This plan category is intended to recognize natural resource features worthy of preservation and those areas of the County that are now used, or are appropriate to be used, for the conservation, production, and management of the regional potable water supply and the supporting infrastructure, consistent with the natural resources of the area. Maximum Density (unit/acre): N/A Intensity Floor Area/Impervious Surface Ratio: 0.10 FAR 0.20 ISR</p>

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Activity Center(AC) By Special Area Plan <u>Permitted Uses:</u> By Special Area Plan</p>	<p>By Special Area Plan</p>		<p>Activity Center (AC) The purpose of this category is to recognize those areas of the County within each local government jurisdiction that have been identified and planned for in a special and detailed manner, based on their unique location, intended use, appropriate density/intensity, and pertinent planning considerations. In particular, it is the intent of this category to recognize those important, identifiable centers of business, public, and residential activity, as may be appropriate to the particular circumstance, that are the focal point of a community, and served by enhanced transit commensurate with the type, scale, and intensity of use. Activity Centers are intended to encompass areas developed in a radial pattern within walking distance (1/4 to 1/2 mile) of a central point or hub served by transit.</p>

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Activity Center(AC) By Special Area Plan <u>Permitted Uses:</u> By Special Area Plan</p>	<p>By Special Area Plan</p>		<p>Multimodal Corridor (MMC) This plan category is intended to recognize those corridors of critical importance to the movement of people and goods throughout the County and that are served by multiple modes of transport, including automobile, bus, bicycle, rail, and/or pedestrian. This category is intended to include those transportation corridors connecting Activity Centers, characterized by mixed-use development, and in particular, supported by and designed to facilitate transit.</p>

Table 1-1: Future Land Use (Continued)

<p>City of Dunedin Land Use Designations/Locational Criteria and Permitted Uses</p>	<p>Maximum Density (dwelling unit/acre)</p>	<p>Intensity Floor Area Ratio (FAR)/ Impervious Surface Ratio (ISR)</p>	<p>Corresponding Countywide Plan Map Categories/Locational Criteria</p>
<p>Scenic/Non-Commercial Corridor (SNCC) Belcher Road from City's southern boundary to Curlew Road; Curlew Road from Alternate US 19 to US 19; Alternate US 19 from Union to Scotland; CR 1 from Union Street to Hermosa. <u>Permitted Uses:</u> Related to Special Rules of the Countywide Rules</p>	<p>Refer to Countywide Plan Rules</p>		<p>Scenic/Noncommercial Corridor (SNCC) The intent and purpose of the Scenic/ Noncommercial Corridor designation is to guide the preservation and enhancement of scenic qualities, to ensure the integrity of the Countywide Plan Map, and to maintain and enhance the traffic operation of these especially significant roadway corridors in Pinellas County.</p>
<p>Water/Drainage Feature (WDF) Throughout city <u>Permitted Uses:</u> Primary- Open and Undeveloped Areas consistent with the water and/ or drainage features characterizing these locations. Secondary- Drainage Structures and facilities, environmental restoration and nonpermanent open space and recreation uses consistent with the primary purpose of stormwater management.</p>	<p>Submerged land; drainage feature- No density/intensity allocation.</p>		<p>No Corresponding Category</p>

OBJECTIVE 2.2:

Encourage development or redevelopment which results in elimination or reduction of uses inconsistent with Dunedin's character and the Future Land Use Map.

Policies:

- 2.2.1 The City will actively promote redevelopment of sites from an incompatible land use to a compatible use.
- 2.2.2 During the review process for the redevelopment of land, the City will focus on issues of compatibility. Redevelopment incompatible with adjacent land uses shall be denied.
- 2.2.3 The City shall not allow land uses that are not compatible or consistent with the Future Land Use Map to expand.
- 2.2.4 During the review process for new development or modifications or expansions to existing development, the City will focus on issues of compatibility. New or expanding development incompatible with adjacent land uses shall be denied.
- 2.2.5 The transition between intensive redevelopment areas and adjacent low density areas should be sensitive to the scale of development in those low density areas.
- 2.2.6 The City will coordinate with adjacent jurisdictions and review local government plans to ensure compatibility with existing and future planned uses along common boundaries.



OBJECTIVE 2.3:

Continue to ensure land development regulations are consistent with the Comprehensive Plan and all development approvals are consistent with those development regulations.

Policies:

- 2.3.1 The City shall utilize the Land Development Code as the primary regulatory document to guide development in accordance with the goals, objectives, and policies of the Comprehensive Plan.
- 2.3.2. Development regulations adopted by the City of Dunedin, State of Florida and Federal Government must be met or exceeded by all development.
- 2.3.3. Land development regulations which implement the Future Land Use Map shall be maintained or developed, and which:
 - 2.3.3.1. Regulate the subdivision of land;
 - 2.3.3.2. Regulate signage;
 - 2.3.3.3. Regulate areas subject to seasonal and periodic flooding and provide drainage and stormwater management;
 - 2.3.3.4. Ensure the compatibility of adjacent land uses; and
 - 2.3.3.5. Specify required open space, landscaping, vehicle parking and safe and convenient on-site traffic flow.
- 2.3.4. Innovative land development regulations and design standards/principles, such as character districts, form-based code, smart growth principles, low-impact development and complete streets principles, shall be encouraged as a way of implementing the Future Land Use Map.
- 2.3.5. The City shall continue to involve residents, business owners, and other applicable stakeholders in the early stages of any development to foster local ideas and address local concerns for the proposed planning areas.



[Image 1-7]Downtown Dunedin



[Image 1-8] Edgewater Drive
Source: hippostcard.com

OBJECTIVE 2.4:

Maintain adequate land designated for residential uses to accommodate the projected population and provide a balanced and organized combination of residential, non-residential, recreational, and public uses.

Policies:

- 2.4.1 The City shall protect stable residential neighborhoods from possible negative impacts due to incompatible land uses, blighting influences, and density increases from redevelopment and new development.
- 2.4.2. The City shall maintain adequate land designated for residential uses, including multifamily and mixed income communities that support a healthy environment, active and passive recreation facilities, walkability, and a prosperous economy.
- 2.4.3. The City shall promote a wide variety of residential uses/types, including detached and attached single-family, residential multifamily residential, mixed-use residential, live-work residential, assisted living, long-term care and multi-generation accommodations among others, consistent with the socioeconomic profiles of the City's households and future needs consistent with the Comprehensive Plan.
- 2.4.4. The City will implement a broad set of programs to engage residents, help neighborhoods promote their unique identities, and develop neighborhood plans to ensure long-term stability and success of the City's residential areas.

OBJECTIVE 2.5:

Consider the essential character attributes and quality of life features found in existing and future residential neighborhoods as part of land use planning and management.

Policies:

- 2.5.1 The City shall ensure that existing residential areas are protected from the incompatible non-residential activities; likewise, non-residential land use areas shall be protected from incompatible residential activities through the development review process.
- 2.5.2. Existing residential areas shall be located and designed to protect life and property from natural and manmade hazards such as flooding, excessive traffic, subsidence, noxious odors, noise, and deterioration of structures.
- 2.5.3. New development shall be compatible with the type and scale of surrounding land uses as referenced in Objective 2.9 of the Future Land Use Element.



OBJECTIVE 2.6:

Ensure non-residential uses are planned and designed to be compatible with environmental and economic resources through intensity and other development standards, consistent with the Future Land Use Map Category Criteria.

Policies:

- 2.6.1 The City shall require the installation of appropriate buffering and/or screening between non-residential and residential land uses.
- 2.6.2. The City shall provide regulations to ensure that non-residential land uses are located in a manner that promotes compatibility with the type and scale of surrounding land uses, where existing programmed public facilities shall not be overburdened, and in compliance with the intensity requirements adopted by the Future Land Use Map Categories.
- 2.6.3. The City shall provide regulations to ensure that any disruption within the community and the relocation of residents resulting from redevelopment efforts is minimized as referenced in the Housing Element.
- 2.6.4. The City shall establish provisions whereby neighborhood commercial development is mixed in or located convenient to residential areas, but not in a manner that will adversely affect the quality of life of the community it serves.

OBJECTIVE 2.7:

Encourage improved land use compatibility through the evaluation of traffic calming techniques, multimodal transportation networks, and the use of transit-oriented development planning.

Policies:

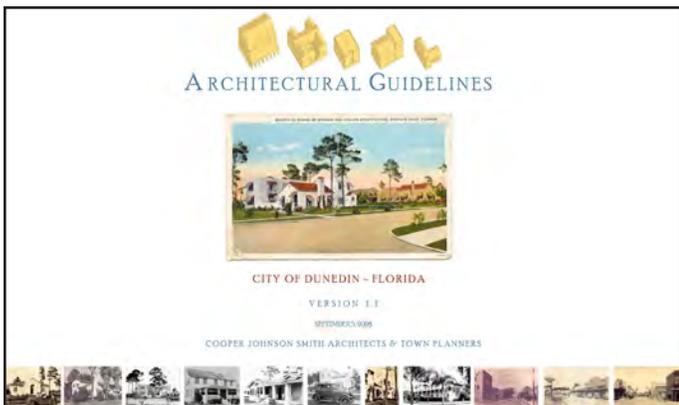
- 2.7.1 The City shall support the Pinellas Trail and examine opportunities for trail extensions or spurs to connect residential neighborhoods and regional attractions and employers.
- 2.7.2. The City shall continuously explore traffic calming techniques in the effort to improve safety and promote a livable environment throughout the City.
- 2.7.3. All proposed development/redevelopment initiatives shall be reviewed for opportunities to improve pedestrian and bicycle access and consider the integration of bicycle and pedestrian transportation modes in all phases of transportation planning, new roadway design, roadway construction, roadway resurfacing, and other capital projects.

- 2.7.4. The City will implement the objectives and policies referenced in the Transportation Element regarding the promotion of alternative transportation strategies, improvement, and expansion of a multimodal transportation system, and regional coordination toward implementation of the Multimodal Transportation Plan.

OBJECTIVE 2.8:
Promote high-quality design standards that support Dunedin’s unique characteristics and identity.

Policies:

- 2.8.1 Development shall be designed to maintain and support the existing or envisioned character of the neighborhood.
- 2.8.2. The City shall preserve the unique qualities of Dunedin and its cultural heritage as exemplified by historic buildings, landmarks, landscapes, streetscapes, lighting, and building design.
- 2.8.3. The City will explore the preparation of redevelopment plans and/or design guidelines for activity centers and redevelopment corridors, also known as visioning corridors/areas, as identified on the Visioning Corridors/Areas map, to minimize adverse impacts on properties in the immediate area.
- 2.8.4. The Architectural Review Committee shall periodically review the approved Design Guidelines and recommend updates to the City to enhance the quality of architectural design, achieve more compatible relationships in the design of buildings, avoid unsightly appearance, and avoid inordinate contrast in building mass, scale, height, articulation, and other design features.



[Image 1-9] Dunedin Architectural Guidelines



[Image 1-10] Craftsman Bungalow
Source: Dunedin Architectural Guidelines

OBJECTIVE 2.9:

Evaluate development and redevelopment for compatibility with the surrounding land and its uses. Development and redevelopment deemed not compatible will be denied. Compatibility shall be broadly defined to include physical dimensions, compatibility with surrounding properties, economic use compatibility, architectural compatibility with commercial areas and residential areas, and other matters that are relevant to the integration of the proposed development or redevelopment into the portion of the City which it is located.

Policies:

- 2.9.1. Physical compatibility of the proposed application with the surrounding environment shall include the following criteria:
 - 2.9.1.1. Height Issues-
Height of the existing structures.
Height compatibility will take into account the impacts on abutting property as to light, air, and view and other possible negative impacts on abutting property from the proposed project.
Land Development Code provisions concerning heights in individual districts or areas establish only the maximum height allowed and the actual permissible height is a decision to be determined by a compatibility analysis during design review or other development approvals. Approved heights may be substantially less based on this compatibility analysis.
 - 2.9.1.2. Architectural Features of the Existing Structures-
Compatibility review will consider whether or not an architectural style is substantially compatible with existing styles within the neighborhood or general area of the proposed project to the degree that such would be aesthetically unpleasing, potentially damage property values, or discourage development and redevelopment within the area. Even in the absence of adopted architectural guidelines, the compatibility of the proposed architectural style shall be considered as to aesthetic and economic impacts.
 - 2.9.1.3. The Historic Aspects of the Existing Structures-
Compatibility review will consider the impact of the proposed project on historic structures in the neighborhood or general area, including aesthetic and economic impacts.
 - 2.9.1.4. Existing Natural Features (e.g., wetlands, open space)-
To protect wetlands, open space and similar types of uses that provide open space, environmental sustainability, waterfront accommodations, marinas and waterfront activities. Preserve public access to waterfront venues, including the visibility of St. Joseph's Sound. Efforts will be made to enhance and protect of waterfront vistas for the general public and from adjacent properties.

- 2.9.1.5. Existing Land Uses-
Land use compatibility is not limited to a specific district designation but deals with exact uses.
Compatibility will include an analysis of whether the proposed application will negatively impact (e.g., including but not limited to noise, odor, traffic, etc.) the adjacent properties based on the proposed use on the subject site. The application shall demonstrate how the proposed use will mitigate potential negative impacts to the surrounding properties.
- 2.9.1.6. Existing Residential Densities-
Existing nonresidential intensities (e.g., floor area ratios, impervious surface ratios).
The existing commercial use of property or the change of an entirely commercial or partially commercial use to a totally residential or lesser commercial use may be considered in a compatibility analysis if such is relevant to the economic viability of the area in which the proposed development or redevelopment is located.
- 2.9.1.7. Economically-related Uses-
A change in or loss of existing or previously established uses constituting an economic impact on the portion of the community in which the project is located.
- 2.9.2. The City will further develop, adopt, and administer the criteria referenced in Policy 2.9.1 into the Land Development Code as it pertains to the compatibility analysis.



GOAL 3 – *Conservation and Preservation*- Continue to protect natural resources and systems throughout the City and ensure that these resources are successfully integrated into the urban environment through land development regulations, management programs, and coordination with future land use intensities and categories.

OBJECTIVE 3.1:

Restrict permanent population densities in the Coastal Storm Areas as identified in Objective 1.1 of the Coastal Management Element in order to help facilitate a more disaster-resilient community.

Policies:

- 3.1.1. Population densities in the Coastal Storm Areas are restricted to the maximum density allowed by the City's Future Land Use Map Categories.
- 3.1.2. The City shall deny an amendment to its Future Land Use Map within the coastal storm area which results in an increase of residential density or intensity.
- 3.1.3. The City shall require development agreements for any new or redeveloped transient uses located within the coastal storm area which requires compliance with Pinellas County and City hurricane evacuation plans and procedures as referenced in Chapter 103-15 of the Land Development Code and Section 3.1.7 of the Coastal Management Element.
- 3.1.4. The City will continue to coordinate with the Tampa Bay Regional Planning Council and Pinellas County to meet the regional objectives for evacuation of permanent populations as well as other emergency concerns.
- 3.1.5. The City shall implement the goals, objectives, and policies of the Coastal Management Element.

OBJECTIVE 3.2:

Protect Dunedin's unique natural and historic resources from unsuitable development.

Policies:

- 3.2.1. Significant hydrological and topographical features shall be preserved.
- 3.2.2. Dunedin's heritage shall be promoted by the identification, designation, and preservation of archeological and historical sites and buildings.
- 3.2.3. Historical and archeological sites on publicly-owned land shall be protected from further development, except in extreme cases of overriding public interest.
- 3.2.4. The City shall continue to promote Chapter 111 of the Land Development Code, which provides guidelines and incentives for the preservation of historically significant sites and structures.

- 3.2.5. The City will encourage adaptive reuse in accordance with Chapter 111 of the Land Development Code.

OBJECTIVE 3.3:

Coordinate land use with natural resources such as topography, geology, soils, wetlands, and floodplains, and the availability of facilities and services to ensure that the level of service standards set forth in the adopted elements are not exceeded.

Policies:

- 3.3.1. During the development review process, natural resources shall be protected before development occurs.
- 3.3.2. During the development review process, aquifer recharge characteristics shall be examined and any negative impacts caused by the development shall necessitate either denial of the project, or modification of the project to alleviate potentially negative impacts.
- 3.3.3. During the development review process, soil capability analyses for stability, permeability, and flood hazards shall be performed, and any developments or redevelopments posing serious threats to soil integrity or are, in turn, threatened by inadequate soil stability, permeability, or flooding shall be required to be modified or denied.
- 3.3.4. Wetlands identified in Map 4-1 of the Comprehensive Plan Appendix will be protected. Potable water wellfields shall be protected by restricting them to preservation or passive recreational use.
- 3.3.5. All development and/or redevelopment activities in floodways and floodplains shall be controlled through consistent application of local, State, and Federal regulations, mitigation and management plans, and the permitting process.
- 3.3.6. New development or redevelopment in the Coastal High Hazard Area shall be permitted consistent with FEMA and City guidelines.



[Image 1-11] Hammock Park



[Image 1-12] Weaver Park

OBJECTIVE 3.4:

Enforce the minimum open space/tree protection standards of the Land Development Code in order to promote the preservation of existing tree canopies, the expansion of tree canopies, and the overall quality of development within the City for all development or redevelopment initiatives.

Policies:

- 3.4.1 All new development or redevelopment of property within the City of Dunedin shall meet the landscape requirements of the Land Development Code.
- 3.4.2 The City shall encourage the use of native plants in landscaped areas, the planting of xeriscaping, and protection of natural features on development sites.

OBJECTIVE 3.5:

Support and retain working waterfronts as much as possible.

Policies:

- 3.5.1 The City will continue to utilize the Resort and Coastal High Hazard Area-Resort Future Land Use Map Category to identify working waterfronts enterprises.
- 3.5.2 The City will assist the State Agencies in the management of Honeymoon Island and Caladesi Island.
- 3.5.3 The City will continue the operation of the City Marina.



OBJECTIVE 3.6:

Work towards decreasing energy consumption, using more renewable energy, and reducing the impact of the built environment on the natural environment.

Policies:

- 3.6.1 The City will continue to maintain the Green City Certification from the Florida Green Builder's Coalition (FGBC) or an equivalent system.
- 3.6.2. Construction and major additions of any City-owned or sponsored new building(s) and affordable housing units shall be certified by the Florida Green Building Coalition (FGBC) or the US Green Building Council (USGBC) as described in Land Development Code Section 105-28.3.3 Green Building Standards or equivalent certifying authority similar to the FGBC or the USGBC, and shall function as a net-zero energy building through the use of solar and/or innovative building techniques.
- 3.6.3. The City will continue to promote sustainable development by creating incentives for compact, mixed-use development patterns; encouraging solar and other alternative energy sources; promoting alternative means of transportation to improve community health and reduce air pollution; protecting trees that absorb greenhouse gases and reduce stormwater runoff and pollutants; and encouraging water-efficient landscaping and protecting water resources by enforcing Section 105-28.4 of the Land Development Code.
- 3.6.4. The City will continue to implement the objectives and policies referenced in the Conservation Element as to reducing energy consumption, using more renewable energy, and reducing the impact of the built environment on the natural environment.



[Image 1-14] Hammock Park



[Image 1-15]
Dunedin Marina

FUTURE LAND USE



GOAL 4 – *Redevelopment and Infill*- Encourage redevelopment and infill development to discourage urban sprawl.

OBJECTIVE 4.1:

Discourage the proliferation of urban sprawl.

Policies:

- 4.1.1. Mixed-use development projects will be encouraged in designated Activity Centers or in the Community Redevelopment Centers to concentrate development and discourage urban sprawl.
- 4.1.2. New strip commercial development (i.e. freestanding, non-shared parking and separate access) shall be discouraged, and the development of shopping centers with several attached businesses, common parking and access will be promoted.

OBJECTIVE 4.2:

Identify areas of the City that are appropriate for redevelopment as livable communities and require that specific sustainable elements be used.

Policies:

- 4.2.1. The visioning corridors/areas will be the focus for redevelopment as referenced on Map 1-3 of the Comprehensive Plan Appendix.
- 4.2.2. The City will improve the appearance and function of visually prominent or distressed corridors in the City as identified on Map 1-3 of the Comprehensive Plan Appendix.
- 4.2.3. The City will build active, attractive communities that are designed at a human scale and encourage walking, bicycling, ridesharing, and use of mass transit.
- 4.2.4. The City will encourage mixed-use development that includes a combination of compatible land uses having functional interrelationships and aesthetic features.
- 4.2.5. Where appropriate, development shall provide a sense of pedestrian scale along the street frontage.
- 4.2.6. The City will promote a variety of transportation modes such as walking, bicycling, ride sharing, and mass transit to increase transportation choices and decrease dependence on the single-occupancy automobile.
- 4.2.7. The City will design and construct pedestrian-oriented streets to include continuous tree-lined sidewalks buffered from traffic by on-street parking and/or landscaping and that include pedestrian amenities such as benches, trash receptacles, bus shelters and lighting.

- 4.2.8. The City will facilitate, when applicable, public-private partnerships, investment and/or new development/redevelopment that will improve public infrastructure systems to increase the City's tax base, support redevelopment and growth, preserve neighborhood character, and encourage community engagement.
- 4.2.9. The City will invest in the built environment to improve the quality of life and attract private investment. The built environment includes buildings, streets, sidewalks, parking, water and sewer pipes, stormwater/drainage systems, green infrastructure (such as parks, trails and landscaping), and placemaking (such as urban design, public art, gateways, or creative signage).
- 4.2.10. Land use decisions in Dunedin shall support the expansion of economic opportunity, the creation of jobs and training opportunities as well as the maintenance of existing industries through establishment of activity centers and redevelopment areas and by coordination with economic development organizations and agencies.

OBJECTIVE 4.3:

Devote effort to redevelopment and infill of existing commercial and residential areas that will provide alternatives to sprawl.

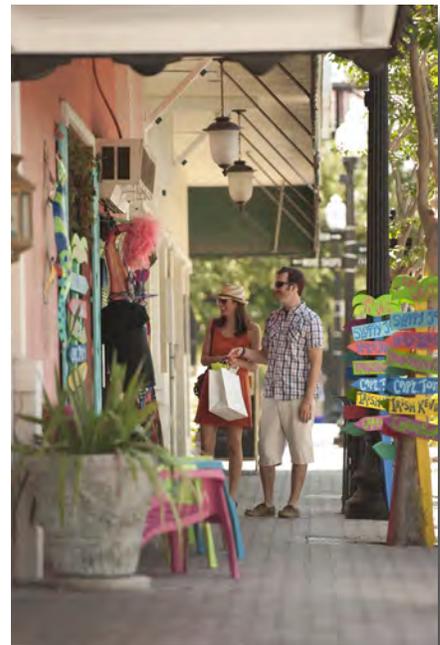
Policies:

- 4.3.1. The City will facilitate and support incremental infill and redevelopment by the way of urban design standards, compact infill development, pedestrian-oriented site configurations and building designs, and capital improvement project proposals to address safety and connectivity issues whenever feasible.
- 4.3.2. The City will identify and promote infill development in established or built-out neighborhoods, often using vacant land or rehabilitating existing properties, to encourage many benefits, including increased property values for residents and businesses, and economic stabilization of distressed neighborhoods.
- 4.3.3. On a continuing basis, the Land Development Code and the site plan approval process shall be utilized in promoting infill development and/or planned developments that are compatible with its surroundings.
- 4.3.4. The City will encourage land use conversions on economically underutilized parcels and corridors, and promote redevelopment activities in these areas.
- 4.3.5. Through economic development programs, the City will facilitate the development of abandoned, idled, or underutilized properties and buildings, bringing them back into productive use.

- 4.3.6. The City will continue to implement redevelopment and revitalization strategies and incentives for private reinvestment in underutilized residential and/or commercial areas where adequate infrastructure exists to support redevelopment.
- 4.3.7. The City will pursue development incentives, funding, public-private partnerships, joint/shared facilities, environmental remediation and property consolidation of small and/or fragmented lots for redevelopment through a development agreement.
- 4.3.8. The City will continue to support and implement the Community Redevelopment Agency (CRA) Downtown Master Plan and other redevelopment plans.
- 4.3.9. The City will continue to support the tax increment financing program and redevelopment efforts of the Downtown area through activities of the Economic Development Department and actions of the City Commission as the Community Redevelopment Agency.



[Image 1-16] Downtown Dunedin-
Main Street and Broadway



[Image 1-17] Downtown Dunedin-
Storefront



2

C H A P T E R T W O

TRANSPORTATION ELEMENT

TRANSPORTATION

Transportation

Historically, Dunedin has focused its transportation planning efforts on automobile movement and roadway capacity. In recent decades, the City has shifted its focus from automobile-centered transportation to multimodal transportation. Other partnering agencies like the Florida Department of Transportation (FDOT) and Forward Pinellas (MPO) have implemented efforts to provide multimodal transportation throughout the State and County within their respective long range plans. One of the primary implementation programs embraced by FDOT and Forward Pinellas is Complete Streets.

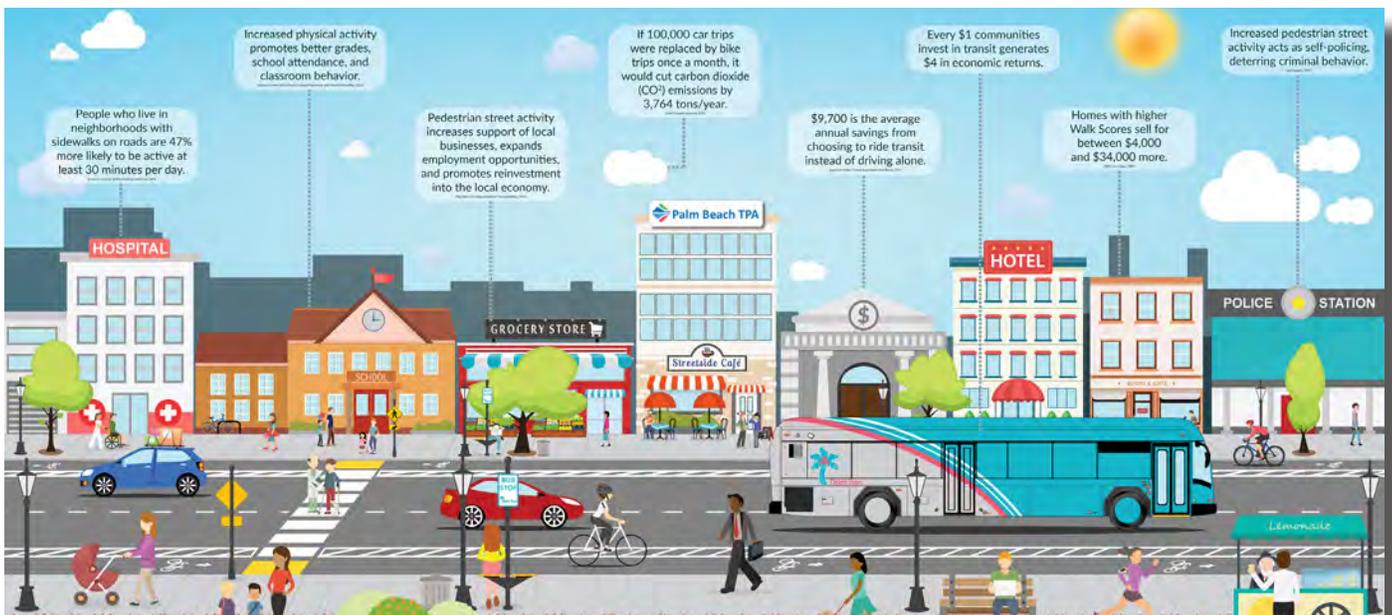
What is Complete Streets?

Complete Streets is a concept that provides safe, convenient access for users of all modes of transportation, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. Streets are designed and operated to enable safe access for all users to:

- Allow pedestrians and bicyclists easy access to cross the streets;
- Allow citizens to walk or bicycle to work;
- Allow transit to operate on time and provide safe access for pedestrians to walk to and from stops/stations; and
- Reduce traffic congestion by offering transportation alternatives to driving.

Complete Streets not only provides safe access for all users, it also:

- Promotes healthy communities: With better access to parks, trails and businesses from residential neighborhoods, people are more likely to choose alternate modes of transportation besides automobiles.



[Image 2-1] Complete Streets
Source: Palm Beach TPA



As a result, according to Image 2-1, people are 47 percent more likely to be physically active for at least 30 minutes per day due to the availability of sidewalks in the neighborhood thereby reducing carbon dioxide emissions to improve air quality.

- Encourage economic development: Based on Image 2-1, pedestrian street activity increases the support of local businesses, expands employment opportunities, and promotes reinvestment into the local economy.

Vision for Complete Streets:

Through the creation and adoption of a multimodal transportation plan, continuous partnership with FDOT and MPO, and adoption of the Complete Streets Policy, the City of Dunedin will be able to execute its vision for better multimodal transportation facilities on existing roadways and corridors.

Complete Streets Policy:

Smart Growth America: The Elements of a Complete Streets Policy provide a foundational framework for the development of the City of Dunedin's Complete Streets Policy. This policy will be included in the City's Multimodal Transportation Plan. The following are the elements that will be considered within the policy:

- Vision and intent - Includes an equitable vision for how and why the community wants to complete its streets. Specifies need to create complete, connected, network and specifies at least four modes, two of which must be biking or walking.
- Diverse Users - Benefits all users equitably, particularly vulnerable users, and the most underinvested and underserved communities.
- Commitment in all projects and phases - Applies to new, retrofit/reconstruction, maintenance, and ongoing projects.
- Clear, accountable expectations - Makes any exceptions specific and sets a clear procedure that requires high-level approval and public notice prior to exceptions being granted.
- Jurisdiction - Requires interagency coordination between government departments and partner agencies on Complete Streets.
- Design - Directs the use of the latest and best design criteria and guidelines and sets a time frame for their implementation.
- Land use and context sensitivity - Considers the surrounding community's current and expected land use and transportation needs.
- Performance Measures - Establishes performance standards that are specific, equitable, and available to the public.
- Project selection criteria - Provides specific criteria to encourage funding prioritization for Complete Streets implementation.
- Implementation steps - Includes specific next steps for implementation of the policy.



[Image 2-2] Main Street Downtown



[Image 2-3] Pinellas Trail



[Image 2-4] Trail Town Designation

Striving for Complete Streets:

As Dunedin continues to strive for a multimodal transportation network, the City has worked on a few initiatives that set forth a foundation for the creation and modification of the Comprehensive Plan goals, objectives, and policies.

2017 Community Visioning Report Update:

As a result of the visioning process, some of the transportation-related improvements that the citizens would like to see in the study areas are:

- To provide better infrastructure for pedestrian and bicycle access and safety;
- To enhance the appearance of the corridors and spur economic development with streetscape improvements; and
- To enhance traffic flow and reduce congestion by providing efficient and safer improvements to the roadway.

Based on community feedback from the visioning process, the recommendations that were made in the report align with the Complete Streets principles to spur redevelopment and revitalization for the vision corridors and areas.

Trail Town Designation:

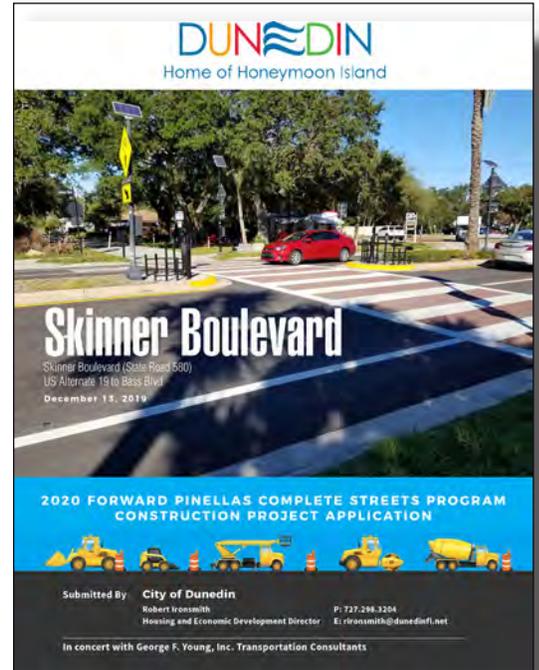
Running through the heart of the City, the Pinellas Trail was once a railroad corridor that delivered goods throughout the area. Presently, the Pinellas Trail is utilized as a vital non-motorized corridor whereby bicyclists and pedestrians are able to travel to key destinations of the City, encouraging economic growth and revitalization. Due to the Trail's importance, in the beginning of 2018, the City was given the first "Trail Town" designation by the Office of Greenways and Trails of the Florida Department of Environmental Protection. This designation emphasizes the importance of having transportation options and reinforces the fundamental principles of Complete Streets.

Skinner Boulevard Complete Streets:

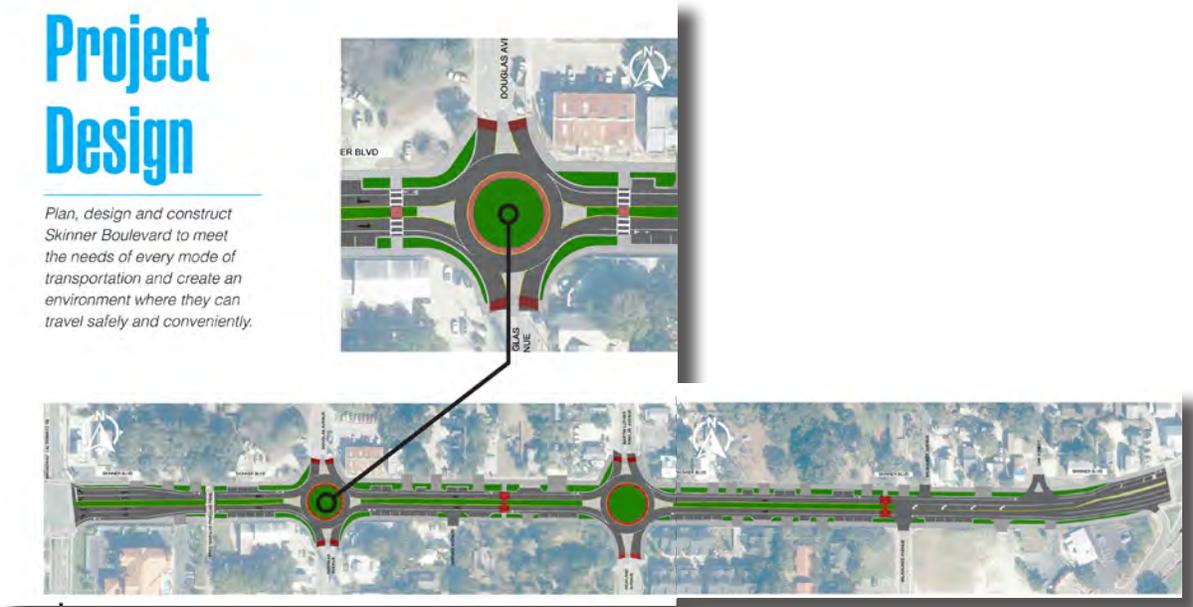
Due to its location within the Downtown Community Redevelopment District and designation as a part of the SR 580 corridor, Skinner Boulevard is a primary candidate for being the City's first Complete Streets project. This half-mile portion of the corridor currently divides the redevelopment district. The City sees this corridor as an opportunity to continue the district's revitalization efforts by planning for infrastructure improvements along the roadway.

Some of the improvements will include speed limit reductions to accommodate other modes of transportation such as wider sidewalks, bicycle facilities, and improved transit stops. Additionally, roundabouts are proposed for two intersections to improve circulation of traffic and safety for bicyclists and pedestrians.

The planning phase of the project was completed in early 2019 and the City is currently working with a consultant to complete the design phase of the project for future construction.



[Image 2-5] Skinner Boulevard Complete Streets Grant Application



[Image 2-6] Skinner Boulevard Complete Streets Project Design



Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- Florida Department of Transportation (FDOT)
- Florida Department of Environmental Protection (FDEP)
- Forward Pinellas (MPO)
- Pinellas Suncoast Transit Authority (PSTA)
- Pinellas County School Board (PCSB)
- Tampa Bay Area Regional Transit Authority (TBARTA)
- Tampa Bay Regional Planning Council (TBRPC)
- Pinellas County

Plans, Programs/Initiatives:

- AARP Livable Communities
- FDOT 2060 Florida Transportation Plan
- FDOT Five-Year Work Program
- FDOT Complete Streets Implementation
- Florida State Implementation Plan (SIP)
- Forward Pinellas Countywide Plan (Countywide Rules, Countywide Plan Map and Countywide Strategies)
- Forward Pinellas 2045 Long Range Transportation Plan (LRTP)
- Forward Pinellas Congestion Management Process (CMP) Policies and Procedures Manual
- Forward Pinellas Complete Streets Program
- Forward Pinellas Transportation Improvement Program (TIP)
- Forward Pinellas Bicycle Pedestrian Master Plan Facilities Element
- Forward Pinellas Countywide Trends & Conditions Report
- Pinellas County Transportation Disadvantaged Program
- Pinellas County Transportation Management Initiatives (TMI)
- PSTA Transit Development Plan
- City of Dunedin Citywide Multimodal Transportation Master Plan
- City of Dunedin Municipal Business Plan and Capital Improvements Plan
- City of Dunedin Land Development Code
- City of Dunedin Code of Ordinances
- City of Dunedin 2017 Visioning Report Update
- City of Dunedin Community Redevelopment Agency Downtown Master Plan 2033
- City of Dunedin Bicycle and Pedestrian Master Plan
- City of Dunedin Douglas Avenue Corridor Study
- City of Dunedin Patricia Avenue Corridor Study
- City of Dunedin Causeway Corridor Report
- City of Dunedin State Road 580 Corridor Study



Related Maps:

- Map 2-1: Functional Classifications
- Map 2-2: Jurisdictions
- Map 2-3: Existing and Proposed Bicycle and Pedestrian Facilities
- Map 2-4: Golf Cart Routes/Crossings
- Map 2-5: Level of Service
- Map 2-6: PSTA Network (Routes and Facilities)



[Image 2-7] Dunedin Causeway Bridge

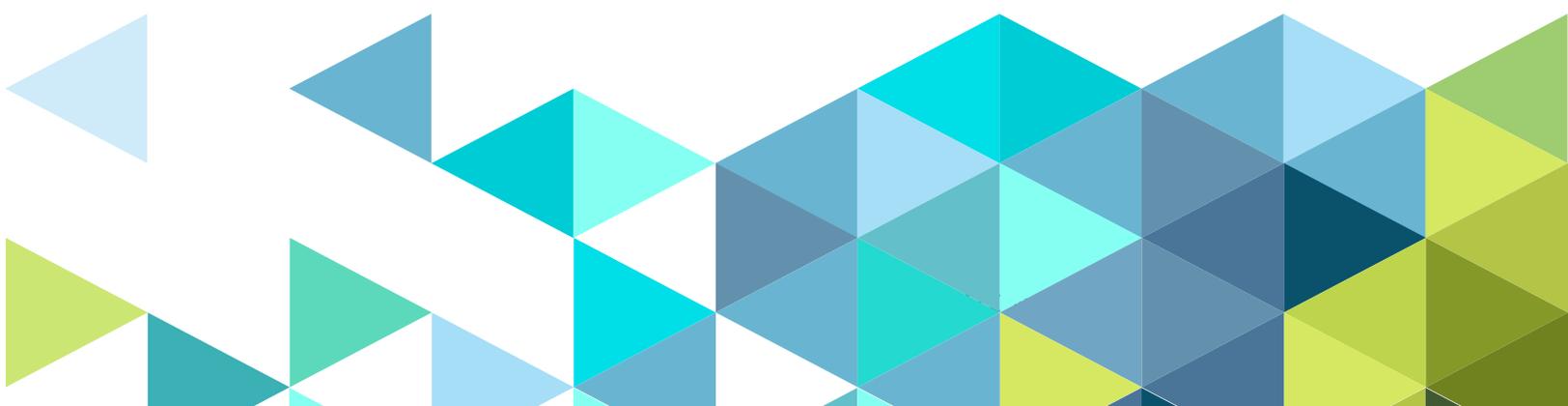
GOAL 1 – Provide a safe and efficient transportation system that serves and enhances the quality of life for Dunedin citizens by implementing the elements of the Complete Streets policy.

OBJECTIVE 1.1:

Maintain and improve the existing multimodal transportation system and implement programs that will facilitate a safe, convenient, sustainable, and efficient network.

Policies:

- 1.1.1. The City will conduct an inventory on the conditions of all local roads, alleys, sidewalks, and bike paths/trails to evaluate and prioritize the order of repair and maintenance.
- 1.1.2. The City will provide adequate funding for the maintenance and repair of its existing transportation infrastructure, including brick streets in historical neighborhoods.
- 1.1.3. The City shall adopt the Level-of-Service (LOS) target for all County, State, and local roads to operate at LOS C for the daily average and D for peak hour (7am-9am & 4pm-6pm) with a volume-to-capacity (v/c) ratio of less than 0.9 or better.
- 1.1.4. The City will identify improvements to alleviate LOS deficiencies in the Capital Improvement Element of the Comprehensive Plan and the Municipal Business Plan and Capital Improvement Plan.
- 1.1.5. The City will set a higher priority for LOS E and F facilities under the Municipal Business Plan and Capital Improvement Plan to alleviate the substandard level of service conditions to the greatest extent possible and advocate support to the proposed improvements.
- 1.1.6. The City shall implement a mobility management system through the application of the Transportation Element policies and the development review process. Policies pertaining to the application of the mobility management system are listed below:
 - 1.1.6.1. All development projects generating new trips shall be subject to payment of a multimodal impact fee.
 - 1.1.6.2. Development projects that generate between 51 and 300 new peak hour trips on deficient roads shall be classified as tier 1 and require to submit a transportation management plan (TMP) designed to address their impacts while increasing mobility and reducing the demand for single occupant vehicle travel.
 - 1.1.6.3. Development projects that generate more than 300 peak hour trips on deficient roads shall be classified as tier 2 and require to conduct a traffic study and submit a report in addition to a TMP based on the findings.
 - 1.1.6.4. Multimodal impact fee assessments may be applied as credit toward the cost of a TMP.

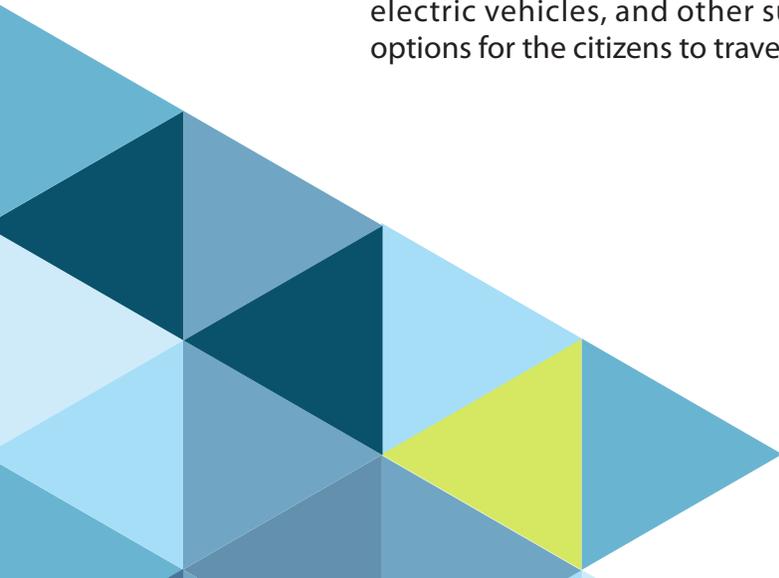
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- 1.1.6.5. A traffic study and/or TMP for a development project not impacting a deficient road corridor shall be required if necessary to address the impact of additional trips generated by the project on the surrounding traffic circulation system.
 - 1.1.6.6. Deficient roads shall include those operating at peak hour level of service (LOS) E and F and/or volume-to-capacity (v/c) ratio of 0.9 or greater without mitigating improvement scheduled for construction within three years.
 - 1.1.6.7. Multimodal impact fee revenue shall be utilized to fund multimodal improvements to local, County, or State facilities that are consistent with the Comprehensive Plan, the City Multimodal Transportation Master Plan as well as the Long-Range Transportation Plan maintained by Forward Pinellas.
 - 1.1.7. The City will promote and encourage Transportation Demand Management strategies to improve capacity on roadways.
 - 1.1.8. The City will promote and, require as appropriate, shared connections and driveways onto roads during the site plan review process.
 - 1.1.9. The City shall discourage Future Land Use Map amendments that would increase the number of vehicle trips generated on deficient roadways.
 - 1.1.10. The City will encourage and support the use of sustainable materials for the construction and maintenance of transportation improvements.
 - 1.1.11. The City shall authorize the Pinellas County Sheriff's Office to continue to enforce all Federal, State, and local traffic laws including the golf cart ordinance and truck route ordinance.
 - 1.1.12. The City will continue to review the annual citizen survey to gather traffic-related concerns and address specific issues regarding neighborhood streets and trails.
 - 1.1.13. The City will develop evaluation criteria to justify the need for safety improvements for all local roads, such as lighting, pedestrian crossings, and traffic calming devices and prioritize the improvements into the Capital Improvement Plan.
- 

OBJECTIVE 1.2:

Promote alternative transportation strategies through local regulations and land uses.

Policies:

- 1.2.1 The City shall promote and encourage the use of bicycle and pedestrian facilities through the implementation of the objectives and policies referenced in the Transportation Element as well as the strategies and actions of the City's Multimodal Transportation Master Plan.
- 1.2.2. The City shall encourage and support the improvement and expansion of interconnected trails with adjacent communities and jurisdictions, including the Pinellas Trail, to increase usage.
- 1.2.3. The City shall encourage and support PSTA and other transit partners (Jolley Trolley, Tiki Rides or similar ride services, etc.) on the improvement and expansion of public transit, including the use of connector buses, downtown circulators, and shuttle services from parking lots within the City and to adjacent communities by working with neighboring jurisdictions to improve facilities and increase usage.
- 1.2.4. The City will repair existing bicycle or sidewalk facilities and construct new bicycle or sidewalk facilities consistent with the Bicycle and Pedestrian Master Plan.
- 1.2.5. The City will assist in funding enhanced public transit amenities.
- 1.2.6. The City will implement transit-friendly design standards in the Land Development Code to promote safe passage of transit of transit users from bus stops to final destinations.
- 1.2.7. During the design review process, the City shall ensure that proposed projects along planned multimodal corridors provide adequate right-of-way for improvements or construction to existing and future transportation facilities.
- 1.2.8. The City will continue to utilize Multimodal Impact Fee revenue to fund multimodal improvements to local, County, or State facilities that are consistent with the Comprehensive Plan as well as the Forward Pinellas 2045 Long Range Transportation Plan.
- 1.2.9. The City will forward requests for access to County or State maintained roadways to each respective agency for comment concerning their respective plans and policies.
- 1.2.10. The City will continue to explore and enhance other modes of transportation including golf carts, autonomous vehicles (AV), water-borne transportation, electric vehicles, and other sustainable innovative vehicles to provide more options for the citizens to travel throughout the City.



- 1.2.11. The City will plan and prioritize infrastructure improvements for other modes of transportation listed in Policy 1.2.9. by providing the following:
 - 1.2.11.1. Charging stations at City facilities;
 - 1.2.11.2. Expanded trails, paths, and crossings to accommodate such modes of transportation at appropriate locations;
 - 1.2.11.3. Continued ferry service; improving and expanding its service routes;
 - 1.2.11.4. Maintenance and improvement of the marina and boat ramps/launching area.
- 1.2.12. The City will continue to implement the AARP Livable Communities initiative to provide options for transportation for all ages and abilities.
- 1.2.13. The City will examine the benefits and potential impacts of ride service vendors, including micromobility devices, ride share services, and bike share services, to ensure that the anticipated impacts are properly addressed and mitigated as well as providing appropriate infrastructure /facilities for such services.
- 1.2.14. The City will continue to improve golf cart accessibility throughout the City by working with Pinellas County and FDOT to ensure proper crossings on County and State roads.

OBJECTIVE 1.3:

Support and coordinate with State and County agencies to plan and implement a safe multimodal transportation system throughout the City.

Policies:

- 1.3.1 The City will continue to participate in State and regional plans and programs to ensure consistency with the City’s Comprehensive Plan.
- 1.3.2. The City will continue to coordinate with State and County agencies in joint planning studies, sharing of Countywide data and analysis, and implementation of projects.
- 1.3.3. The City will support and participate in future planning efforts that would provide for enhanced regional transit, such as bus rapid transit (BRT), light rail and transit-oriented development (TOD) standards and plans.
- 1.3.4. The City will support efforts that plan for emerging transportation technologies, including autonomous vehicles and related infrastructure.



[Image 2-8] Complete Streets Concept
Source: Skinner Blvd Complete Streets



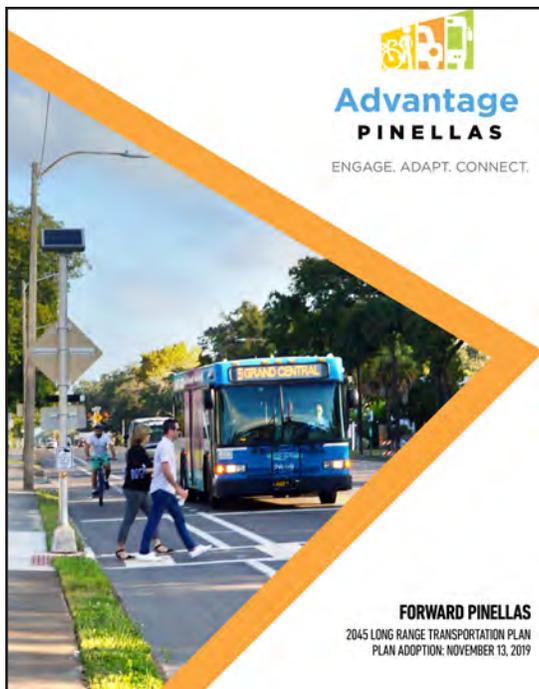
[Image 2-9] Clearwater Ferry

- 1.3.5. The City will support State and regional freight strategic planning efforts to identify alternative routes for areas that have potential conflicts with bicycle and pedestrian paths to ensure safety and consistency with the community vision.
- 1.3.6. The City will continue to collaborate with Forward Pinellas and PSTA to identify and monitor the unmet transportation disadvantaged needs within the City and promote the Pinellas County Transportation Disadvantaged Program.
- 1.3.7. The City will continue to work with Pinellas County to evaluate the transportation system's ability to provide safe evacuation routes during an emergency event and make adjustments to existing and future routes as needed.
- 1.3.8. The City will continue to engage with County and State agencies during the site plan review process of development projects to ensure that site access management standards are met.
- 1.3.9. The City will continue to participate in Forward Pinellas's transportation related committees and working groups, such as the Technical Coordinating Committee (TCC), to ensure that City projects and priorities are represented on a regional level, and collaborate with partnering agencies to gain and share knowledge.
- 1.3.10. The City will support efforts planned by other transportation agencies to alleviate congestion.
- 1.3.11. The City will continue to support, promote, and provide outreach programs to reinforce traffic rules, as well as bicycle and pedestrian safety protocol.

OBJECTIVE 1.4: Implement and update the Citywide Multimodal Transportation Master Plan.

Policies:

- 1.4.1. The City will provide adequate staffing and funding to implement the Citywide Multimodal Transportation Master Plan in full coordination with adjacent cities and agencies.
- 1.4.2. The City will monitor and update the Citywide Multimodal Transportation Master Plan to be consistent with the Comprehensive Plans and Capital Improvement Plans of the City, other local governments, FDOT, PSTA, PCSB, TBARTA, TBRPC, and Forward Pinellas.
- 1.4.3. The City will implement and maintain effective public information procedures to plan transportation methods responsive to the public need.
- 1.4.4. The City will monitor and, if appropriate, update the Citywide Multimodal Transportation Master Plan to incorporate relevant goals from State and County transportation plans.



[Image 2-10] Pinellas County MPO L RTP
Source: Forward Pinellas



[Image 2-11] FDOT 2060 Florida Transportation Plan
Source: FDOT



3

C H A P T E R T H R E E

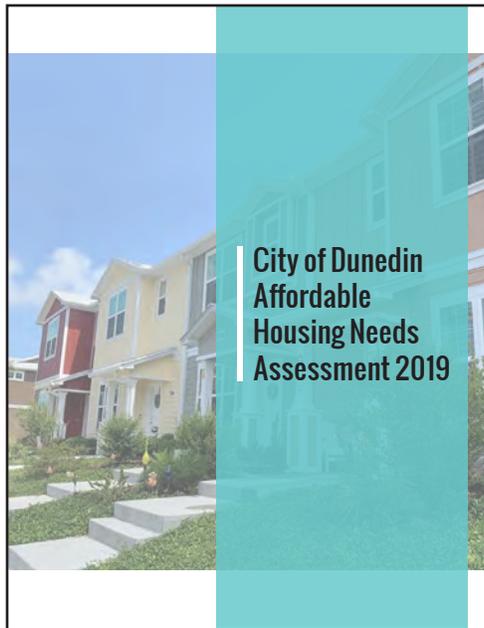
HOUSING ELEMENT



Housing Element

The most basic human necessities are safety, shelter, and protection from the elements. This is why affordable, safe, and decent housing is vital for the success and well-being of people and communities. The U.S. Department of Housing and Urban Development (HUD) defines affordable housing as, “In general, housing for which the occupant(s) is/are paying no more than 30 percent of his or her income for gross housing costs, including utilities.” However, housing alone does not allow for communities to thrive. The ability for people in communities to access jobs, education, shops, grocery stores, healthcare, and parks via various modes of transportation is a significant determinant in quality of life for community residents. Additionally, considerations must be made to address the needs of people multi-generationally, caring for those young and old.

In order to create a community that is desirable for a broad spectrum of people, cities must provide its inhabitants an environment in which they can flourish. Careful consideration must be made to the planning, design, creation, preservation, and rehabilitation of housing. This Element works hand-in-hand with the Dunedin Affordable Housing Needs Assessment, created by Langton Consulting in partnership with the City of Dunedin, to outline current conditions, anticipate community needs, and make recommendations for affordable housing opportunities.



[Image 3-1] Dunedin Affordable Housing Needs Assessment 2019

Roadmap to Affordable Housing

In March 2019, the City of Dunedin contracted with Langton Consulting, a Florida-based public affairs consulting firm, to conduct an analysis on the current housing market and produce the Dunedin Affordable Housing Needs Assessment 2019. Langton was subsequently contracted to develop the *Strive for 2035: Dunedin Comprehensive Plan – Housing Element* as there were synergies between this project and the Dunedin Affordable Housing Needs Assessment 2019 document.

Current Housing Conditions

Substandard Housing:

HUD defines *standard condition* for housing as both meeting HUD Housing Quality Standards (HQS) and meeting all State and local codes. *Substandard condition* is defined as *in poor condition and [it] is both structurally and financially unfeasible to rehabilitate*. In the same vein, HUD also outlines four common housing problems, including:

1. Lacking complete plumbing facilities;
2. Lacking complete kitchen facilities;
3. **More than one person per room, and as discussed in the next section, and;**
4. Cost burden greater than 30 percent.

The analysis revealed that Dunedin residents experienced overcrowding at 0.8 percent of occupied units, lacking complete plumbing facilities at 0.9 percent of occupied units, and lacking kitchen facilities at 2.2 percent of occupied units. In comparison, these ratios are lower than what is found in Pinellas County and the State of Florida.



[Image 3-2] Substandard Housing

Housing Stock:

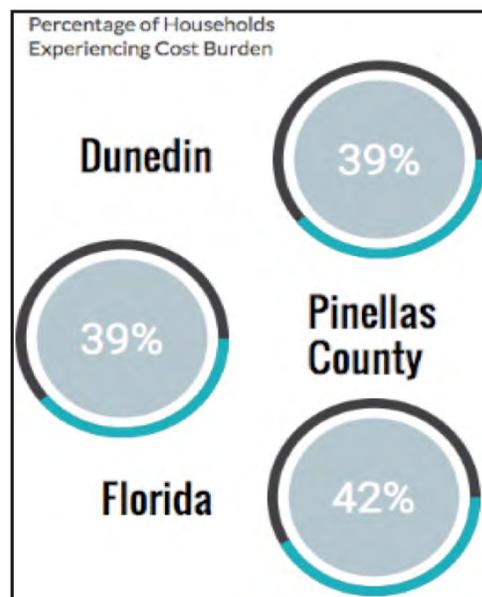
According to Shimberg Center for Housing Studies 2013-2017 five-year estimates, 853 or 4.4 percent of the 17,256 occupied households in Dunedin are considered substandard, either having overcrowding (1.01 or more persons per room), using no fuel, or lacking complete plumbing or kitchen facilities. According to Shimberg Center for Housing Studies 2013-2015 five-year estimates, 13,283 or 64.53 percent of houses in Dunedin were built in 1979 or earlier, and only 1,337 or 6.5 percent were built in the year 2000 or later. As for the housing mix, 53 percent of houses are single-family, 38 percent of houses are multifamily, and 9 percent of houses are mobile homes.

Cost Burden:

The U.S. Department of Housing and Urban Development (HUD) considers cost burden one of the four common housing problems. According to HUD, households who are paying more than 30 percent of their income for housing are cost-burdened and households paying more than 50 percent of their income on housing costs are severely cost-burdened. Based on the U.S. Census Bureau 2013-2017 five-year estimates, 38.52 percent of all Dunedin households are cost-burdened. Of these cost-burdened households, 27.66 percent of these are low-income (households earning less than 80 percent of the area median income). These ratios are very similar to what is found in Pinellas County and lower than the State of Florida.



[Image 3-3] Low-Income Household Cost Burden

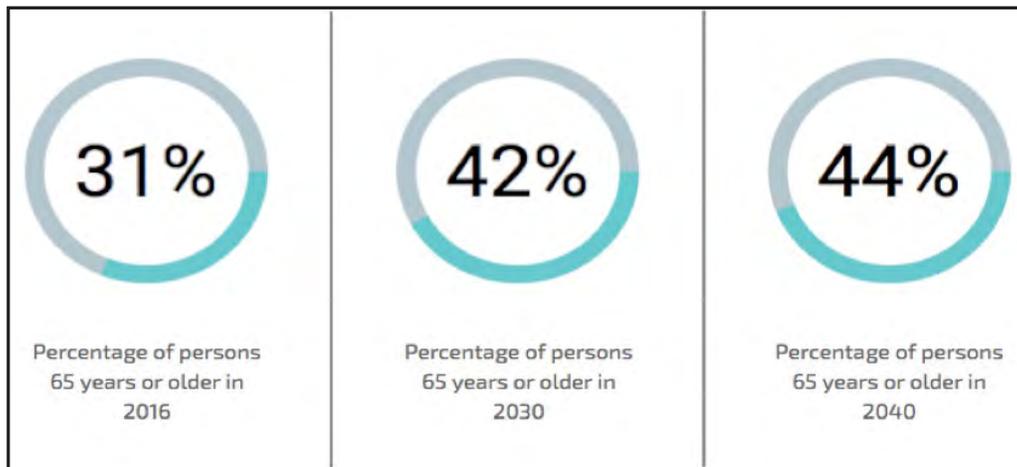


[Image 3-4] Housing Cost Burden

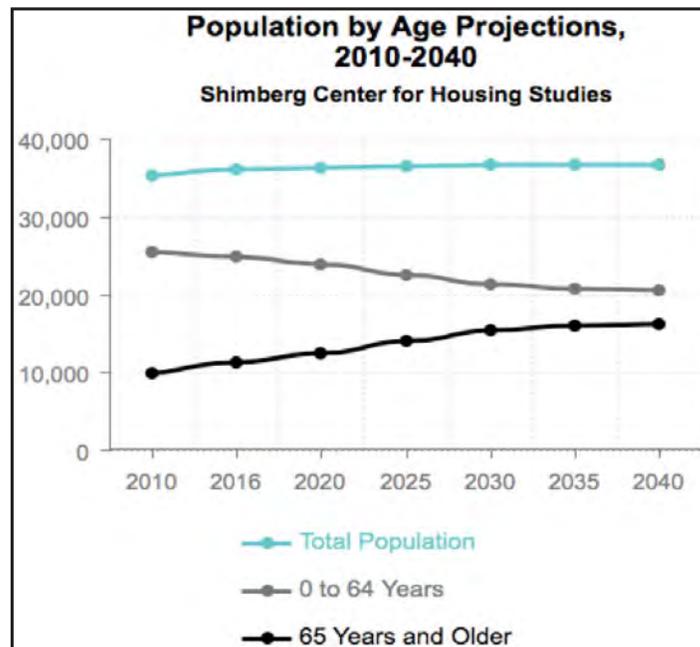
Senior Population:

According to HUD, by 2050 the portion of the population 65 years and older in the United States is expected to double. A similar trend is anticipated for Dunedin with the current population of individuals 65 years and older growing from 11,316 or 31 percent in 2016 to 16,085 or 44 percent by 2040 according to projections by Shimberg Center for Housing Studies.

Of the 11,316 total residents that are 65 years and older, 7,841 are head of household based on 2016 data. Within this subset, 3,358 or 43 percent are considered low-income at 80 percent AMI or less. 1,847 or 24 percent of these households are considered both low-income and cost-burdened.



[Image 3-5] Senior Population Projection



[Image 3-6] Population Projections by Age

Affordable Housing:

The City of Dunedin has in recent years successfully executed the development of several affordable housing communities resulting in the creation of 55 housing units. These include the Shady Grove Townhomes, the Creek Park Townhomes, and Eco Village. Through various Federal, State, and local programs, these units are made to be affordable to eligible low-moderate income households. Similarly, both Shady Grove Townhomes and Eco Village provide a pathway to homeownership for low to moderate-income households. Eco Village, Dunedin's most recent development, has received national recognition for the first affordable, LEED-certified, net-zero energy townhome development.



[Image 3-7] San Christopher Apartments

Senior Housing:

There currently is one affordable housing facility for seniors in the City of Dunedin. Palm Lake Village is a 55 and over community that offers 475 apartments. Housing Choice Vouchers are accepted and Palm Lake and amenities include a swimming pool with ADA compliant access ramps, six laundry facilities; a community center with library, television, pool table and computer center; and access to public transportation is also available. Currently, all units are occupied and there are 173 applicants on the waiting list. The wait time for a unit is approximately 12 to 24 months.

Similarly, for senior persons that desire to age in place and are considered low-income (household income less than 80 percent AMI), Dunedin works in partnership with Pinellas County to provide an Independent Living Program, which offers eligible applicants up to \$7,000 to improve the accessibility of their homes. Additionally, there are several agencies in Pinellas County that offer non-medical assistance to seniors who wish to reside in their own homes.



[Image 3-8] Palm Lake Village

Past Successes

The City of Dunedin is committed to providing affordable housing for residents across all income levels. In recent years, Dunedin has worked with County programs and local housing providers, to develop new, affordable housing communities. Three examples of affordable and/or workforce housing developments include:

- **Shady Grove Townhomes** were constructed by Habitat for Humanity of Pinellas County. The homes constructed by Habitat for Humanity are built by volunteers and the organization makes no profit on the sales. To qualify for a Habitat for Humanity home an applicant must earn an income that is 30 percent-80 percent of the area median income for Pinellas County. The Shady Grove townhomes consist of 19 income-restricted units. With Federal "HOME" funds helping to support the project, the average mortgage payment for owners at Shady Grove townhomes is \$650 per month. Habitat has the first right-of-refusal if someone wants to sell his or her home. If the townhome is sold between zero and five years after purchase, the home can be purchased back at the original price. After five years, the townhome must be bought back at the assessed price.



[Image 3-9] Shady Grove Townhomes

- **Creek Park Townhomes** were built in association with the Bright Community Trust, which is a community land trust (CLT). Community land trusts are community-based, nonprofit organizations, created to ensure long-term housing availability and affordability. The Creek Park Townhomes slightly differ from the traditional model of CLTs because they are rental properties, whereas normally CLTs develop homes for sale. The Creek Park townhomes, located at 1000 Creek Park Drive, range from 1,200 to 2,000 square feet. The project also received State SHIP funds toward construction.

Creek Park Townhomes is a mixed-income community with 11 income-restricted townhomes integrated with market rate housing. The market rate townhomes are used to subsidize the income-restricted units. As transportation costs factor into the affordability of a community, Creek Park Townhomes are accessible to the Pinellas Suncoast Transit Authority (PSTA) bus routes 61, 78, and the Dunedin/Palm Harbor Connector (DPC). Housing Choice Vouchers are accepted, and eligible families are those earning up to 120 percent of the AMI.



[Image 3-10] Creek Park Townhomes

- **Eco Village** is an affordable housing community with some innovative energy-saving features. Once the site of a troubled public housing project that has since been donated by Pinellas County to the City of Dunedin, Eco Village was developed through an RFP process specifically for the construction of affordable, "green" housing.

After build-out, Greenenergy Communities will have 25 townhomes available for purchase. In addition to a solar panel array on every garage, every townhome is equipped with Energy Star® appliances, low-flow showerheads and faucets, heavily insulated walls and roofs, double-pane windows, fiber- cement siding, heat pump water heaters, and small, yet high efficiency air conditioners.

In order to call Eco Village home, residents have to live here as their primary residence. Owners pay \$25 per month to lease the land and cannot resell their unit higher than 3.5 percent of the assessed property value. Currently, there are five townhomes for people with incomes under 80 percent of the area median income, 15 townhomes for people with incomes 80 percent to 120 percent of the area median income, five townhomes for people at any income level, and many more townhomes under construction.



[Image 3-11] Eco Village

Keeping up the momentum to support and encourage the maintenance and development of affordable housing, the City of Dunedin and the Dunedin Community Redevelopment Agency (CRA) will continue working with the City's Planning and Development Department to identify potential affordable housing locations for future projects.





Opportunities to Additional Affordable Housing

Dunedin is a community with many great qualities that are attractive to creating affordable housing opportunities. Residents and visitors alike are drawn to Dunedin due to its ability to provide a great quality of life, a flourishing Downtown district, access to public services, and a location for many outdoor entertainment and recreation options. Recent studies suggest that the City can anticipate a modest growth in population by the year 2040.

With the anticipated increase in the projected population growth on the horizon, it is imperative that the City of Dunedin encourage and support furthering affordable housing opportunities for current and future residents. In order to do so, the City must consider new initiatives and expand on past successes, as well as create a new dedicated funding stream to achieve this goal.

Some examples of ways the City can further affordable housing include:

- Consider utilizing code enforcement fines towards building a reserve for contributing to affordable housing projects;
- Consider waiving or payment-in-lieu of impact fees to contribute toward a reserve for affordable housing projects;
- Consider utilizing surplus property sales as seed money toward affordable housing projects and initiatives;
- Strengthen relationships with current affordable housing partners, and seek out new potential partners locally and Statewide;
- Eliminate barriers caused by local regulatory policies that hinder the expansion of affordable housing opportunities; and
- Create incentive options for affordable housing developers to encourage the creation of new single-family and multifamily affordable housing.



Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- Dunedin Housing Authority
- Pinellas County Community Development Department
- Pinellas County Housing Finance Authority
- Pinellas County Housing Authority
- Pinellas County Human Services
- Pinellas County Homeless Leadership Board
- Habitat for Humanity of Pinellas County
- Bright Community Trust
- American Association of Retired Persons (AARP)
- University of South Florida (USF)

Plans, Programs/Initiatives:

- City of Dunedin Affordable Housing Needs Assessment 2019
- City of Dunedin Municipal Business Plan and Capital Improvements Plan
- City of Dunedin Land Development Code
- City of Dunedin Code of Ordinances
- Down Payment Assistance Program – Administered by the Pinellas County Community Development Department
- Home Improvement No Interest Loan Program – Administered by the Pinellas County Community Development Department
- Independent Living Program – Administered by the Pinellas County Community Development Department
- First-time Homebuyer Program – Administered by the Pinellas County Housing Finance Authority
- Services for Seniors – Administered by local housing services providers in partnership with Pinellas County Community Development Department
- Public Housing Program – Administered by the Pinellas County Housing Authority
- Housing Choice Voucher Program – Administered by the Pinellas County Housing Authority
- Target Area Homebuyer Programs – Administered by the Pinellas County Housing Finance Authority
- Development of Mixed-Income Rental Housing – Administered by the Pinellas County Housing Finance Authority
- State Housing Initiative Partnership (SHIP) – Administered by the Pinellas County Housing and Community Development Department
- Community Development Block Grant Program (CDBG) – Administered by the Pinellas County Community Development Department
- HOME Investment Partnership Program (HOME) – Administered by the Pinellas County Community Development Department
- Community Housing Trust Fund – Administered by the Pinellas County Community Development Department
- Neighborhood Stabilization Program – Administered by the Pinellas County Community Development Department

Plans, Programs/Initiatives (continued):

- Homelessness Prevention and Rapid Re-Housing Program – Administered by the Pinellas County Human Services Department in collaboration with the Pinellas Rapid Re-Housing Collaborative
- Emergency Solutions Grant Program – Administered by the Pinellas County Community Development Department
- University of South Florida Community Sustainability Partnership Program- Affordable Housing Plan for the City of Dunedin
- AARP Livable Communities

For more information regarding any of the programs listed above, please visit the City of Dunedin's website at www.dunedingov.com/city-departments/economic-development/affordable-housing or see the Available Funding Sources Inventory included in the Appendices section of the 2019 Dunedin Affordable Housing Needs Assessment.



GOAL 1 – *Housing for All*- Ensure the availability of quality, safe, sanitary, and affordable housing to meet the needs of all current and future residents with varied needs, desires, incomes, and ages.

**OBJECTIVE 1.1:
Maintain a balanced mix of all housing types that will allow for residential needs for the existing and future population.**

Policies:

- 1.1.1. The City shall maintain current mixed-use regulations within areas defined by the Land Development Code.
- 1.1.2. The City shall maintain development regulations that support the integrity of the Comprehensive Plan, as well as the quantity, quality, and character of the City’s housing stock.
- 1.1.3. The City shall monitor new housing to determine whether the existing and new housing stock meets the housing needs of seniors and low- to moderate-income families.
- 1.1.4. The City shall encourage, in cooperation with State and County agencies, opportunities for Community Residential Homes where appropriate.



[Image 3-12] Oak Bend Townhomes
Source: SDG Architecture



**GOAL 2 – Affordable Housing Needs Assessment & Inventory/
Programming for Affordable Housing- Ensure adequate
programming for affordable and accessible housing for all
citizens.**

OBJECTIVE 2.1:

**Prepare and update the Dunedin Affordable Housing Needs Assessment
& Available Funding Inventory.**

Policies:

- 2.1.1. The City will review and update the Dunedin Affordable Housing Needs Assessment & Available Funding Inventory every five years to include new and current housing market data and available funding programs to address affordable housing opportunities.

OBJECTIVE 2.2:

**Review affordable housing subsidy options to ensure adequate funding for
affordable housing.**

Policies:

- 2.2.1. The City will establish one dedicated funding source to create or expand affordable housing programs and initiatives.
- 2.2.2. The City will pursue State and Federal funding sources for infrastructure improvements, public facilities, and for the construction of affordable housing.
- 2.2.3. When available, the City will utilize funds to improve neighborhood conditions in very-low to moderate income areas and increase the supply of safe, affordable and sanitary housing for very-low to moderate income persons and elderly persons.
- 2.2.4. The City will review opportunities within the banking community for possible funding sources.
- 2.2.5. The City will explore opportunities for development of a demolition program to create affordable housing.

OBJECTIVE 2.3:

Provide a variety of affordable, accessible housing options for all ages, incomes, and abilities.

Policies:

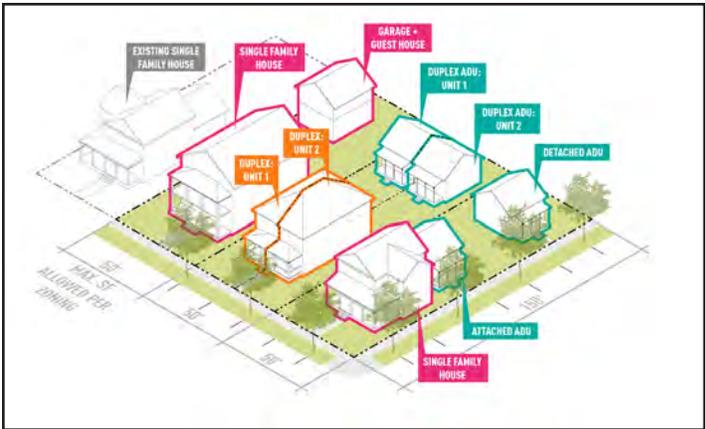
- 2.3.1. The City will continue to anticipate future housing market challenges in order to provide a variety of affordable and accessible housing options that appeal to all ages and abilities and allow Dunedin to remain a well-balanced, thriving inter-generational community.
- 2.3.2. The City will continue to assess the existing public and private housing programs and identify potential ways to further increase access to affordable and standard housing for all citizens according to the Fair Housing Act.
- 2.3.3. The City shall comply with the provisions of the Federal Uniform Relocation Assistance and Real Property Acquisition Act of 1970, Public Law 91-646, when Federally-funded rehabilitation projects displace residents.
- 2.3.4. The City will continue to coordinate with the Dunedin Housing Authority and Pinellas County Housing Authority for the relocation of displaced residents as a result of Federally-funded rehabilitation projects.

OBJECTIVE 2.4:

Continue to support programs for homeless prevention.

Policies:

- 2.4.1. The City will continue to support Federal, State, and local legislation and resolutions that encourage or provide homelessness prevention services, including Resolution 16-13 for the Housing First philosophy.
- 2.4.2. The City will continue to coordinate with Pinellas County and other nonprofit organizations for Homelessness Prevention Programs.



[Image 3-13] Missing Middle Housing Types
Source: Kronberg Urbanists+Architects

OBJECTIVE 2.5:

Continue to monitor the development and location of group homes to insure that these facilities are not concentrated in one area as referenced in Chapter 107 of the Land Development Code.

Policies:

- 2.5.1. Sites for group homes in suitable residential locations shall meet all regulatory requirements per Federal and State regulations.
- 2.5.2. The group home criteria and principles listed in the Land Development Code shall be used to guide the location of group homes licensed by the Agency for Health Care Administration (AHCA) and foster care facilities licensed by the Florida Department of Children and Families.

GOAL 3 – Housing Redevelopment & Rehabilitation- Examine existing Federal, State, and local regulations to address improvements to existing housing market conditions and encourage affordable housing opportunities.

OBJECTIVE 3.1:

Review ordinances, regulations, policies and procedures to assist in providing development tools to encourage affordable housing opportunities.

Policies:

- 3.1.1. The City will explore removing regulatory barriers to the greatest extent possible to facilitate affordable housing opportunities.
- 3.1.2. The City will consider the use of funds from sale of surplus property and a portion of code enforcement proceeds to be directed toward affordable housing programs and/or bank.
- 3.1.3. The City will explore incentives and exemptions to local land development regulations for developers and affordable housing providers with regard to creating affordable housing projects (i.e., Parkland Impact Fee, parking, density bonus, art ordinance, zoning, etc.)
- 3.1.4. The City will continue to allow accessory dwelling units in single-family zoning districts.
- 3.1.5. The City will explore the options of allowing a greater variety of housing types and sizes, including tiny homes, as a possible solution for aging-in-place and affordable housing.
- 3.1.6. The City will develop programs consistent with the City's essentially built-out constraints to maintain existing sites for affordable housing.
- 3.1.7. The City will continue to maintain zoning districts and future land use designations which facilitate affordable housing.

OBJECTIVE 3.2:

Explore creative solutions to address substandard housing.

Policies:

- 3.2.1. The City will utilize code enforcement personnel and implement programs to remove or rehabilitate substandard dwelling conditions and improve the current housing stock.
- 3.2.2. The City will support the establishment of neighborhood associations to consolidate interest in housing maintenance activities and increase civic pride.
- 3.2.3. The City will continue to enforce FEMA regulations for structures located within the floodplain as referenced in Objective 2.1 of the Coastal Management Element.

OBJECTIVE 3.3:

Encourage the use of Universal Design & Net Zero Energy Construction.

Policies:

- 3.3.1. The City will consider opportunities to provide subsidies for universal design and net zero construction standards for new construction, rehabilitation, and redevelopment of existing housing.

OBJECTIVE 3.4:

Continue to preserve historical and architecturally significant housing.

Policies:

- 3.4.1. The City shall preserve historically and architecturally significant housing in Dunedin as identified by the National Register of Historic Places, the Florida Master Site File, Local Historic Resources List, Dunedin Historical Museum and Dunedin Historic Preservation Committee in accordance with all State laws and statutes, and local ordinances.
- 3.4.2. The City shall continue to enforce the Historic Preservation Ordinance.





OBJECTIVE 3.5:

Continue to address Short Term Vacation Rentals based on current housing market conditions.

Policies:

- 3.5.1. The City will continue to establish a best practice and regulatory policy to address Short Term Vacation Rentals and how they affect the local affordable housing market.

OBJECTIVE 3.6:

Provide transportation options for affordable housing projects.

Policies:

- 3.6.1. The City will encourage the location of affordable housing projects near mass transit routes.
- 3.6.2. The City will continue to support public transportation agencies to improve service and amenities for mass transit.
- 3.6.3. The City will continue to enforce the requirements for providing bicycle parking in the Land Development Code.



[Image 3-14] J.O. Douglas House
Source: Dunedin Historical Museum

GOAL 4 – Collaborative Partnerships- Continue to partner, collaborate and support existing Countywide and local programs for affordable housing from intergovernmental and nonprofit agencies to ensure citizens receive appropriate resources for housing needs.

OBJECTIVE 4.1:

Partner with real estate developers and local housing providers.

Policies:

- 4.1.1. The City will continue collaborative and coordination efforts by participating in the Pinellas County Housing Consortium, the Pinellas County Community Development Department, and other affordable housing developers.

OBJECTIVE 4.2:

Continue to promote affordable housing programs through community outreach techniques.

Policies:

- 4.2.1. The City will utilize the City Communications Department, Public Advisory Committee, and Pinellas County Community Development to help communicate resources.
- 4.2.2. The City shall make information available to residents through press releases, the City website, Dunedin TV, and printed information at City locations to heighten public awareness of the affordable housing programs offered by the City, County, and State.
- 4.2.3. The City shall participate in community fairs and workshops to help communicate available resources to residents regarding affordable housing.
- 4.2.4. The City will provide public information to instruct the housing industry, legal professionals, and the general public of fair housing rights and responsibilities.

OBJECTIVE 4.3:

Continue to support affordable housing programs and initiatives from County and non-profit agencies.

Policies:

- 4.3.1. The City will continue to work with Pinellas County and promote its initiatives and programs to help low/moderate income families and investors to rehabilitate housing units in declining neighborhoods.
- 4.3.2. The City will continue to promote and support programs and initiatives from non-profit housing agencies.

GOAL 5 – Economic Development/Workforce Housing Opportunities- Continue to promote economic development programs to support workforce housing opportunities for the community.

OBJECTIVE 5.1:

Support existing economic development programs provided by the State, County, and Local agencies.

Policies:

- 5.1.1. The City will support economic development efforts and programs to increase high-skill, high-wage employment opportunities.

OBJECTIVE 5.2:

Continue the outreach efforts to local businesses and form partnerships to promote economic development programs.

Policies:

- 5.2.1. The City will support economic development initiatives through existing programs to continue public-private partnerships for job creation.
- 5.2.2. The City will create opportunities for youth to engage in local education, job training, and employment programs with a path to high-wage careers.

OBJECTIVE 5.3:
Explore incentives for affordable and workforce housing.

Policies:

- 5.3.1. The City will explore exemptions for new construction or redevelopment of workforce housing projects (i.e., Parkland Impact Fee, parking, density bonus, art ordinance, zoning, etc.).

OBJECTIVE 5.4:
Provide transportation options for workforce housing projects.

Policies:

- 5.4.1. The City will encourage the location of workforce housing projects near mass transit routes.
- 5.4.2. The City will continue to support public transportation agencies to improve service and amenities/facilities for mass transit.
- 5.4.3. The City will continue to enforce the requirements for providing bicycle parking in the Land Development Code.



C H A P T E R F O U R

4

CONSERVATION ELEMENT



Conservation

Conservation has long been a commitment for the City of Dunedin. Elements of conservation that are implemented in the City include water replenishment and waste reduction programs, water quality of lakes and aquifer systems, and preservation of coastal and other environmentally sensitive areas. The City has staff and community members who continually work on water programs, including stormwater, drinking water, wastewater, water quality efforts and water education. Coastal areas are preserved and protected by multiple City Departments, including Parks and Recreation, as well as through the educational programs that inform and create progressive action.

Dunedin is known for its world-class beaches - Honeymoon Island and Caladesi Island State Parks, extensive park system, and walkability. Being a coastal city, Dunedin is especially vulnerable and recognizes the effects of climate change, including sea level rise and destructive storms. To address this issue, the City shall adhere to the goals, objectives, and policies placed within the Coastal Management Element to address climate change and sea level rise.

With regards to lowering its greenhouse gas emissions, the City of Dunedin has recently adopted a resolution to commit to 100 percent renewable energy and other strategies to lower emissions. The City also continues to implement new technology and practices to better preserve its natural resources and plan to be a resilient City in the face of climate change.

Education is a large component to fighting environmental degradation. The City of Dunedin continues to find creative and effective ways to communicate and educate its residents, employees, and visitors on environmental quality, protection, and the next steps toward action.

The City of Dunedin understands the urgency of climate change and environmental degradation. The City is committed to the conservation of its natural resources and continues to look to the future. Dunedin is at the forefront of the movement that is creating positive changes and is a Statewide model for environmental sustainability and stewardship.



[Image 4-1] Dunedin Causeway



Intergovernmental Partners and Stakeholders' Plans, Programs/Initiatives

Partners and Stakeholders:

- U.S. Environmental Protection Agency (USEPA)
- U.S. Fish and Wildlife Services (USFWS)
- U.S. Green Building Council (USGBC)
- Green Building Initiative
- Florida Department of Environmental Protection (FDEP)
- Florida Fish and Wildlife Conservation Commission (FWC)
- Florida Green Building Coalition (FGBC)
- Southwest Florida Water Management District (SWFWMD)
- Tampa Bay Regional Planning Council (TBRPC)
- Tampa Bay Estuary Program
- Duke Energy
- Pinellas County Extension
- Pinellas County
- Keep Pinellas Beautiful
- Southeast Sustainability Director's Network (SSDN)
- Florida Sustainability Director's Network (FSDN)
- Recycle Florida Today (RFT)
- International Council for Local Environmental Initiatives (ICLEI)
- University of South Florida Patel College of Sustainability (USF)
- Ocean Allies
- Mission Blue

Plans, Programs/Initiatives:

- Leadership in Energy and Environmental Design Certification Program (LEED)
- Green Globes Certification Program
- Ready for 100: Commitment to 100 Percent Renewable Energy
- Mission Blue Hope Spot Initiative
- Duke Energy Park & Plug Electric Vehicle Charging Stations
- Florida Green Building Coalition Certification Designation
- Earth Day Celebrations
- Tree City USA
- Recycling Education
- Florida Water Star Program
- SWFWMD Regional Water Supply Plan
- Pinellas County Utilities' Potable Water Supply Facilities Work Plan
- SWFWMD Water Use Permit (City of Dunedin Water Supply Facilities Work Plan)
- Florida State Implementation Plan (SIP)
- University of South Florida Community Sustainability Partnership Program- Carbon Footprint Study
- Dunedin's Resilient Environmental Action Master Plan (DREAM)
- City of Dunedin Stormwater Master Plan

Related Maps:

- Map 4-1: Significant Wetlands

GOAL 1 – Water Consumption/Waste Management- Implement water replenishment, waste reduction, waste material recycling and hazardous waste reduction programs.

OBJECTIVE 1.1:

Develop, monitor, enforce, and staff projects, programs, and regulations that will mitigate excessive potable water loss and protect existing potable water sources.

Policies:

- 1.1.1. The City shall continue to enforce local development and building code requirements calling for the incorporation of water saving systems or devices.
- 1.1.2. The City will continue to maintain the municipal reclaimed water distribution system to irrigate all golf courses and City recreational, park facilities and expand residential services.
- 1.1.3. The City shall enforce emergency conservation restrictions in accordance with the mandates of SWFWMD.
- 1.1.4. The City will continue to monitor and protect the functions of water recharge.
- 1.1.5. The City will continue to coordinate with FDEP and its water use permits as well as SWFWMD and its Regional Water Supply Plan regarding issues related to consumptive use permits, water conservation measures, and education information.
- 1.1.6. The City will continue to provide public outreach efforts on water use conservation.
- 1.1.7. The City will continue to promote the use of native plants in landscaped areas, and use xeriscaping wherever possible to reduce water usage.

OBJECTIVE 1.2:

Reduce solid waste generation, removal, and disposal.

Policies:

- 1.2.1. The City will continue to promote reduction of waste generation through education and training.
- 1.2.2. The City will continue to transition away from single-use plastic and single-use materials through education and training.
- 1.2.3. The City will conduct waste audits of City buildings to understand the type of waste that is being generated and how to effectively educate employees on waste reduction measures to make changes.



OBJECTIVE 1.3:

Continue to implement a waste materials recycling program that will reduce landfill and resource recovery plant volumes.

Policies:

- 1.3.1. The City will continue to collect, at a minimum, newspaper, clear glass, and plastic polyethylene terephthalate bottles (PET- Number 1 and Number 2), cardboard, used beverage containers (UBC- aluminum and steel cans) and mixed paper products.
- 1.3.2. The City will continue to promote correct recycling methods through education and training.

OBJECTIVE 1.4:

Continue to implement the Pinellas County Hazardous Waste Management Program.

Policies:

- 1.4.1. The City will continue to assist Pinellas County in their public outreach efforts on safe disposal of hazardous waste.
- 1.4.2. The City shall enforce Federal, State, and County regulations regarding hazardous waste disposal.
- 1.4.3. The City will continue education efforts to inform citizens and businesses on the proper disposal of hazardous waste.
- 1.4.4. The City will support the continued development of environmentally safe hazardous waste treatment, storage, and disposal facilities.
- 1.4.5. The City shall require all hazardous waste generators to properly maintain their own wastes.
- 1.4.6. The City will continue to encourage the research, development, and implementation of recycling, resource recovery, energy recovery, and other methods of using garbage, trash, sewage, slime, sludge, hazardous waste and other waste for alternative energy sources.
- 1.4.7. The City will continue to encourage coordination of intergovernmental and interstate waste management efforts.
- 1.4.8. The City will continue to identify, develop, and encourage environmentally sound wastewater treatment and disposal methods.
- 1.4.9. The City will continue to expand, where possible, those methods established allowing low-volume generators of hazardous waste to safely dispose of these materials in a convenient manner.
- 1.4.10. The City will research and promote earth-friendly pest control and weed control methods to discourage the use of conventional pesticides and herbicides.



GOAL 2 – *Water Quality*- Preserve and/or improve the water quality of drainage basins and aquifer systems.

OBJECTIVE 2.1:

Remove, relocate, or redesign existing and potentially hazardous sources of pollutants.

Policies:

- 2.1.1. The City will implement runoff detention and pollutant removal techniques in constructing new or replacement of drainage collection and outfall systems.
- 2.1.2. The City will continue to enforce the Pinellas County’s Fertilizer Ordinance and provide public outreach efforts on sustainable gardening and lawn care maintenance.
- 2.1.3. The City will continue tertiary treatment at the Wastewater Treatment Plant and the expansion of the Reclaimed Water System in order to reduce wastewater flows into St. Joseph’s Sound.
- 2.1.4. The City will continue to protect the existing wetlands as identified in the Significant Wetlands Map and as referenced in Objective 1.6 and 1.7 of the Coastal Management Element.
- 2.1.5. The City will continue to encourage property owners to abandon septic tanks when sanitary sewer service is available to their properties for connection as referenced in Objective 1.2 of the Sanitary Sewer Subelement of the Support Services Element.

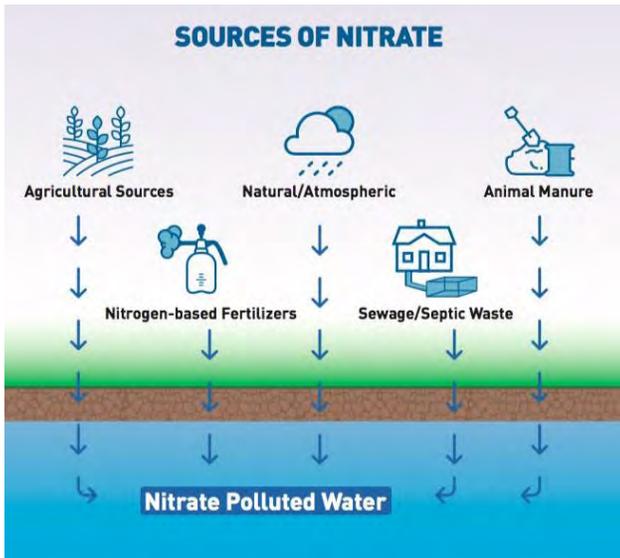
OBJECTIVE 2.2:

Continue to develop and implement conservation management and maintenance techniques.

Policies:

- 2.2.1. Where appropriate, cost-effective, and on a prioritized basis as to need, general public benefit and subject to budgeted funds availability, the City will maintain a program to stabilize drainage ways, creeks, lakes, and coastal shorelines from excessive erosion. Such a program will benefit the public at large and not be beneficial to only a limited number of property owners.

- 2.2.2. The City will continue to maintain drainage ways and basins free from obstructions that will inhibit adequate flows.
- 2.2.3. The City will continue to implement the design parameters and phases established in the City's Stormwater Master Plan.
- 2.2.4. The City will continue to provide public outreach efforts for the importance of sustainable stormwater maintenance practices.
- 2.2.5. The City will continue to work with intergovernmental partners and industry stakeholders to discover and learn new conservation techniques to apply into the existing operation/maintenance policies.



[Image 4-2] Nitrate Sources Pollutants
Source: Beta Analytic Inc.



[Image 4-3] City of Dunedin Wastewater Plant

GOAL 3 – Coastal/Wetland Preservation- Preserve coastal and environmentally sensitive areas.

OBJECTIVE 3.1:

Protect and preserve natural wildlife, vegetation, minerals, soils, and wetlands.

Policies:



[Image 4-4] Dunedin Causeway



[Image 4-5] Caladesi Island



[Image 4-6] St. Joseph's Sound

- 3.1.1. The City will continue to work with intergovernmental partners and industry stakeholders to learn emerging threats to land resources and develop ways to mitigate the threats through policy and regulation.
- 3.1.2. The City will continue to protect lands designated Preservation and Recreation/Open Space in the Future Land Use Map and referenced by policies in the Future Land Use and Recreation and Open Space Elements.
- 3.1.3. The City will continue to protect existing natural resources as identified and referenced by policies in the Conservation, Coastal Management and Recreation and Open Space Elements.
- 3.1.4. The City will continue to enforce Federal, State, County and local mandates for the protection of wildlife and wildlife habitats and particularly endangered species.
- 3.1.5. The City will continue to cooperate with adjacent municipalities and Pinellas County for the conservation of any vegetative communities located under more than one jurisdiction.
- 3.1.6. The City will continue to enforce Federal, State and County regulations when polluted sites are known.
- 3.1.7. The City will continue to protect the location and function of mangroves, wetlands and other ecological communities as identified and referenced by policies in the Conservation, Future Land Use, Coastal Management and Recreation and Open Space Elements.
- 3.1.8. The City will continue to assist the State in its management and protection of Honeymoon Island and Caladesi Island.
- 3.1.9. The City will continue to assist Pinellas County in its management and maintenance of Dunedin Causeway.
- 3.1.10. The City will continue to assist the State and Pinellas County in the protection and improvement of water quality to St. Joseph's Sound.

- 3.1.11. The City will continue to provide public outreach effort on the importance of and steps to protect the preserved lands and natural resources.
- 3.1.12. Impacts to natural wildlife, vegetation, minerals, soils and wetlands shall be considered and mitigated in development review process as referenced by policies in the Future Land Use and Recreation and Open Space Elements.

OBJECTIVE 3.2:

Support efforts for the re-establishment of marine reproduction and feeding grounds and benthic vegetation in St. Joseph's Sound.

Policies:

- 3.2.1. The City shall support the regulations by the Pinellas County Water and Navigation Control Authority for all future construction of bulkheads, seawalls, docks, or other water based structures to ensure that the marine ecological systems are not adversely impacted.
- 3.2.2. Through intergovernmental cooperation with the City of Clearwater, Pinellas County, Tampa Bay Regional Planning Council, and Tampa Bay Estuary Program, the City will coordinate adequate sites for water-dependent uses, promote increased water quality, and reduce exposure of degradation to natural resources in the estuarine system of St. Joseph's Sound.
- 3.2.3. The City will continue to support and assist intergovernmental agencies as well as citywide initiatives in public outreach efforts on the importance of marine ecological systems.
- 3.2.4. The City will continue to maintain and protect the shoreline from erosion as referenced in Policy 1.6.5. of the Coastal Management Element.



OBJECTIVE 3.3:

Preserve, augment, and prioritize water accessibility for public use along coastal waters.

Policies:

- 3.3.1. The City will continue to support Pinellas County Sheriff's Office with the enforcement of Florida State Statute 327.4109 and 376.15 regarding anchoring, mooring, or dereliction of vessels on St. Joseph's Sound.
- 3.3.2. The City will continue to enforce the Coastal Zone Protection Act of 1985 which shall include continuing and replacing adequate physical public access to the shoreline, public transportation, and parking facilities.
- 3.3.3. The City will continue the implementation of the City of Dunedin Zoning Code, which minimizes land use conflicts.

GOAL 4 – Air Quality- Promote the highest standards of air quality for the City of Dunedin's citizenry.

OBJECTIVE 4.1:

Through intergovernmental coordination and assisting in the enforcement of Federal, State, and County laws, the City shall help to ensure air quality emissions meet all required standards.

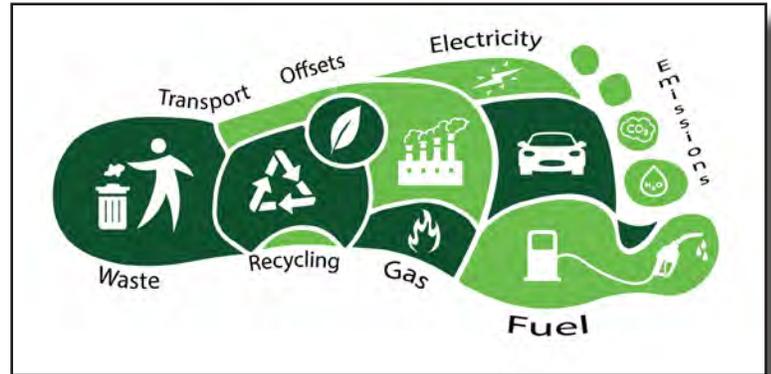
Policies:

- 4.1.1. The City will continue to maintain and improve air quality levels to safeguard human health and prevent damage to the natural environment through City policies and support the enforcement of Federal, State, and County regulations.
- 4.1.2. The City will continue to ensure that developments and transportation systems are consistent with the maintenance of optimum air quality through land development standards and regulations.
- 4.1.3. The City will provide public outreach efforts on the importance of air quality and the steps to protect or improve air quality.
- 4.1.4. The City will continue to promote the use of alternative energy resources that do not degrade air quality.





[Image 4-7] Air Quality Index (AQI)
Source: EPA



[Image 4-8] Carbon Footprint
Source: Ecomatcher

GOAL 5 – Carbon Footprint- Understand and develop ways to reduce greenhouse gas emissions and the City’s carbon footprint by utilizing clean energy sources and other sustainable methods.

OBJECTIVE 5.1:

Continue research, education, planning, and development with solar energy and other clean renewable energy sources.

Policies:

- 5.1.1. The City shall implement the adopted resolution for the Ready for 100 program, which commits the City to convert its municipal facilities to one hundred percent clean energy by 2035 and the rest of the City by 2050, to reduce the amount of greenhouse gases being produced.
- 5.1.2. The City shall enforce the Land Development Code requirements for new municipal buildings to include solar in their operations.
- 5.1.3. The City will continue to encourage new construction or renovation projects to consider solar as a primary source of energy by promoting the City's solar incentives as referenced in the Land Development Code.
- 5.1.4. The City will explore retrofitting the remaining municipal buildings in the City to operate under clean renewable energy sources such as solar.

- 5.1.6. The City will research and assess whether Photovoltaic (PV) solar equipment can be retrofitted to any municipal building.
- 5.1.7. The City will research Duke Energy partnerships for vehicular solar canopies and if applicable, devise plans to implement canopies in designated parking areas.
- 5.1.8. The City will continue to encourage commercial development projects to provide clean renewable energy sources such as solar as a primary source of energy.
- 5.1.9. The City will continue to provide public outreach efforts on solar energy and other clean renewable energy sources.
- 5.1.10. The City will research and promote other clean renewable energy sources.

OBJECTIVE 5.2:

Research, develop, and plan for vehicle fleet to be revamped through alternative fuel options and reduce greenhouse gas emission as feasible.

Policies:

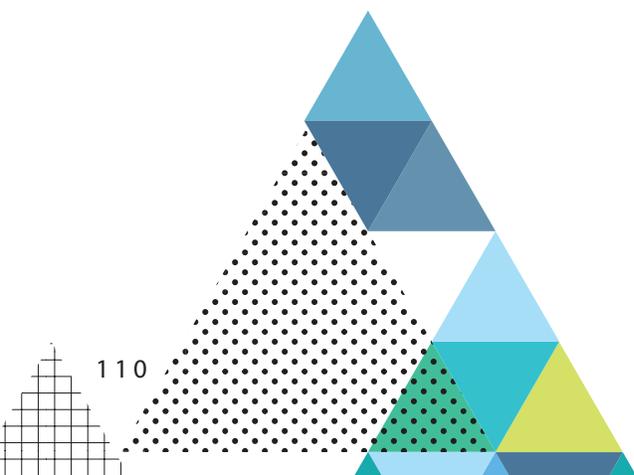
- 5.2.1. The City will research biodiesel, hybrid, and electric options for the vehicle fleet.
- 5.2.2. The City will reiterate the City’s idling policy through education and possible equipment.

OBJECTIVE 5.3:

Explore and develop ways to reduce greenhouse gas emission created by City employees.

Policies:

- 5.3.1. The City will develop employee efficiency and sustainability training for new hires and present employees.
- 5.3.2. The City will explore incentivizing alternative employee commuting practices.
- 5.3.3. The City will continue to encourage paperless operations amongst departments through education as well as the implementation of the electronic permitting and meeting agenda management software.
- 5.3.4. The City will explore increasing remote working capabilities for employees to lessen the total greenhouse gas emissions caused by commuting.





OBJECTIVE 5.4:
Reduce greenhouse gas emissions.

Policies:

- 5.4.1. The City will input and track data in a software called ClearPath through the International Council for Local Environmental Initiatives (ICLEI) to monitor progress of greenhouse gas reduction.
- 5.4.2. The City will provide public outreach efforts on ways to reduce greenhouse gas emissions.

OBJECTIVE 5.5:
Research, plan, and devise ways to reduce greenhouse gases being produced as a result of organic waste and organic systems and industries.

Policies:

- 5.5.1 The City will research ways other municipalities handle organic waste and composting.
- 5.5.2 The City will explore biodigesters and other equipment that reduces emissions and waste from organic materials.
- 5.5.3 The City will continue the backyard composting program within the City and will continue to provide public outreach efforts on the benefits in composting.

OBJECTIVE 5.6:
Implement the Sustainability Master Plan- DREAM.

Policies:

- 5.6.1 Dunedin's Resilient Environmental Action Master Plan (DREAM) will continue to be implemented and updated to ensure consistency with the Comprehensive Plan's goals, objectives, and policies of this Conservation Element.
- 5.6.2 The City shall provide adequate staffing and funding to develop, support, and implement the master plan.

GOAL 6 – *Level of Service*- Maintain that level of service for conservation and coastal management as adopted in the plan.

OBJECTIVE 6.1:

Implement the level of service standards found in the Transportation, Recreation and Open Space Elements, and the Sanitary Sewer, Portable Water, Stormwater and Solid Waste Subelements. These standards for facilities and services shall be the same as those for the remainder of the City.

Policies:

- 6.1.1. The City shall not allow proposed development unless adopted levels of service standards are met.
- 6.1.2. The City shall continue to enforce the Land Development Regulations that specify restoration and mitigation criteria for any future environmentally-related disruptions or degradations.
- 6.1.3. Where appropriate and cost-effective, stormwater studies shall be comprised of aspects of comprehensive watershed management planning, including flood protection, water quality, environmental health, and climate change.
- 6.1.4. The City will continue to implement and adjust LDC Table 105-28.4, Sustainability Worksheet, for future development.



C H A P T E R F I V E

5

COASTAL MANAGEMENT ELEMENT



Coastal Management

The purpose of the Coastal Management Element is to provide policies that will direct the City of Dunedin towards a long range plan centered on protecting our valued coastal assets. This Element of the Comprehensive Plan outlines the course of action for the management of our aquatic life, marine habitats, and coastal resources and properties. Ecological planning principles are addressed in this section as they relate to protecting our citizens, property, and coastal resources from the effects of hurricanes, storm surge, sea level rise as well as adverse anthropogenic impacts.

Coastal management requires intergovernmental efforts. The City of Dunedin must remain in compliance with Federal, State, and County regulations when preserving coastal and environmentally sensitive areas. Coordinated efforts must also be made when monitoring the water quality of our estuarine, gulf, and tidally affected lakes, drainage basins, and aquifer system. The City of Dunedin's Coastal High Hazard Areas (CHHA), the areas situated along the coast that are most likely to experience flooding after tropical storm events, must also be managed as designated by the Federal Emergency Management Agency (FEMA).

[Image 5-1] Honeymoon Island



[Image 5-2] Weaver Park

The Coastal Management Element of the Comprehensive Plan also includes goals which keep the City in compliance with Section 163.3178, Florida Statutes, also known as the Peril of Flood Act. The Peril of Flood Act was signed into law in 2015 by Governor Rick Scott and requires the City to acknowledge the risk of floods, storm surges, and sea level rise when considering “redevelopment principles, strategies, and engineering solutions” in order to achieve effective flood risk mitigation. There are six components that the Coastal Management Element that must address in regards to the Peril of Flood Act:

1. Include development and redevelopment principles, strategies, and engineering solutions that reduce the flood risk in coastal areas which results from high-tide events, storm surge, flash floods, stormwater runoff, and the related impacts of sea-level rise.
2. Encourage the use of best practices development and redevelopment principles, strategies, and engineering solutions that will result in the removal of coastal real property from flood zone designations established by the Federal Emergency Management Agency (FEMA).
3. Identify site development techniques and best practices that may reduce losses due to flooding and claims made under flood insurance policies issued in this state.
4. Be consistent with, or more stringent than, the flood-resistant construction requirements in the Florida Building Code and applicable flood plain management regulations set forth in 44 C.F.R. part 60.
5. Require that any construction activities seaward of the coastal construction control lines established pursuant to s. 161.053 be consistent with chapter 161 of Florida State Statute.
6. Encourage local governments to participate in the National Flood Insurance Program (NFIP) Community Rating System (CRS) administered by FEMA to achieve flood insurance premium discounts for their residents.

The goals, objectives, and policies of this Element reflect the six components of the Peril of Flood Act. Additionally, the goals, objectives, and policies of this Element reflect the City's vision of preserving public access for recreation and preservation as well as working with the community to implement practices that protect and restore the health of the waters.



Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- Federal Emergency Management Agency (FEMA)
- U.S. Environmental Protection Agency (USEPA)
- U.S. Fish and Wildlife Services (USFWS)
- U.S. Army Corps of Engineers
- Florida Department of Environmental Protection (FDEP)
- Florida Department of Transportation (FDOT)
- Florida Division of Emergency Management
- Florida Fish and Wildlife Conservation Commission (FWC)
- Southwest Florida Water Management District (SWFWMD)
- Tampa Bay Regional Planning Council (TBRPC)
- Tampa Bay Area Regional Transit Authority (TBARTA)
- Pinellas County
- Pinellas County Local Mitigation Strategy Working Group (LMS)
- Pinellas Suncoast Transit Authority (PSTA)
- Pinellas County Extension
- Tampa Bay Watch

Plans, Programs/Initiatives:

- National Flood Insurance Program (NFIP)
- Community Rating System (CRS)
- Pinellas County Local Mitigation Strategy Plan
- University of South Florida Community Sustainability Partnership Program- Peril of Flood Act Comprehensive Plan Amendment and Sea Level Rise Study
- City of Dunedin Code of Ordinances
- City of Dunedin Land Development Code
- City of Dunedin Stormwater Master Plan

Related Maps:

- Map 5-1: Coastal Storm Area
- Map 5-2: Hurricane Storm Surge Area / Evacuation Routes & Shelters
- Map 5-3: Topographic Map
- Map 5-4: Soil Map
- Map 5-5: Hydrological Features
- Map 5-6: Public Waterways and Coastal Access Areas

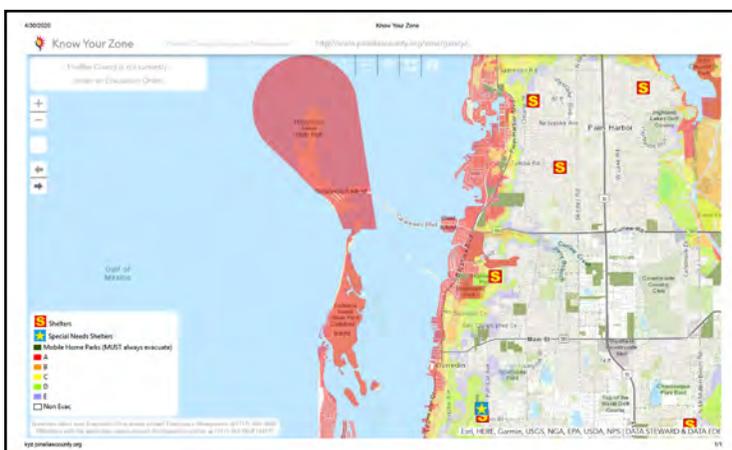
GOAL 1 – Pre-Disaster Mitigation- Protect human life, public and private property, and coastal resources from the effects of hurricanes and other natural disasters in the Coastal Storm Area.

OBJECTIVE 1.1:

Identify the Coastal Storm Area which may be impacted by high tide events, storm surge, flash floods, stormwater runoff, and the related impact of sea level rise.

Policies:

- 1.1.1. The Coastal High Hazard Area (CHHA) shall be the area defined by the Sea, Lake and Overland Surges from Hurricanes (SLOSH) model to be inundated from a category one hurricane, as reflected in the most recent Regional Evacuation Study.
- 1.1.2. The Coastal Storm Area shall be the area delineated in the floodplain map of the Coastal Management Element, which encompasses the following:
 - 1.1.2.1. The Coastal High Hazard Area as defined in Policy 1.1.1 and shown on Map 5-1.
 - 1.1.2.2. Those isolated areas surrounded by the CHHA or by the CHHA and a body of water.
 - 1.1.2.3. The entire parcel of land if 20 percent or more of the parcel is located within the CHHA.
 - 1.1.2.4. All land connected to the mainland of Dunedin by bridges or causeways.
 - 1.1.2.5. All land located within the Velocity Zone as designated by FEMA.
 - 1.1.2.6. All land beyond the Coastal Construction Control Line (CCCL).
 - 1.1.2.7. All land located within FEMA designated flood zones.



[Image 5-3] Pinellas County Know Your Zone Map
Source: Pinellas County



[Image 5-4] Dunedin Causeway



[Image 5-5] Dunedin Marina

OBJECTIVE 1.2:

Direct permanent population concentrations away from the Coastal Storm Area and preserve water accessibility for public use along coastal waters.

Policies:

- 1.2.1. The City shall strictly enforce building and construction codes in accordance with FEMA standards and the National Flood Insurance Program's (NFIP) regulations and all State, regional, and County standards.
- 1.2.2. The City shall prohibit the construction of hospitals, nursing homes and additional, or expansions to, mobile home parks within the coastal storm area. The City shall prohibit the construction of adult congregate living facilities within the coastal storm area. The City shall prohibit new ACLFs and the expansion of the existing ACLFs within the CHHA. This provision shall not be in conflict with the mandates of Chapter 419, Florida Statutes, Community Residential Homes.
- 1.2.3. The City shall not amend the Future Land Use Map or Zoning Map to permit any mobile home parks to be located within the Coastal Storm Area.
- 1.2.4. The City shall coordinate and partner with intergovernmental agencies to help preserve public access to coastal waters as referenced in the Conservation Element Objective 3.3 and Recreation & Open Space Element Policy 4.3.4.
- 1.2.5. Adequate sites and access for water-dependent uses shall be coordinated and permitted through Pinellas County, Florida Department of Environmental Protection (FDEP), the Southwest Florida Water Management District (SWFWMD) and/or the Army Corps of Engineers.

OBJECTIVE 1.3:

Enforce land development regulations to protect public and private property and human life from the effects of hurricane winds, flooding, and other claims made under flood insurance policies issued in the State.

Policies:

- 1.3.1. The City shall limit public expenditures that subsidize development permitted in the Coastal Storm Area, except for the restoration or enhancement of natural resources.
- 1.3.2. The City shall ensure that the construction of necessary infrastructure improvements in the Coastal Storm Area are phased to coincide with the demands generated by development or redevelopment to support the densities permitted by the City's adopted Future Land Use Map.
- 1.3.3. The City's public expenditures in the Coastal Storm Area, when available, will include program improvements that will restore and enhance natural resources.
- 1.3.4. The City shall encourage more efficient and climate resilient construction practices locally by:
 - 1.3.4.1. Enforcing base finish floor elevation standards with respect to projected sea level rise scenarios and flooding potential per Chapter 105-21 of the Land Development Code.
 - 1.3.4.2. Enforcing the City's cumulative substantial improvement and substantial damage five-year time period regulation per Chapter 105-21 of the Land Development Code.
 - 1.3.4.3. Evaluating the extension of the City's substantial improvement and substantial damage regulatory threshold to less than 50 percent.
- 1.3.5. The City will encourage sustainable stormwater infrastructure design in future development and redevelopment projects.
- 1.3.6. The City shall not increase permanent residential density and intensity within the Coastal Storm Area.



OBJECTIVE 1.4:
Protect historical and archaeological resources in the Coastal Storm Area.

Policies:

- 1.4.1. The City will promote the preservation of historic and archaeological resources by providing information to the public and encouraging private groups to nominate sites to preserve.
- 1.4.2. The City will enforce the Land Development Code Chapter 111 which establishes performance standards for development and sensitive reuse of historic resources.

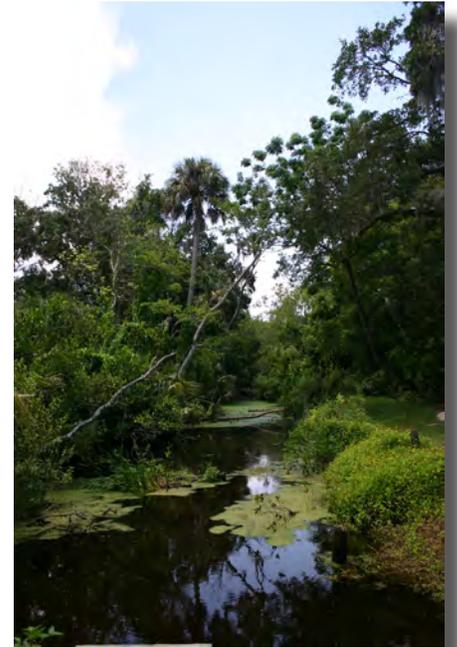


[Image 5-6] 204 Scotland St

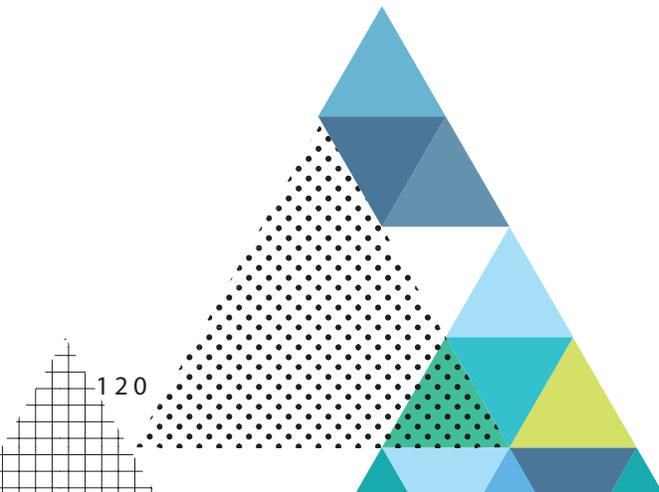
OBJECTIVE 1.5:
Establish and recognize, as applicable, level of service standards found in the Transportation, Recreation & Open Space and Support Services Elements to the Coastal Storm Area.

Policies:

- 1.5.1. Public facilities, infrastructure and utilities in the Coastal Storm Area shall be maintained as necessary, consistent with the adopted level of service standards found in the Transportation, Recreation & Open Space and Support Services Elements.



[Image 5-7] Hammock Park



OBJECTIVE 1.6:

Continue to protect coastal wetlands, water surface and wildlife habitats through maintenance and enforcement of regulations.

Policies:

- 1.6.1. Development applications shall be reviewed to ensure that proposed new development or redevelopment will be located in a manner that preserves wetlands; enhances water quality; and protects wildlife habitat. New development shall be guided away from environmentally sensitive areas.
- 1.6.2. The City shall work toward reducing the existing quantity and improving the quality of stormwater runoff to natural water bodies as referenced in the Conservation Element Objective 2.1, 2.2 and 3.2.
- 1.6.3. The City shall coordinate with Pinellas County and other local governments for water quality monitoring and related program planning.
- 1.6.4. The City shall protect natural shorelines which provide stabilization and protect landward areas from storm impacts.
- 1.6.5. The City will continue to maintain and protect the shoreline from erosion by supporting the installation and maintenance of native coastal vegetation along appropriate areas of the coastline and utilize hard structures only when alternative options are unavailable.

OBJECTIVE 1.7:

Utilize the Future Land Use Map to protect coastal wetlands, water surface, and wildlife habitats.

Policies:

- 1.7.1. The City shall permit park and recreation uses in coastal areas.
- 1.7.2. Future land use designations as identified in Table 1-1 of the Future Land Use Element will recognize, protect, and conserve wetlands and wetland functions.
- 1.7.3. Where incompatible land uses occur; best practice mitigation strategies shall be implemented as one means to compensate for loss of wetlands functions. Such practices shall include low-impact development strategies such as, but not limited to, utilizing rain gardens, permeable pavement, and bioretention areas or similar practices.
- 1.7.4. The City will continue to maintain existing Recreation/Open Space Future Land Use designations within the Coastal Storm Area.



OBJECTIVE 1.8:

Update the Pre-Disaster Mitigation Data on an annual basis.

Policies:

- 1.8.1. The City shall implement the most recent sea level rise and floodplain management data available to the objectives mentioned in this goal.
- 1.8.2. Each year the City will engage with its intergovernmental partners and stakeholders to stay abreast of, support, and promote best practices for coastal management as well as gather input and/or data from these partners and stakeholders to determine how it can be used to measure and achieve the objectives in this Element.

GOAL 2 – Flood Risk Management- Manage and regulate new development, redevelopment, and investment in public facilities, utilities, and infrastructure to reduce flood risk in the coastal areas resulting from high-tide events, storm surge, flash floods, stormwater runoff, and the related impacts of sea level rise and to reduce losses due to flooding and claims made under flood insurance policies issued in this State.

OBJECTIVE 2.1:

Plan and manage development and redevelopment in the City to reduce risk and losses due to flooding resulting from high-tide events, storm surge, flash floods, stormwater runoff, and the related impacts of sea level rise.

Policies:

- 2.1.1. Development and redevelopment plans and proposals in the Coastal Storm Area shall be reviewed for compliance with the goals, objectives, and policies of the Comprehensive Plan and other appropriate plans and references, including the City's National Flood Insurance Policy (NFIP) Community Rating System (CRS) Program and Floodplain Management Plan (Pinellas County Local Mitigation Strategy Plan).
- 2.1.2. The flood-resistant construction requirements in the Florida Building Code and applicable floodplain management regulations set forth in 44 C.F.R. part 60, or more stringent controls, shall continue to be applied to development and redevelopment in the Coastal Storm Area.
- 2.1.3. The City shall require, to the greatest extent possible, the use of Low-Impact Development (LID) site design methods for new development, redevelopment, additions, and retrofits and modifications to properties in the Coastal Storm Area.

- 2.1.4. LID design standards shall provide for site design, engineering, and stormwater management designs and retrofits that reduce run-off, mitigate flood impacts, and provide for the on-site absorption, capture and reuse of rainwater. The standards shall encourage or require the increased use of “green” stormwater management treatments, native/Florida-friendly landscape material, porous paving materials, and the preservation and restoration of natural drainage characteristics.
- 2.1.5. The City shall grant building permits in compliance with the rules of FEMA.
- 2.1.6. Impacts to natural protective features such as mangroves, seagrasses, and wetlands shall be considered in review of development and redevelopment proposals.

OBJECTIVE 2.2:

Implement hazard mitigation efforts to reduce flood risk resulting from high tide events, storm surge, flash flood, stormwater runoff, and the related impacts of sea level rise.

Policies

- 2.2.1. The City shall encourage and support hazard mitigation efforts through continuation of the following activities:
 - 2.2.1.1. Participation in the National Flood Insurance Program’s Community Rating System (CRS);
 - 2.2.1.2. Participation in the Pinellas County’s Local Mitigation Strategy;
 - 2.2.1.3. Administration of building regulations consistent with or more stringent than Florida Building Code and FEMA regulations;
 - 2.2.1.4. Review and implement appropriate policies and strategies developed by partner agencies or through interagency hazard assessment and mitigation initiatives;
 - 2.2.1.5. Restriction of development in floodways.
- 2.2.2. The City will continue to monitor and coordinate with the Pinellas County Local Mitigation Strategy Working Group and evaluate the feasibility of incorporating recommendations from the initiative and other intergovernmental hazard planning initiatives into the Comprehensive Plan and Land Development Code.
- 2.2.3. The City will continue to apply the Florida Building Code and Land Development Code requirements to ensure noncompliant structures are brought into conformance with FEMA flood elevations standards or flood-proofed consistent with FEMA standards.
- 2.2.4. The City shall enforce any construction activities seaward of the coastal construction control lines established pursuant to Section 161.053, Florida Statutes shall be consistent with Chapter 161, Florida Statutes.

OBJECTIVE 2.3:

Continue to participate in the Community Rating System (CRS). The City shall also continue to educate the public on flood risks, as well as mitigation strategies and available programs to reduce flood hazards and improve the City's Community Rating System (CRS) score, which will allow for decreased flood insurance premiums.

Policies:

- 2.3.1. The City will continue to prepare and disseminate communications and conduct NFIP community workshops to provide information regarding the benefits of acquiring flood insurance.
- 2.3.2. The City will continue to update, maintain, and make available for public review flood risk maps and related information indicating risks associated with high tide events, storm surge, flash flood, stormwater runoff, and related impacts of sea level rise.
- 2.3.3. The City shall continue to make flood elevation certificates available for public review.

OBJECTIVE 2.4:

Adapt the built environment to address impacts of sea level rise as an integral part of the City's planning process and in its coordination with partner agencies.

Policies:

- 2.4.1. The anticipated impacts of storm surge and sea level rise shall be a consideration in the implementation and administration of the Comprehensive Plan, building and life-safety codes, capital improvement plans, emergency management plans, land development codes, water resource and stormwater management plans, coastal management plans, and economic development programs.
- 2.4.2. The City shall share information and work with local, State and Federal entities in the continued assessment of vulnerabilities and the development of mitigation and adaptation strategies to address the impacts of sea level rise.





OBJECTIVE 2.5:

Designate Adaptation Action Areas(s), identifying locations vulnerable to the impacts of sea level rise. Include Adaption Action Area mapping in the Future Land Use Plan map series and define policies and strategies applicable to development, redevelopment and investment in public facilities, infrastructure and utilities addressing long term impacts.

Policies:

- 2.5.1. The City will map Adaptation Action Areas to identify areas vulnerable to the impacts of sea level rise.
- 2.5.2. The City will identify and implement adaptation policies to increase community awareness and evaluate the impacts of requiring elevated finished floors, additional freeboard and wet/dry flood-proofing in areas outside designated floodplains but within the designated Adaptation Action Area.
- 2.5.3. The City will work with local, State and regional partners to identify funding sources to support adaptation projects located within Adaptation Action Areas.
- 2.5.4. The City will evaluate the costs and benefits of adaptation alternatives in the location and design of new infrastructure and the fortification of retrofiting of existing infrastructure.
- 2.5.5. After the mapping of Adaptation Action Areas, the City will follow a regular program of assessment and reevaluation to ensure policies and strategies address known and anticipated risks associated with sea level rise. Activities shall include acquisition of necessary modeling data and programs to update the Adaptation Action Area map(s) in the City's Future Land Use Plan map series.

OBJECTIVE 2.6:

Complete vulnerability and resilience to sea level rise assessments as well as identify cost-effective strategies to ensure resiliency when constructing or reconstructing utilities or infrastructure.

Policies:

- 2.6.1. The City shall continue to evaluate opportunities to protect public facilities, infrastructure and utilities from the impacts of sea level rise.
- 2.6.2. The City shall utilize the completed Citywide Vulnerability Assessment to prioritize infrastructure improvements.

OBJECTIVE 2.7:

Evaluate and identify which public investments and infrastructure should be built or rebuilt, modified, or relocated to maximize the effective lifespan in accordance to the Citywide Vulnerability Assessment.

Policies:

- 2.7.1. The City shall identify public facilities and infrastructure at risk from sea level rise and plan for updates to the assessment every five years. The City shall analyze vulnerability to public facilities and infrastructure, including but not limited to: public buildings and facilities, such as police and fire stations as well as parks and recreation facilities; water and water reclamation and treatment facilities, transmission lines and pumping stations; stormwater systems; roads and bridges; and other transportation and transit infrastructure.
- 2.7.2. Based on the Citywide Vulnerability Assessment, the City will determine appropriate strategies related to the modification in place [e.g. elevation, hardening, relocations, etc.] or strategic retreat from areas at risk.
- 2.7.3. The City shall evaluate elevating roads and bridges above base flood elevation to maintain dry access in situations where flood waters tend to washout the roads. Construction, reconstruction or repair can include not only drainage, but also stabilization or armoring of vulnerable shoulders or embankments.
- 2.7.4. The City shall expand upon existing emergency management communication efforts and initiate a program to coordinate resilience and adaptation initiatives with the owners of private utilities, hospitals and clinics, nursing homes and assisted living facilities and other privately-operated facilities in locations identified as vulnerable to impact from sea level rise.

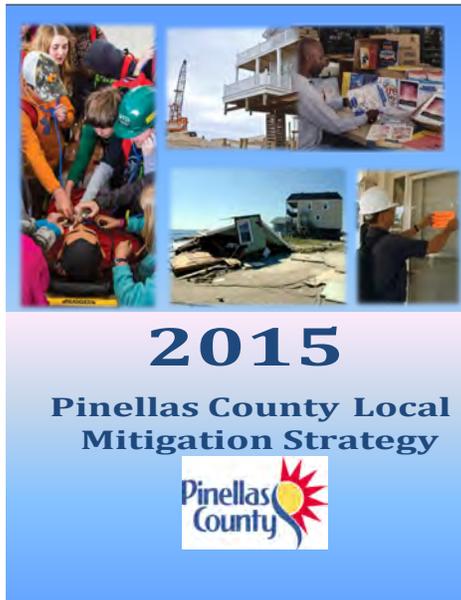
OBJECTIVE 2.8:

Update the Flood Risk Management Data on an annual basis.

Policies:

- 2.8.1. The City shall implement the most recent sea level rise and floodplain management data available to the objectives mentioned in this goal.
- 2.8.2. Each year the City will engage with its intergovernmental partners and stakeholders to stay abreast of, support, and promote best practices for coastal management as well as gather input and/or data from these partners and stakeholders to determine how it can be used to measure and achieve the objectives in this Element.

GOAL 3 – *Emergency Management*- Ensure the safety of all citizens and visitors from the dangers of natural disaster events such as high-tide, storm surge, flash floods, stormwater runoff, and other impacts related to sea level rise.



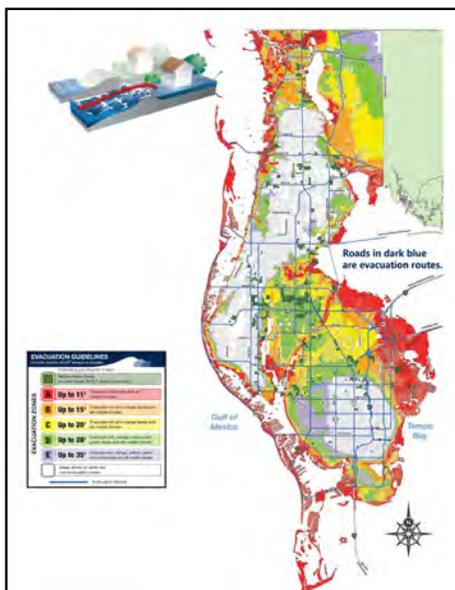
[Image 5-8] Pinellas County LMS
Source: Pinellas County

OBJECTIVE 3.1:

Ensure all steps are taken to prepare and respond to emergencies in the event of natural disaster.

Policies:

- 3.1.1. The City will continue to participate with Pinellas County and surrounding municipalities in the update, adoption, and implementation of the Pinellas County Local Mitigation Strategy (LMS), contained within the current Pinellas County Emergency Management Plan, and ensure consistency with City plans.
- 3.1.2. The City will continue to actively participate in hurricane evacuation planning with Pinellas County and other local, regional, and State organizations in order to improve existing evacuation routes and shelter spaces as shown in the Evacuation Routes, Shelters and Zones Map.
- 3.1.3. The City shall work with Pinellas County to continuously improve its out-of-county hurricane evacuation clearance time. Consistent with Pinellas County's standard, the adopted level of service standard for out-of-county hurricane evacuation clearance time for a Category 5 storm event as measured on the Saffir-Simpson scale shall be no greater than 50 hours.
- 3.1.4. The City shall support roadway maintenance and construction projects on identified critical links and major evacuation routes.
- 3.1.5. The City shall encourage transportation alternatives for persons evacuating to a public shelter including the Pinellas County Emergency Management Department's pre-registration program for evacuation assistance.
- 3.1.6. The City shall require disaster plans for all hospitals, nursing homes and assisted living facilities, as required by law, to be kept on file with the Emergency Management Coordinator.



[Image 5-9] Pinellas County Evacuation Zones and Routes
Source: Pinellas County

- 3.1.7. The City shall require development agreements for any new or redeveloped transient uses located within the Coastal Storm Area. Development agreements shall also meet the requirements of Chapter 103-15 of the Land Development Code, which requires compliance to Pinellas County and City hurricane evacuation plans and procedures.
- 3.1.8. The City shall not approve any Comprehensive Plan amendment that adversely affect the level of service for full hurricane evacuation to outside the County for a Category 5 storm event.

OBJECTIVE 3.2:
Update the Emergency Management Data on an annual basis.

Policies:

- 3.2.1. The City shall implement the most recent sea level rise and floodplain management data available to the objectives mentioned in this goal.
- 3.2.2. Each year the City will engage with its intergovernmental partners and stakeholders to stay abreast of, support, and promote best practices for coastal management as well as gather input and/or data from these partners and stakeholders to determine how it can be used to measure and achieve the objectives in this element.



GOAL 4 – *Post-Disaster Redevelopment*- Accomplish post-disaster redevelopment in a manner that minimizes public and private vulnerability to future disasters.

OBJECTIVE 4.1:

Comply with all applicable State, County, and City construction regulations, codes, and standards related to post-disaster redevelopment.

Policies:

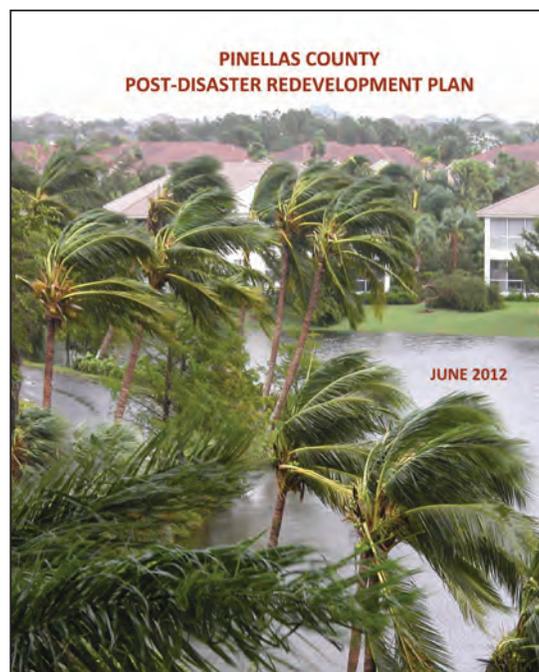
- 4.1.1. The City shall develop operating policies that address post-disaster redevelopment needs to facilitate permissible reconstruction in a timely manner, which includes participating in the Pinellas County Local Mitigation Strategy.
- 4.1.2. Structures located in FEMA-designated high hazard flood zones which have incurred damage over 50 percent of the assessed value shall be redeveloped within the density and intensity standards established in the Comprehensive Plan and built to present Land Development Code, Florida Building Code, and FEMA standards.
- 4.1.3. Sanitary sewer facilities and lift stations shall be built to resist infiltration by floodwaters.
- 4.1.4. Immediate repair and clean-up actions after a storm shall be limited to debris removal and repair of existing primary structures to allow re-occupancy (incurred damage less than 50 percent of assessed value). Long-term repair and redevelopment shall consist of upgrading structures and accessory facilities to expand habitable space or repair of incurred damage greater than 50 percent of the assessed value. Post-disaster redevelopment and long-term repair can only be permitted consistent with the requirements of FEMA, Florida Statutes, and the City's Land Development Code.
- 4.1.5. Damaged infrastructure shall continue to be repaired or rebuilt to minimize the potential for future damage. Relocating public facilities outside the Coastal Storm Area shall be considered, unless the facility is necessary to serve the population of the area.
- 4.1.6. Temporary-building moratoriums may be declared in the Coastal Storm Area when 50 percent or more of the homes have been destroyed in order to assess impacts and feasibility of redevelopment.
- 4.1.7. Repair and rebuilding of critical facilities such as water facilities, sanitary sewer treatment plants and lift stations, and other utilities damaged in future storms shall be reconstructed to minimize hurricane and flooding vulnerability.
- 4.1.8. Coastal infrastructure shall be maintained and replaced as necessary to insure adequate levels of service.
- 4.1.9. The City will maintain or expand agreements with other jurisdictions and private building plans reviewers and inspectors to aid with reconstruction after a disaster.

OBJECTIVE 4.2:

Include provisions for repair and cleanup, assessment of infrastructure, and limited redevelopment on properties that are repeatedly damaged related to post-disaster redevelopment.

Policies:

- 4.2.1. The City shall protect public health and safety, long-term repair, and redevelopment activities through immediate actions such as repair and/or cleanup.
- 4.2.2. The City shall address the removal, relocation, or structural modification of damaged infrastructure and be consistent with Federal funding provisions and unsafe structures.
- 4.2.3. The City shall discourage development in areas of repetitive loss properties.
- 4.2.4. The City shall continue to review inventories of repetitive loss properties provided by FEMA and continue working with State officials to improve the process of reducing vulnerability and loss for listed properties.
- 4.2.5. As part of post-disaster planning and management, the City shall prepare inventories of properties for acquisition and removal, including repetitive loss properties, and establish clear priorities for the use of acquisition resources.
- 4.2.6. The City shall address the replacement, removal, relocation, or structural modification of damaged and unsafe structures and infrastructure and distinguish between the recovery phase and long-term redevelopment.



[Image 5-10] Pinellas County Post Disaster Redevelopment Plan
Source: Pinellas County

OBJECTIVE 4.3:

Plan for the reestablishment of businesses after a disaster as it is critical to the redevelopment of the Coastal Storm Area.

Policies:

- 4.3.1. The City will partner with local businesses and organizations to aid in disaster planning and recovery for businesses located within the Coastal Storm Area and citywide.
- 4.3.2. The City will utilize its CRS program in developing an informational program about hurricane preparedness for local businesses.

OBJECTIVE 4.4:

Update the Post-Disaster Redevelopment Data on an annual basis.

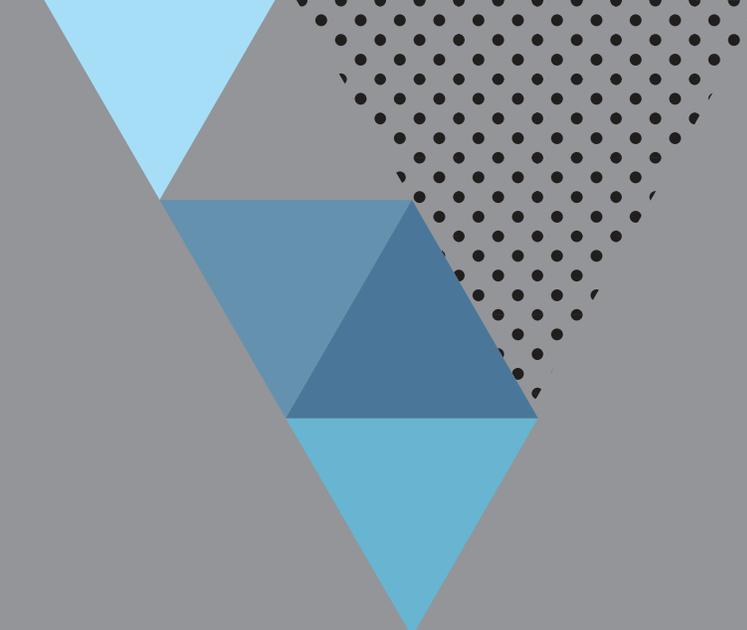
Policies:

- 4.4.1. The City shall implement the most recent sea level rise and floodplain management data available to the objectives mentioned in this goal.
- 4.4.2. Each year the City will engage with its intergovernmental partners and stakeholders to stay abreast of, support, and promote best practices for coastal management as well as gather input and/or data from these partners and stakeholders to determine how it can be used to measure and achieve the objectives in this element.



[Image 5-10] Business Preparedness
Source: Pinellas County





6

C H A P T E R S I X

RECREATION & OPEN SPACE ELEMENT



RECREATION

Recreation & Open Space

The City of Dunedin has an outstanding array of parks, open spaces, and recreational facilities with a reputation unmatched in Pinellas County. The City's unique design, walkable downtown, and prominent parks and recreation system all contribute to the citizenry's high quality of life. Parks and recreation support economic, physical, and social health while creating a sense of place and connecting people to nature.

Dunedin's parks and recreation system is characterized by a diversity of opportunities. It is known foremost for its well-maintained active and passive parks, natural areas, and recreational facilities and programs for all ages. The system is also characterized by a golf course, a marina, a fine art center, a professional baseball stadium in addition to training facilities and other special purpose facilities. Natural areas support conservation, environmental education and habitat connectivity.

Several guiding principles provide direction for the goals and initiatives of the City's recreation and open space requirements. These include the Parks & Recreation Department's mission, vision and core values as well as the Parks & Recreation Strategic Plan.



[Image 6-1] Scotsdale Park

[Image 6-2] Dunedin Summer Camp



Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- Florida Recreation and Park Association (FRPA)
- National Recreation and Park Association (NRPA)
- Friends of the Hammock, Inc.
- Kiwanis Club of Dunedin
- Rotary Clubs of Dunedin
- Dunedin Youth Guild
- VFW Post 2550
- Special Olympics
- Dunedin Scottish Arts Foundation
- Dunedin Youth Soccer
- Dunedin Little League
- Dunedin Golf Club
- Dunedin Fine Art Center
- Dunedin Youth Sailing
- Dunedin Boat Club
- Dunedin Windlasses
- Toronto Blue Jays
- Achieva Credit Union
- Dunedin Chamber of Commerce
- Downtown Dunedin Merchants Association
- Pinellas County
- Pinellas County School Board
- Florida Department of Environmental Protection (FDEP)
- Florida State Parks
- Florida Fish and Wildlife Conservation Commission (FWC)

Plans, Programs/Initiatives:

- Dunedin Parks & Recreation Strategic Plan
- National Parks and Recreation Month
- LiveWell Dunedin
- Arbor Day Celebration and Tree Give-Away
- Tree City USA
- Dr. Martin Luther King, Jr. Unity Celebration
- Senior Awareness Day
- Intergenerational Week
- Every Child A Swimmer
- Dunedin for Youth Scholarship Fund
- Purple Heart Day Ceremony
- Before & After School Programs
- Summer Camps

Related Maps:

- Map 6-1: Parks, Open Space, Urban Trails



GOAL 1 – *Level of Service Standards*- Maintain at a minimum the levels of service adopted in this plan for Recreation and Open Space.

OBJECTIVE 1.1:

Ensure that parks and recreation facilities are adequately provided.

Policies:

- 1.1.1. The City shall maintain a minimum level of service standard of 6 acres/1,000 population of developed parks.
- 1.1.2. Parks shall be equitably distributed throughout the City to provide equal access for all residents. The City shall strive to provide access to new parks in any of the service area gaps identified in the Parks and Recreation Strategic Plan.
- 1.1.3. Parks and recreation facilities shall provide for the minimum number of amenities identified in the Parks and Recreation Strategic Plan. The City shall strive to provide additional facilities for any identified deficits.
- 1.1.4. The City shall assess a parkland impact fee to all new residential development to ensure that parks and recreation facilities are adequately provided.



[Image 6-3] Hammock Park

[Image 6-4] Dunedin Community Center



GOAL 2 – *Preservation of Lands*- Establish a public position for the preservation of open space.

OBJECTIVE 2.1:

Retain valuable open space, recreational and natural areas free from development or change in use and acquire appropriate parcels for park development or redevelopment.

Policies:

- 2.1.1. The lands owned by the City of Dunedin for recreation and open space shall be retained for such purposes.
- 2.1.2. The City will continue to identify and place in the public trust those parcels that are unique for their location or vegetative character as park and open space within a highly urbanized fabric.
- 2.1.3. The City shall take measures to protect environmentally sensitive lands, natural areas, and unique habitats by developing and implementing best management practices for mitigating impacts to the environmentally sensitive lands, natural areas, and unique habitats, utilizing transfer of development rights, and City purchase for conservation of environmentally sensitive lands, natural areas, and unique habitats.
- 2.1.4. The City will continue, and expand where feasible, the partnership between the City and the Pinellas County School Board in making facilities available to Dunedin's citizenry.

GOAL 3 – *Programming*- Support innovative recreational and activity programs that fully utilize the City's facilities and serve all of its areas, interests and demographic groups.

OBJECTIVE 3.1:

Continue to promote strong interest in City-sponsored recreational programs as they benefit the public's well-being. Ensure parks and recreational programs address the needs of all of the users as participation in such activities increases the physical, emotional and mental health of the community.

Policies:

- 3.1.1. The City will continue to develop specialized programs which are attractive to all age groups including youth, teens, adults and seniors and provide for their social, physical and emotional health and development.

- 3.1.2. The City will continue City-sponsored or joint-sponsored recreation activities and events that generate community pride, engagement and civic involvement.
- 3.1.3. The City will continue to provide the funding and personnel to develop and promote recreational programs and facilities that serve for therapeutic recreation, inclusive design and programs and ADA accessibility.
- 3.1.4. The City will continue to promote activities that will enhance cooperation between City government, civic groups and private interests.
- 3.1.5. The City will continue to provide programs in nature education and opportunities for the citizens to connect with the natural environment in an otherwise urbanized community.

GOAL 4 – *Facilities-* Support and expand parks and recreation facilities.

OBJECTIVE 4.1:

Identify, acquire and develop parcels of land as identified in the Parks and Recreation Strategic Plan.

Policies:

- 4.1.1. The City will acquire and/or develop parcels that will increase the public's access to coastal and significant inland waters and boating facilities.
- 4.1.2. The City will acquire and/or develop general green space parcels that will increase the public's access to recreation or nature opportunities.
- 4.1.3. The City will continue to incorporate measures to provide unrestricted access and use of park and recreational facilities for those with disabilities and mobility restrictions.



[Image 6-5] TD Ballpark

OBJECTIVE 4.2:

Provide adequate funding for the operations, maintenance, repairs and replacements of the existing and future park and recreational facilities.

Policies:

- 4.2.1. The City will continue to provide sufficient equipment, personnel and funding to maintain all parks and recreation facilities.
- 4.2.2. The City will continue to provide adequate capital improvement funding of current and future parks and recreation facilities consistent with the Parks and Recreation Strategic Plan.
- 4.2.3. The City will continue to provide improvements to the facility inventory consistent with its current recreational activities to include adequate bicyclist/pedestrian trails, playgrounds and athletic fields.
- 4.2.4. The City will provide additional and replacement facilities identified in the Parks and Recreation Strategic Plan including the replacement of Highlander Pool, additional athletic fields and an additional or replacement dog park.

OBJECTIVE 4.3:

Continue to support public and private partnerships in the mutual use of parks and open space lands to meet recreational and environmental needs.

Policies:

- 4.3.1. The City will continue to initiate joint programs with the non-profit organizations, civic groups and private enterprise.
- 4.3.2. The City will continue, and augment where feasible, the partnerships and interlocal agreements between the City and other local municipalities and governmental entities which will allow for an increase in access to park and recreational facilities and programs.
- 4.3.3. The City will continue to support efforts to retain Major League Baseball Spring Training activities in Dunedin.
- 4.3.4. The City will continue to work with Pinellas County to maintain and improve the public recreational use of the Dunedin Causeway through infrastructure improvements.



[Image 6-6] Highlander Pool



[Image 6-7] Dunedin Highland Games



140



7

C H A P T E R S E V E N

SUPPORT SERVICES

Support Services

The support service element addresses the City's four publicly-provided utility systems which are divided into Subelements in this document:

- Sanitary Sewer,
- Potable Water and Natural Groundwater Recharge,
- Stormwater, and
- Solid Waste.

The purpose of this element is to identify and plan for the future demands of the City's utility services as well as providing policies for sustainable operation towards environmental stewardship. Additionally, the support services element addresses the maintenance and plans for improvements of each utility system to ensure quality service to its current and future citizens.

The following sections will provide the goals, objectives and policies of each of the Subelement as well as an overview of how the City will provide sustainable, quality operations for each utility system.



[Image 7-1] Dunedin Wastewater Plant



[Image 7-2] Dunedin Water Treatment Facility



[Image 7-3] Drainage Pipe for Stormwater



[Image 7-4] Solid Waste Truck



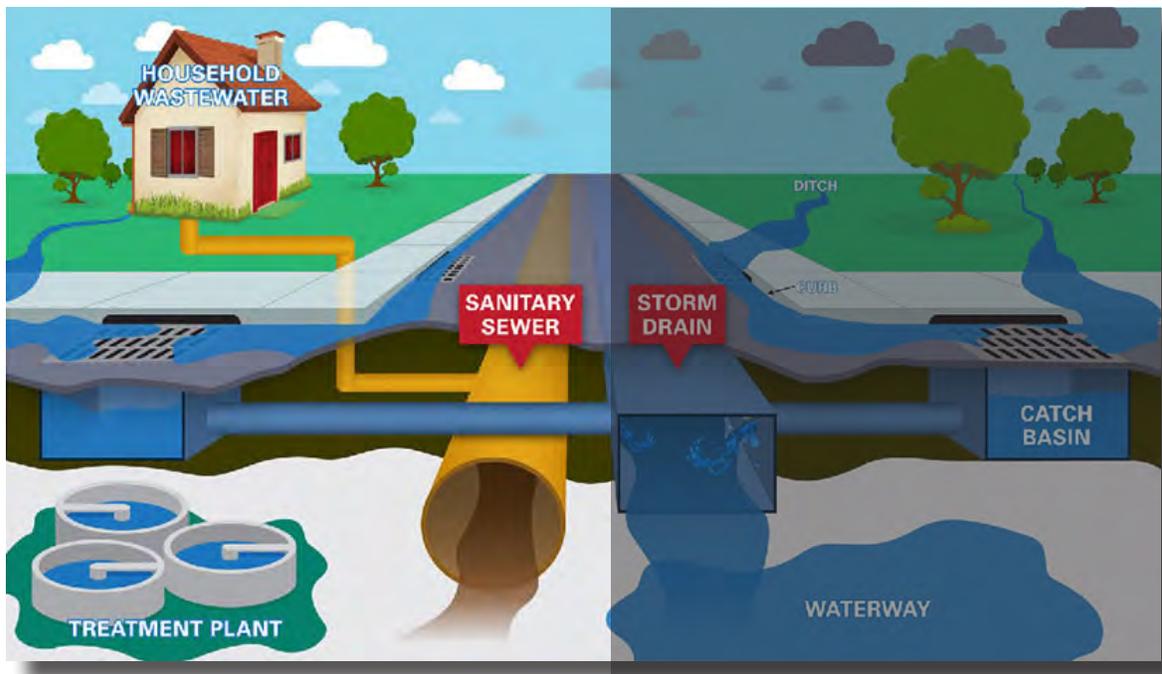
SUPPORT SERVICES
SANITARY SEWER

Sanitary Sewer

The City owns and operates a centralized Wastewater Treatment Plant that makes use of the A2O (Anaerobic/Anoxic/Oxic) treatment process, which involves anaerobic, anoxic and aerobic treatment with an internal recycle. The A2O process reduces the total suspended solids and biochemical oxygen demand content of the raw sewage, and it effectively removes nutrients such as phosphorus and nitrogen.

The wastewater treatment plant was built in 1991 and is designed to treat an average of 6.0 million gallons per day (MGD) with a peak flow of 12.6 MGD. It operates under Florida Department of Environmental Protection (FDEP) permit #FL0021326-004DWI. As the system is aging, maintenance, repairs, and rehabilitation projects are underway to update parts of the system and ensure their long-term reliability.

A portion of the treated wastewater is diverted to feed the reclaimed water system. Construction of the reclaimed distribution system began in 1991 and the first customer was connected in 1992. The development and use of the City's reclaimed water system has reduced the volume of fresh water discharged to St. Joseph Sound by 70 percent. This high quality treated wastewater is available for irrigation only. In addition, the use of reclaimed water for irrigation purposes has reduced the quantity of water pumped from the City's wellfield. Lower pumpage reduces the potential effect of local aquifer stress and salt-water intrusion.



[Image 7-5] Sanitary Sewer vs. Stormwater
Source: azstorm.org

Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- Florida Department of Environmental Protection (FDEP)
- Southwest Florida Water Management District (SWFWMD)
- Pinellas County

Plans, Programs/Initiatives:

- City of Dunedin Municipal Business Plan and Capital Improvements Plan
- City of Dunedin Code of Ordinances
- City of Dunedin Land Development Code

Related Maps:

- Map 7-1: Wastewater System
- Map 7-4: Reclaimed Water System





[Image 7-6] Wastewater Plant

GOAL 1 – Provide adequate sanitary sewer services to meet existing and future demands.

OBJECTIVE 1.1: Increase capacity or extend wastewater facilities concurrently with development.

Policies:

- 1.1.1. The City will permit development if adequate wastewater facilities are provided to serve the development.
- 1.1.2. The treatment capacity shall be maintained at 102 gallons per capita per day, not to exceed the permitted design of 6.0 million gallons per day, average daily flow. Infiltration/Inflow should not exceed 275 gpcpd, and improvements are continuing to further reduce this amount.
- 1.1.3. The Public Works Department will make the City’s Wastewater Atlas available to developers through the City’s website or by inquiry.



[Image 7-7] Lift Station

OBJECTIVE 1.2: Encourage property owners to abandon septic tanks on their properties and connect to the City sanitary sewer system if available.

Policies:

- 1.2.1. The City shall not issue new permits for septic tanks if sewer connection is available to the property.
- 1.2.2. The Public Works Department shall extend sewer lines to ensure that all incorporated City areas served by septic tanks are served by the wastewater system.
- 1.2.3. The City will continue to connect new commercial and industrial uses to the sanitary sewer system if available.



[Image 7-8] Sanitary Sewer Cover



OBJECTIVE 1.3:

Improve surface and groundwater quality when wastewater treatment and disposal alternatives are being developed.

Policies:

- 1.3.1. The City will adhere to all State and Federal regulations.
- 1.3.2. The City will continue to adhere to the City's Sewer Ordinance which requires properties to connect to the sanitary sewer system within 90 days of the availability of the system.

OBJECTIVE 1.4:

Improvements to wastewater collection and treatment facilities shall be programmed into the Capital Improvements Element of the Comprehensive Plan and Capital Improvement Program.

Policies:

- 1.4.1. The staff of the Wastewater Division shall annually assess the Capital Improvement Program.
- 1.4.2. The staff of the Wastewater Division shall set priorities for replacement, correcting wastewater facility deficiencies and providing for future facility needs based on urgency for need, funding availability and effectiveness at achieving levels of service.

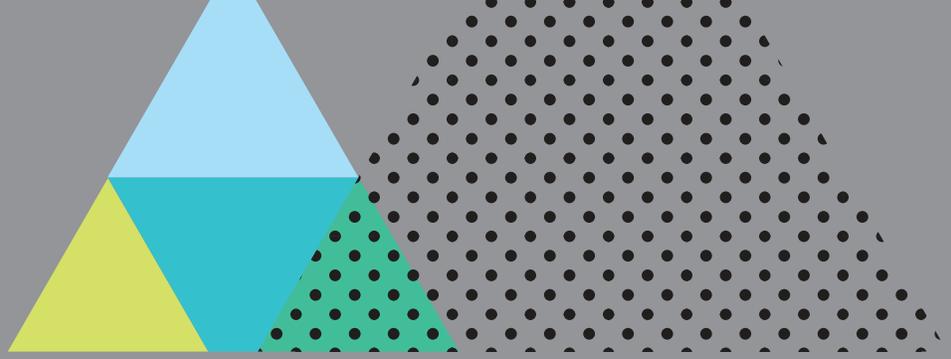
OBJECTIVE 1.5:

Reduce stormwater impacts on the sanitary sewer system.

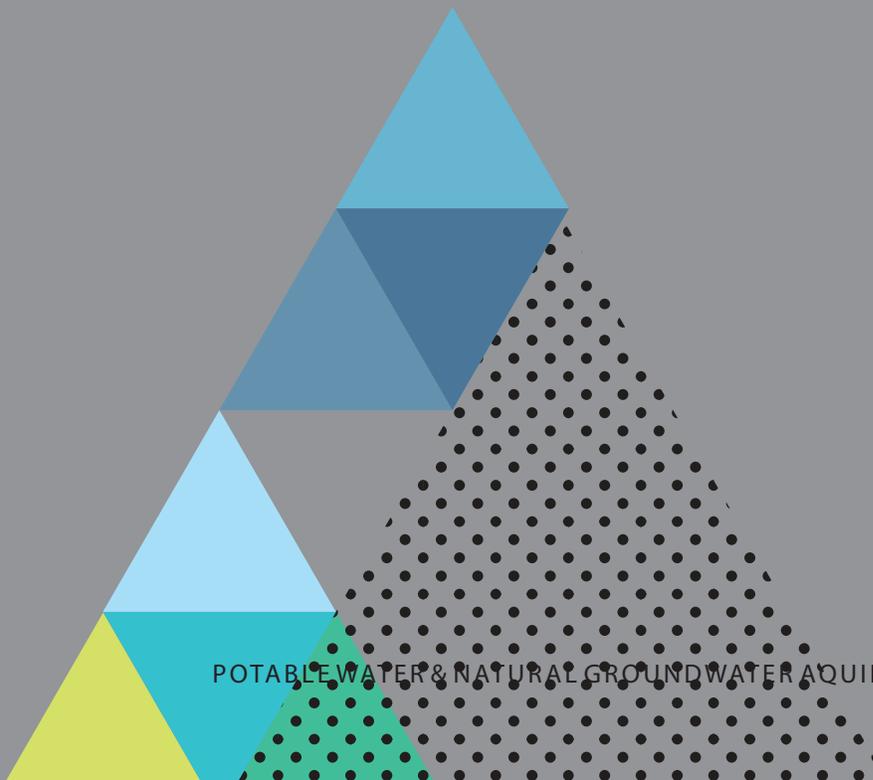
Policies:

- 1.5.1. The City shall ensure new infrastructure is installed according to best practices and comply with requirements of regulatory agencies.
- 1.5.2. The City will continue to repair, line and/or replace sanitary sewer mains that are failing and allowing infiltration of groundwater.
- 1.5.3. The City will continue to rehabilitate and line old manholes to prevent infiltration.
- 1.5.4. The City will install rain dishes at low points from preventing inflow.





SUPPORT SERVICES
POTABLE WATER
& NATURAL
GROUNDWATER
AQUIFER RECHARGE



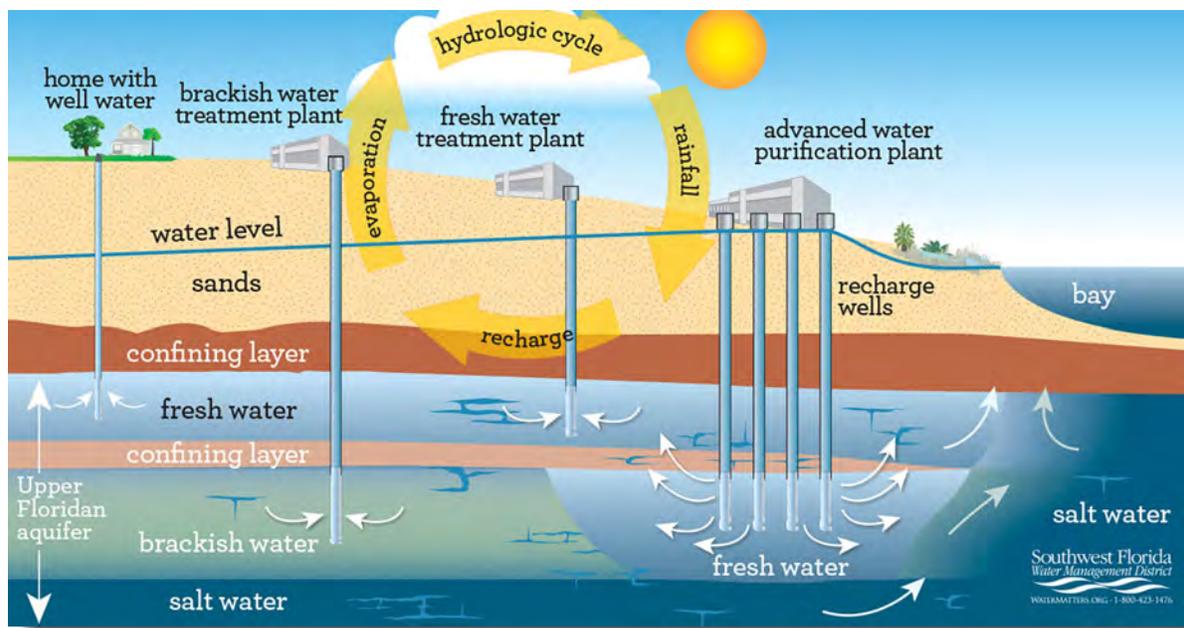
Potable Water & Natural Groundwater Aquifer Recharge

The focus of the City of Dunedin Potable Water & Natural Groundwater Aquifer Recharge Subelement is to continue good water management practices, advance the improvements planned for the water treatment plant and improve the distribution system. The City is one of the few communities in the County that maintains centralized treatment and management of their local water supply wells for meeting its population's potable needs. Due to the City's location next to the Gulf of Mexico, migration of salt water into its water supply wells has always been a concern.

Wells located throughout the City deliver raw water for treatment at the water plant, which ultimately provides the finished water quality that meets all State and Federal regulations related to the production of safe drinking water. All improvements to the wellfield and the raw water transmission system will continue to meet or improve these water quality standards.

Improvements and modifications to the City's Reverse Osmosis (RO) Water Treatment Facility are underway and will maintain or improve the water quality delivered to the City's population. As salt water intrusion is a possible concern, the RO facility will continue to allow for the treatment of water with elevated chloride concentrations or water that is brackish.

The City's underground pipe network consists of 7.5 miles of raw water transmission piping, four-2 million gallon ground storage tanks, one treatment plant, 1,300 fire hydrants, and approximately 138 miles of distribution piping ranging in size from 2 to 18 inches in diameter. As much of this piping is aging and their materials of construction are deteriorating, the City is replacing it with updated and more resilient materials.



[Image 7-9] Water Cycle

Source: Southwest Florida Water Management District

Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- U.S. Environmental Protection Agency (USEPA)
- Florida's Department of Environmental Protection (FDEP)
- Southwest Florida Water Management District (SWFWMD)
- Tampa Bay Water
- Pinellas County

Plans, Programs/Initiatives:

- Florida Water Star Program
- SWFWMD Regional Water Supply Plan
- Pinellas County Utilities' Potable Water Supply Facilities Work Plan
- SWFWMD Water Use Permit (City of Dunedin Water Supply Facilities Work Plan)
- City of Dunedin Municipal Business Plan and Capital Improvements Plan
- City of Dunedin Code of Ordinances
- City of Dunedin Land Development Code

Related Maps:

- Map 7-2: Potable Water System
- Map 7-3: Identified Wellhead Protection Areas
- Map 5-4: Soil Map
- Map 5-5: Hydrological Features





[Image 7-10] Well Station



[Image 7-11] Water Treatment Facility

GOAL 1 – Potable Water- Provide sufficient and quality amount of potable water to current and future users.

OBJECTIVE 1.1: Improve potable water quality and account for existing facility deficiencies.

Policies:

- 1.1.1. The City shall replace all deficient pipes and galvanized pipes that have a diameter of four inches or less.
- 1.1.2. The level of service for supply capacity shall be an average of less than 110 gallons per capita per day.
- 1.1.3. The level of service for storage capacity shall be the amount of potable water pumped for use during an average day.
- 1.1.4. The level of service for water quality shall meet or exceed all State and Federal Safe Drinking Water Act requirements, as amended.
- 1.1.5. The City shall clean water lines as needed.
- 1.1.6. The City will continue the well monitoring/abandonment project to address saltwater intrusion concerns.
- 1.1.7. The City will continue the existing program of backflow prevention in order to protect potable water wells from contamination.
- 1.1.8. The City will continue to address the incidences of rusty water from citizens.
- 1.1.9. The City will continue to inventory the existing facilities and upgrades in the “Water Distribution Management System” to track installation and scheduled replacement.
- 1.1.10. The City will continue to obtain appropriate permits to allow for the discharge of high chloride content water if development of brackish water supplies requires such modifications.



OBJECTIVE 1.2:
Maintain optimal water pressure.

Policies:

- 1.2.1. The City will loop water systems to eliminate dead ends to improve water flow.

OBJECTIVE 1.3:
Perform improvements to potable water facilities.

Policies:

- 1.3.1. The existing water treatment facility shall be upgraded and rehabilitated. These upgrades shall incorporate updated technology and increase water quality.

OBJECTIVE 1.4:
Encourage development in areas where there is adequate potable water capacity to discourage urban sprawl.

Policies:

- 1.4.1. The Public Works Department will make the City's potable water atlas available to developers through the City's website or by inquiry.

GOAL 2 – *Natural Groundwater Aquifer Recharge*-Protect, maintain and inventory natural groundwater aquifer recharge areas.

OBJECTIVE 2.1:
Restrict development on prime aquifer recharge areas.

Policies

- 2.1.1. The Zoning Code shall include provisions to protect prime aquifer recharge areas.
- 2.1.2. The purchase of prime aquifer recharge areas shall be studied and based on the public's interest, appropriateness and financial feasibility.

OBJECTIVE 2.2:

Implement the current drainage and zoning regulations to protect and enhance the functions of natural drainage features.

Policies:

- 2.2.1. The City will continue to encourage the use of erosion control tools in new development adjacent to identified surface water area.
- 2.2.2. The City will continue to maintain natural lakes, creeks, canals, and stormwater retention and conveyance features to protect the natural drainage features.

OBJECTIVE 2.3:

To encourage the protection of permeable open space in order to maintain recharge areas to the surficial aquifer.

Policies:

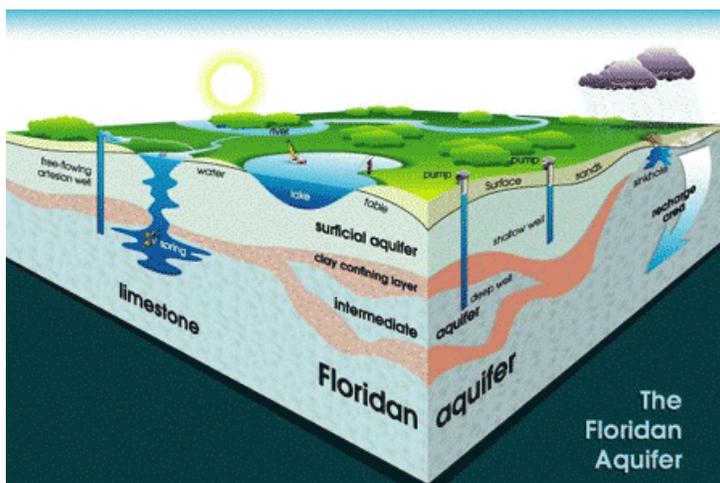
- 2.3.1. The City shall discourage variances from the impervious surface ratio requirement.
- 2.3.2. The City shall maintain existing preservation programs for the protection of permeable green open space.

OBJECTIVE 2.4:

Protect all natural groundwater recharge areas that have functional hydrological characteristics.

Policies:

- 2.4.1. The City shall continue to acknowledge the groundwater aquifer recharge as an important contributor to the hydrological process and adhere to all State and Federal regulations.



[Image 7-12] Florida Aquifer System
Source: <https://wec.ifas.ufl.edu>



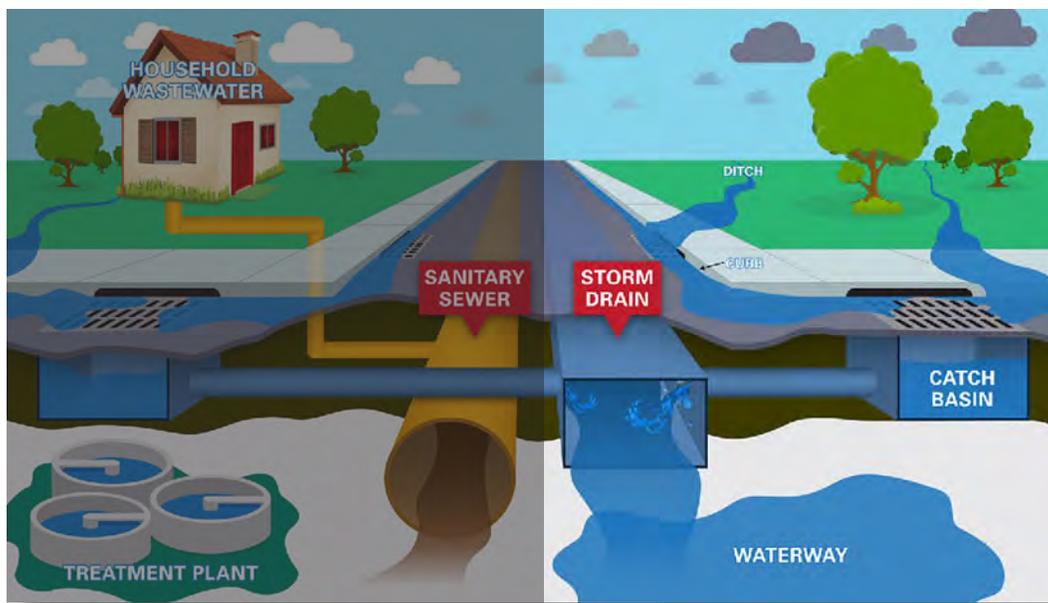
SUPPORT SERVICES **STORMWATER**



Stormwater

The focus of the City of Dunedin Stormwater Subelement is to establish a level of service threshold to protect people, property, and the environment. The City of Dunedin has identified Five Epic! Goals to help guide the City's future objectives, with two of these goals directly associated with Stormwater management. Epic! Goal number 3 is to "Promote Dunedin as The Premiere Coastal Community, protecting and improving our natural resources for the enjoyment of all. Epic! Goal number 4 is "Be the Statewide model for environmental sustainability stewardship". The Stormwater Subelement highlights the emphasis on the City of Dunedin drainage, natural systems and water quality compliance and improvement.

The City of Dunedin has taken a two- step approach to meeting the Stormwater related Epic! Goals. The first is to address Stormwater drainage issues throughout the City through preventive maintenance of Stormwater infrastructure, and implementation of major capital improvements identified in the City's Stormwater Master Plan and the Curlew Creek Watershed Management Plan. The second function is to implement the regulatory requirements set forth in the National Pollutant Discharge Elimination System (NPDES) Municipal Separate Storm Sewer System (MS4) Federal Permit issued by the Environmental Protection Agency (EPA). Multiple inspections are performed annually to ensure the maintenance of swales, ditches, ponds, catch basins, grates, inlets, storm drains and other conveyance and treatment infrastructure. In addition, inspections and maintenance of treatment infrastructure, street-sweeping of City residential and collector/arterial roadways, aquatic weed control of ditches and channels, and inspections of City-owned ponds/lakes are routinely performed to identify illicit discharges into the City Stormwater system. The Stormwater Section also provides pre-disaster support, and post disaster emergency response and debris removal.



[Image 7-13] Sanitary Sewer vs. Stormwater
Source: azstorm.org

Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- U.S. Environmental Protection Agency (USEPA)
- U.S. Army Corps of Engineers
- Florida's Department of Environmental Protection (FDEP)
- Florida Stormwater Association (FSA)
- Southwest Florida Water Management District (SWFWMD)
- Florida Department of Transportation (FDOT)
- Tampa Bay Regional Planning Council (TBRPC)
- Keep Pinellas Beautiful
- Pinellas County

Plans, Programs/Initiatives:

- Clean Water Act
- National Pollutant Discharge Elimination System (NPDES)
- National Flood Insurance Program (NFIP)
- Community Rating System (CRS)
- City of Dunedin Stormwater Master Plan
- City of Dunedin Municipal Business Plan and Capital Improvements Plan
- City of Dunedin Code of Ordinances
- City of Dunedin Land Development Code

Related Maps:

- Map 7-5: Major Drainage Basins & Facilities
- Map 5-3: Topographic Map
- Map 5-5: Hydrological Features





[Image 7-14] Stormwater Runoff
Source: Caltrans

GOAL 1 – *Water Quality*- Preserve or improve water quality through the implementation of best management practices in a comprehensive watershed management approach.

OBJECTIVE 1.1:

Prioritize the preservation of natural drainage systems in the drainage project design and implementation. Net benefit water quality improvements will be implemented through redevelopment and capital improvement program.

Policies:

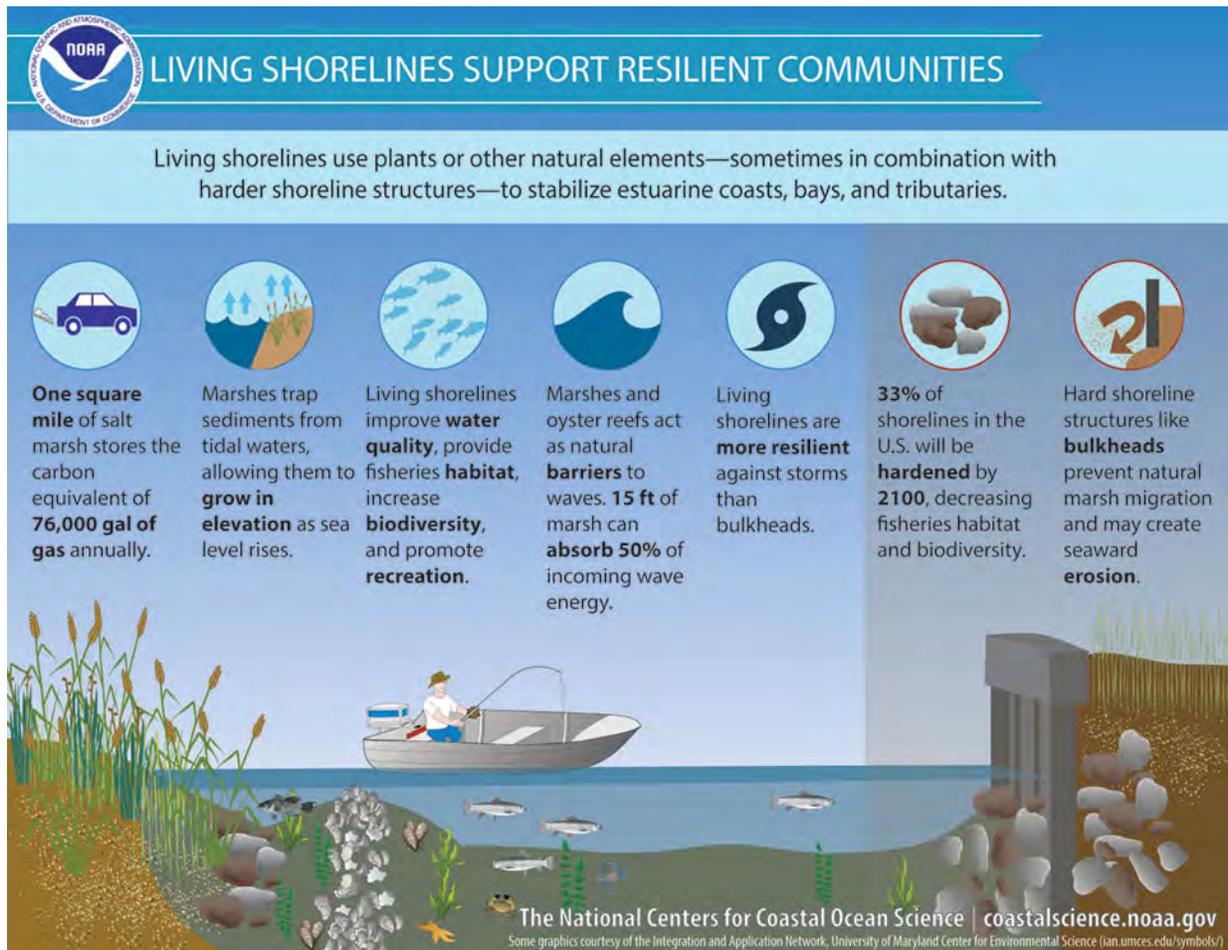
- 1.1.1. The City shall protect and treat wetlands as effective natural storage and treatment areas.
- 1.1.2. The City will preserve the natural course of drainage ways.
- 1.1.3. The City will discourage alterations to the existing drainage system, such as channelization of natural drainage ways, in areas of new construction.
- 1.1.4. The City shall pursue a “net benefit” approach to improving water quality.

OBJECTIVE 1.2:

Approach stormwater management in a holistic fashion by incorporating flood protection and preserving natural systems.

Policies:

- 1.2.1. Where appropriate, the City will implement practices that will include a comprehensive watershed management planning approach which include flood protection, stormwater quality, erosion and sedimentation control.
- 1.2.2. The City will incorporate living shorelines in the design of capital improvement projects for erosion and sediment control and ecological habitat benefits.
- 1.2.3. The City shall provide maintenance and inspections for drainage systems to ensure compliance with the Community Rating System (CRS).



[Image 7-15] Living Shorelines

Source: NOAA

GOAL 2 – Flood Protection- Reduce localized flooding and the potential for property damage through an integrated stormwater utility maintenance and capital improvement program.

OBJECTIVE 2.1:

Improve drainage deficiencies on both public and private lands for flood control and integrate net benefit water quality goals.

Policies:

- 2.1.1. All public projects undertaken to improve drainage deficiencies shall be in conformance with the capital improvement program as approved annually by the City Commission.
- 2.1.2. The City will implement the recommendations found in the Stormwater Master Plan regarding flood control and water quality.
- 2.1.3. The City will conduct a vulnerability assessment and implement the recommendations as a result of the assessment.
- 2.1.4. The City will provide multifunctional stormwater facilities in flood risk areas and encourage developers to provide multifunctional stormwater facilities within their projects (i.e. park pavilion to be used for detention ponds as well).
- 2.1.5. The level of service for flood control shall be of the following:
 - 2.1.5.1. Stormwater Management Systems that discharge to open drainage systems shall be designed for a 25-year/24-hour storm event.
 - 2.1.5.2. Stormwater Management Systems that discharge to closed drainage basins shall be designed for a 100-year/24-hour storm event.
- 2.1.6. The level of service for water quality shall include outlet structures sized or designed to convey the specified release rate or allowable predevelopment runoff rate from the contributing area, which outlet also shall provide for the retention or detention with filtration of the first half inch of runoff (or, as an option, the runoff from the first one inch of rainfall) for not more than 72 hours for water quality purposes as referenced in the Code of Ordinances, Section 78-457.(a)(5)e.



GOAL 3 – *Regulatory Compliance*- Achieve and maintain compliance with the national pollutant discharge elimination system (NPDES) permit, municipal separate storm sewer system (MS4) requirements, total maximum daily load (TMDL) allocations, and other regulatory programs.

OBJECTIVE 3.1:

Address water quality protection, pollution control and attainment of water quality standards through the implementation of capital improvement program, development review and regulations.

Policies:

- 3.1.1. The City shall maintain a program in which all Federal, State and local water quality standards will be adhered to in order to protect or improve surface and groundwater from development.
- 3.1.2. All drainage improvements made to existing drainage facilities will include a provision for cost-effective water quality improvements.
- 3.1.3. The City shall implement necessary best management practices by working with other public and private stakeholders for Total Maximum Daily Load (TMDL) requirements.
- 3.1.4. The City shall implement the recommendations of the Stormwater Master Drainage Plan for the pretreatment of stormwater.
- 3.1.5. The City shall eliminate the discharge of inadequately treated wastewater and stormwater runoff into the waters owned or regulated by the State of Florida.



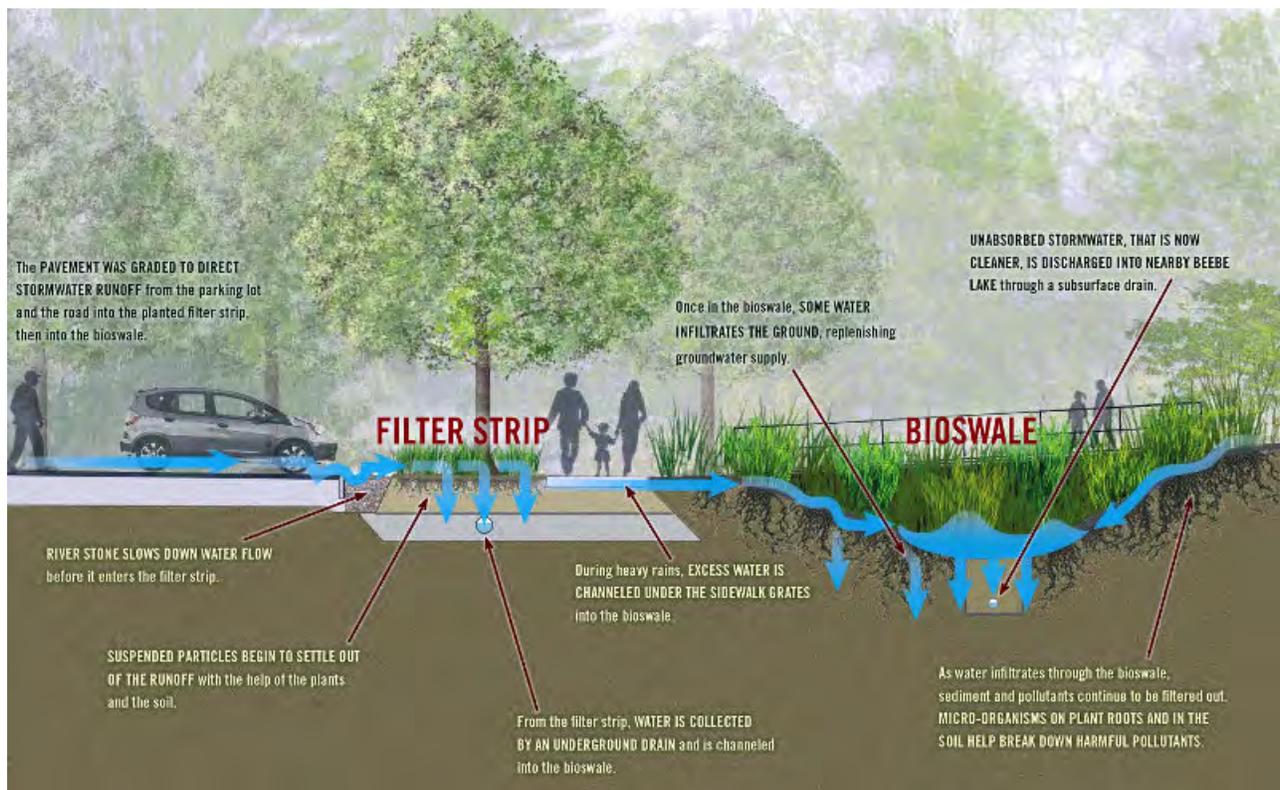
GOAL 4 – *Low-Impact Development*- Promote sustainable, low-impact development infrastructure in future development and redevelopment projects.

OBJECTIVE 4.1:

Encourage sustainable infrastructure design for development and redevelopment projects located in flood zones.

Policies:

- 4.1.1. The City will continue to direct development away from FEMA flood zones.
- 4.1.2. The City will continue to assess the life span of City development projects according to Sea Level Rise and Storm Surge Vulnerability.
- 4.1.3. The City will provide design guidelines for green infrastructure and low-impact development in the Land Development Code for projects located in flood zones.
- 4.1.4. The City will continue to consider sustainability in the design for implementation and management of infrastructure.



[Image 7-16] Green Infrastructure
Source: cornellplantations.org



SUPPORT SERVICES
SOLID WASTE



Solid Waste

Municipal Solid Waste (MSW) refers to the garbage that is produced as a result of human activities that is discarded as unwanted and useless. MSW is generated from industrial, residential and commercial activities in a given area, and can be handled in a variety of ways depending on the type of waste. The proper removal and disposal is vital to human and environmental health. The Solid Waste Division provides regular collection services for Dunedin residents and businesses. Residential MSW and household recyclables are collected curbside in 90-gallon automated rolling carts; Commercial MSW is collected by metal dumpsters, open top roll-off containers, and compactors. The Division also manually collects yard & bulk waste curbside from residential customers.

The Division contracts with a private entity for the curbside collection of recyclables and the servicing of two recyclables drop-off collection centers. The Division staff consists of 21 full-time equivalent (FTE) employees and contains three operating programs: Administration, Residential, and Commercial. The Administration program prepares and maintains the Division budget; develops and operates new and existing services; produces its community outreach & education programs; maintains inventories; and handles the day-to-day customer service support. The Residential program provides collection services for residential garbage, recyclables, and yard & bulk waste. The Commercial program provides collection services with large dumpsters, temporary dumpsters and permanent open-top and compactor containers, and other miscellaneous special collection services.



[Image 7-17] Residential Solid Waste Pick Up

The process and procedures for handling the City's MSW should be detailed in a solid waste management plan, and should be performed in a manner that is environmentally compatible, economically low-cost, and aesthetically pleasing and energy efficient. The City's residential and commercial MSW is transported to the Pinellas County Waste-to-Energy Facility. The Waste-to-Energy (WTE) plant burns garbage, reducing its volume by 90 percent. This means there is less material to go in the landfill. This facility is designed to incinerate the MSW, as delivered, with little, if any, pre-processing. Combustion takes place in three large enclosed boilers where contained water is heated to form steam, which is then used to drive a large turbine to generate electricity. Both ferrous (steel) and non-ferrous (aluminum) metals are recovered from the ash by using magnets and eddy currents. Revenues from the sale of electricity help reduce the facility's operational costs, and the recovered metals are sold to smelters for recycling and the ash is used for landfill cover. The residential yard & bulky waste materials are collected and transported to a transfer station where Class III and construction & debris (C & D) is disposed and processed for recovery. The City's residential recycling contractor is responsible for collecting, transporting, processing, and marketing of recyclable materials. The recyclable materials are transported to an intermediate transfer site nearby then transported to a Materials Recovery Facility (MRF) outside Pinellas County.



[Image 7-18] Solid Waste Operation

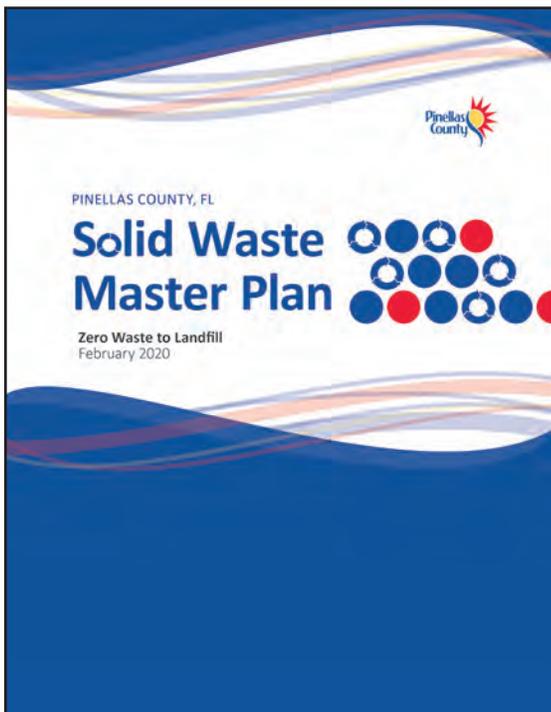
Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

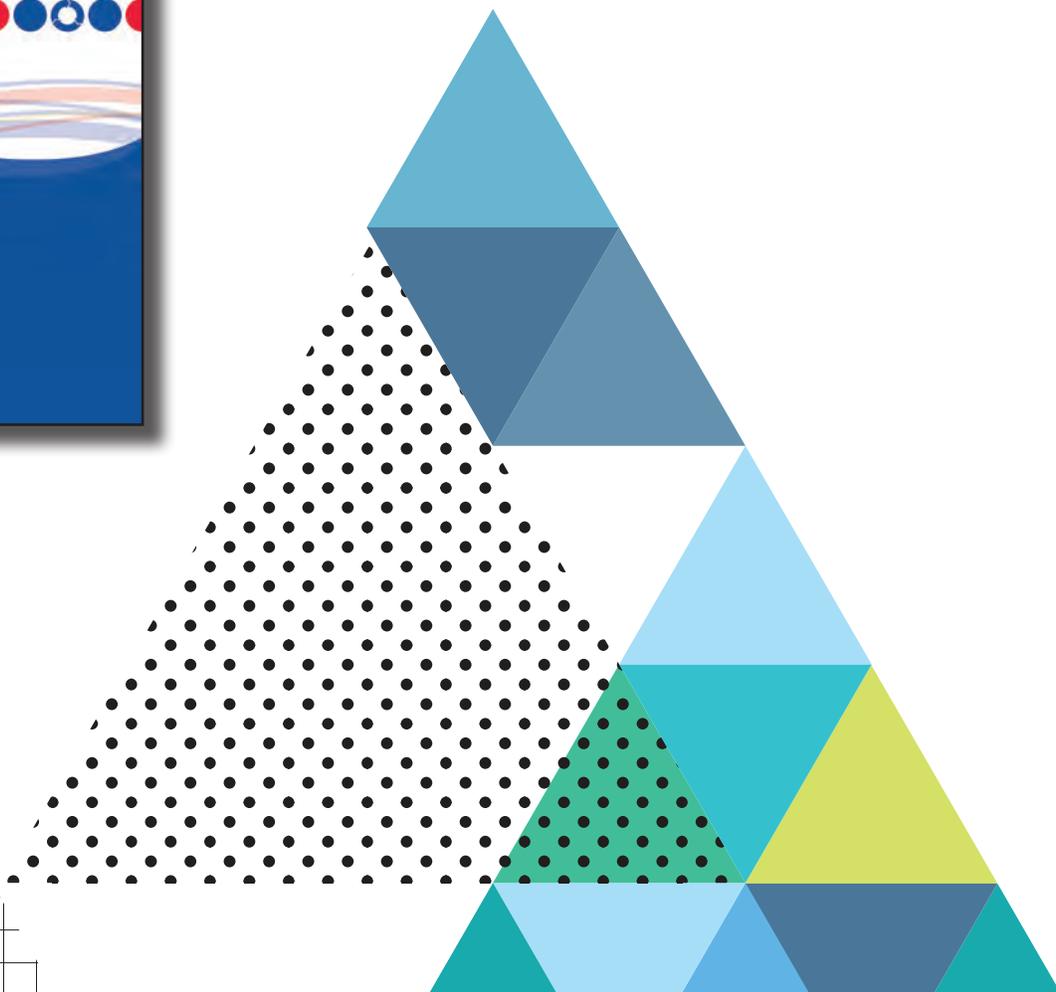
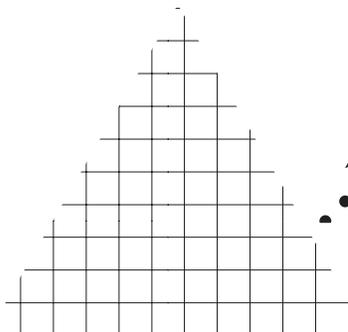
- U.S. Environmental Protection Agency Waste Wise
- Solid Waste Association of North America (SWANA)
- Recycle Florida Today (RFT)
- Pinellas County

Plans, Programs/Initiatives:

- Pinellas County Solid Waste 30 Year Master Plan
- City of Dunedin Municipal Business Plan and Capital Improvements Plan
- City of Dunedin Code of Ordinances
- City of Dunedin Land Development Code
- Earth Day Celebrations
- Recycling Education



[Image 7-19] Pinellas County Solid Waste Master Plan
Source: Pinellas County





[Image 7-20] Recycling Drop-off

Florida has a recycling problem. ...but Dunedin can be part of the solution!

Floridians are contaminating curbside and drop-off recycling bins with materials that are not meant to be there — mainly plastic bags, yard debris, tangles, like cords and hoses; and clothing. *Contamination drives costs up and spoils good recycling efforts.*

Let's **RETHINK** what we recycle and **RESET** our behavior to focus on recycling the basics at curbside... *and drop off sites.*

Back To The Basics

Focus on recycling these items in the City's Recycling Program:

- Aluminum & Steel Cans
- Plastic Bottles & Jugs
- Cardboard & Paper
- Glass Bottles & Jars

When in doubt — throw it out!

DUNEDIN
Home of Honeymoon Island
Solid Waste & Recycling
727-298-3215

rethink. reset. recycle.
FloridaRecycles.org

[Image 7-21] Recycling Guidelines

GOAL 1 – Collect, recycle and dispose of all solid waste generated responsibly.

OBJECTIVE 1.1:

Adhere to all of the goals, objectives and policies contained in the Pinellas County Solid Waste and Resource Recovery Element of the Comprehensive Plan.

Policies:

- 1.1.1. The City will continue to coordinate with intergovernmental agencies with waste management efforts.

OBJECTIVE 1.2:

Analyze and correct capacity and deficiencies annually.

Policies:

- 1.2.1. The City shall address collection deficiencies through the acquisition of additional collection vehicles or route restructuring.
- 1.2.2. Priority shall be given to the acquisition of collection vehicles in a timely and cost-effective manner.
- 1.2.3. The purchase of new solid waste collection and recycling equipment shall be programmed into the capital improvement program.
- 1.2.4. The level of service shall be to collect up to 1.301 tons per year per capita (average of 4.4 pounds per capita per day).

OBJECTIVE 1.3:

Expand the recycling program to further reduce landfill and resource recovery plant volumes.

Policies:

- 1.3.1. The City will expand the City's processing agreement to account for more recyclables from multifamily and commercial recycling dwelling in the City.
- 1.3.2. The City will continue its backyard compost program and will continue to explore industrial composting options with Pinellas County and regional organizations.
- 1.3.3. The City will continue education and outreach efforts to inform citizens and businesses of the single-stream recycling guidelines to minimize contamination in the collected recyclables.



C H A P T E R E I G H T

CAPITAL
IMPROVEMENTS
ELEMENT

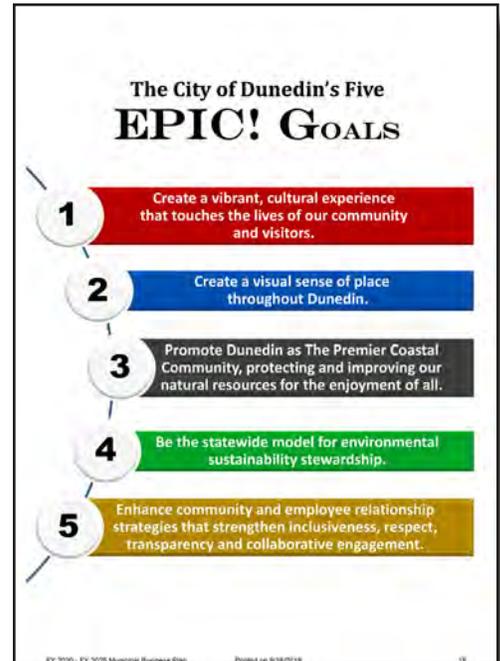
Capital Improvements

The Capital Improvement Plan (CIP) is a multi-year plan that identifies the specific steps the City will take to ensure the provision of new and existing facilities and services. The CIP identifies capital outlay and major operating expenses over the next six years. Each project is aligned with one or more Epic! Goals of the Municipal Business Plan.

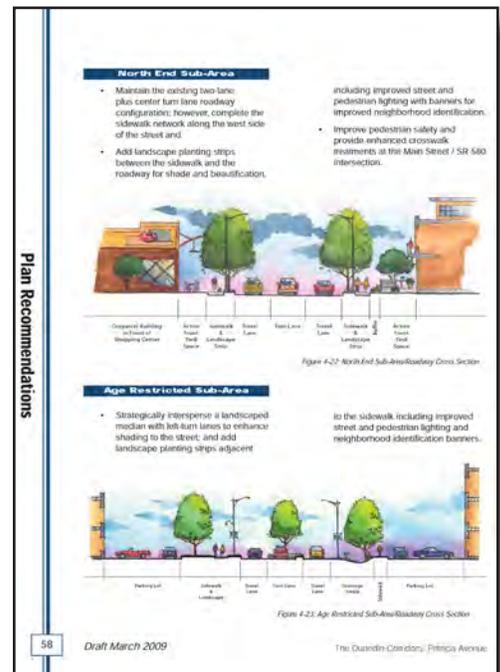
Scope of the CIP

Projects included in the City's CIP are anticipated to cost \$25,000 or more and/or will last five or more years. All City departments are involved in the collaborative effort of proposing and developing project scopes. The leadership team then reaches a consensus on the CIP schedule based on fund availability, project timing and alignment with the Epic! Goals. To determine inclusion, priority and implementation of the CIP, projects are selected based on the alliance with one or more of the following guiding principles:

- Economic Development**
 Diversify the local economy by encouraging entrepreneurship, small business growth, corridor enhancements and mixed-use projects to increase the City's tax base, create jobs and generate revenue. Work with the private real estate community and Dunedin's local partners to provide housing opportunities that support economic growth and produce housing for all income ranges.
- Fiscal Sustainability**
 Ensure the fiscal sustainability of the City in order to meet the needs of the community in the delivery of services.
- Build, Restore and Maintain Infrastructure**
 Improve upon infrastructure reliability through sound stewardship principles and practices, which must be compatible with our environment and scalable to the realities of fiscal constraints.
- Internal Operations & Services**
 Create new policies to address future service needs based on standards, best business practices, legal safe guards and fiscal sustainability. Embrace emerging technologies and trends in government to leverage operational efficiencies.



[Image 8-1] Epic! Goals



[Image 8-2] Patricia Avenue Corridor Study

- **Community Relationships**

Employ a proactive strategy of effective communication by building positive and authentic relationships within the community with key individuals, groups and organizations. Make sensitivity to community concerns and issues pervasive throughout the organization by incorporating citizen engagement tools that utilize technology, ensuring public feedback is easily accessible to all.

- **Human Resources**

Nurture and retain a highly qualified, professional and diverse workforce that is service-driven, responsive and effective in accomplishing the mission of the City.

- **Environmental Sustainability**

Take a leadership position in environmental stewardship by promoting green building, converting to solar energy, adopting a climate action plan, maintaining a green city rating and integrating sustainability into City operations.

Based on the direction of the City Commission, the first year of the CIP comprises a majority of the City's capital budget for the upcoming fiscal year. The CIP is reviewed annually, with the slate of projects for the upcoming fiscal year being refined and projects in future years programmed as they are identified.

The Capital Improvements Element provides a long-term strategy to guide the City toward a fiscally sustainable capital budget to maintain or improve the levels of service of public services as well as expand and revitalize public facilities.



Intergovernmental Partners and Stakeholders' Plans, Programs/ Initiatives

Partners and Stakeholders:

- Florida Department of Transportation (FDOT)
- Southwest Florida Water Management District (SWFWMD)
- Tampa Bay Regional Planning Council (TBRPC)
- Forward Pinellas (PPC/MPO)
- Pinellas County School Board (PCSB)
- Pinellas County

Plans, Programs/Initiatives:

- FDOT Five-Year Work Program
- Forward Pinellas 2045 Long-Range Transportation Plan (LRTP)
- Forward Pinellas Transportation Improvement Plan (TIP)
- Pinellas County School Board Five-Year Work Plan
- City of Dunedin Municipal Business Plan and Capital Improvements Plan
- City of Dunedin Code of Ordinances
- City of Dunedin Land Development Code
- City of Dunedin Community Redevelopment Agency Downtown Master Plan 2033
- City of Dunedin Bicycle and Pedestrian Master Plan
- City of Dunedin Douglas Avenue Corridor Study
- City of Dunedin Patricia Avenue Corridor Study
- City of Dunedin Causeway Corridor Report
- City of Dunedin State Road 580 Corridor Study



[Image 8-3] Give Me Shelter-
Artistic Bus Shelter

GOAL 1 – Take necessary actions to provide public facilities to enhance quality of life of citizens, protect investments in existing facilities, and maximize the use of existing facilities.

OBJECTIVE 1.1:

Correct existing deficiencies, replace worn out or obsolete facilities, and accommodate desired future growth by providing capital improvements.

Policies:

- 1.1.1. The Capital Improvements Element shall address those projects in the Future Land Use, Transportation, Housing, Conservation, Coastal Management, Recreation and Open Space, Support Services Elements of the Comprehensive Plan.
- 1.1.2. The City shall set priorities, schedules and fund all budgeted and/or approved capital improvement projects based on the following:
 - 1.1.2.1. Protection of public health, safety and welfare as well as improve the quality of life of the citizens; fulfill a legal commitment to provide facilities; preserve or achieve full use of existing facilities; or to correct an existing deficiency.
 - 1.1.2.2. Increase the efficiency of existing facilities, prevent or reduce future improvement costs, provide services to developed areas lacking full service or promote infill development and/or redevelopment.
 - 1.1.2.3. Logical extension of facilities within a designated service area.

OBJECTIVE 1.2:

Maintain the adopted levels of service by assessing impact fees that are proportionate to the cost of facility improvements to future development.

Policies:

- 1.2.1. The City shall continue to participate in the Countywide Multimodal Impact Fee Ordinance.
- 1.2.2. The City shall continue to implement its impact fee ordinances to finance public facility improvements necessitated by development and to adequately maintain adopted level of service standards.

OBJECTIVE 1.3:

Coordinate land use decisions and manage its fiscal resources to ensure the provision of needed capital improvements for previously issued development permits and for future development and redevelopment.

Policies:

- 1.3.1. The City shall continue to adopt an annual Municipal Business Plan & Capital Improvements Plan as a part of its budgeting process.
- 1.3.2. The projects listed in the Capital Improvements Element shall be consistent with the proposals of the annual Municipal Business Plan & Capital Improvements Plan. The projects shall be consistent with the implementation and maintenance of the levels of service defined by the Comprehensive Plan.
- 1.3.3. Efforts shall be made to secure grants and private funds whenever available to finance the provisions of capital improvements.
- 1.3.4. Fiscal policies of the City to direct capital improvement expenditures shall be consistent with the goals, objectives and policies of the Comprehensive Plan.
- 1.3.5. Debt service shall be evaluated annually to ensure a healthy debt structure. Indicators shall include but not be limited to the following:
 - 1.3.5.1. The City's ratio of total annual governmental debt service to total annual governmental revenues shall not exceed the percent referenced in the debt policy.
 - 1.3.5.2. The City's maximum ratio of outstanding capital debt service to the property tax base shall not exceed the percent referenced in the debt policy.
- 1.3.6. The majority of outstanding capital indebtedness shall be managed through the appropriate funds such as the City's enterprise funds, one cent sales tax fund, and development impact fees to minimize impacts on the property tax base.
- 1.3.7. In the event of a failure of a referendum on which the City bases projected capital expenditures in the Capital Improvement Program, the City will take the following actions:
 - 1.3.7.1. Investigate the raising of other rates which may be utilized for the affected projects;
 - 1.3.7.2. Investigate deletion of non-essential affected projects;
 - 1.3.7.3. Investigate the modification of the level of service standard; or
 - 1.3.7.4. Investigate other appropriate revenue sources such as assessment districts, sales tax, user fees and fund transfers.





[Image 8-4] Strategic Planning Engagement 2018

**OBJECTIVE 1.4:
Coordinate development policies of the Comprehensive Plan, land development regulations and availability of necessary public facilities needed to support development prior to issuance of development order permits.**

Policies:

- 1.4.1. Public facilities and services shall meet or exceed the established levels of service and be made available for new development. A developer shall assure that all public services and facilities are in place prior to issuance of certificate of occupancy or that the following shall apply for the provision of services to new development:
 - 1.4.1.1. Wastewater, stormwater, and solid waste facilities shall be in place and available to serve the new development no later than the issuance of a certificate of occupancy.
 - 1.4.1.2. Adequate potable water supply shall be in place and available to serve new development no later than the issuance of a certificate of occupancy. Prior to approval of a building permit, the City shall assure there is adequate water supply to serve the new development and will be made available no later than the anticipated date of the City's issuance of a certificate of occupancy.
- 1.4.2. The City shall continue the implementation of Chapter 106 of the Land Development Code to ensure that adequate public facilities capacity is concurrent with the impacts of a proposed development.

- 1.4.3. Proposed plan amendments and requests for new development and redevelopment shall be evaluated according to the following criteria:
 - 1.4.3.1. Consistency with the goals, objectives and policies of the Comprehensive Plan and of other applicable plans of County, regional, State or special district agencies.
 - 1.4.3.2. Demand for public facilities that are included in the Comprehensive Plan.
 - 1.4.3.3. Conformance with Regulatory Future Land Use Plan Map.
 - 1.4.3.4. Adopted levels of service are maintained according with the Comprehensive Plan.
- 1.4.4. The City will continue to apply the provisions of Chapter 104, "Development Agreements," of the Land Development Code, governing permitted uses of land, public facilities servicing and impacted by development, public land dedication, permit requirements and regulatory consistency.

OBJECTIVE 1.5:

Continue the coordination of level of service standards for public facilities operated and maintained by State, regional, County or local entities.

Policies:

- 1.5.1. The timing, location and capacity of public facilities as reviewed in annual capital improvements budget shall be coordinated with other service providers to ensure that required services will be available when needed and to minimize utility disruptions.

OBJECTIVE 1.6:

Coordinate with the School District, to provide the opportunity for high student achievement through the availability of high quality public educational facilities.

Policies:

- 1.6.1. The City of Dunedin shall coordinate with the School District and local government partners in accordance to the executed Public Schools Interlocal Agreement.





9

C H A P T E R N I N E

PROPERTY RIGHTS ELEMENT

Property Rights

In 2021, the Florida Legislature passed House Bill 59, Chapter 2021-195, Laws of Florida, which amended section 163.3177, Florida Statutes to require that each local government adopt a "Property Rights Element" into its Comprehensive Plan.



[Image 9-1] South Dunedin Character Overlay District Workshop

GOAL 1 – Consider all relevant information including that of the property owner and the public health, safety, and welfare.

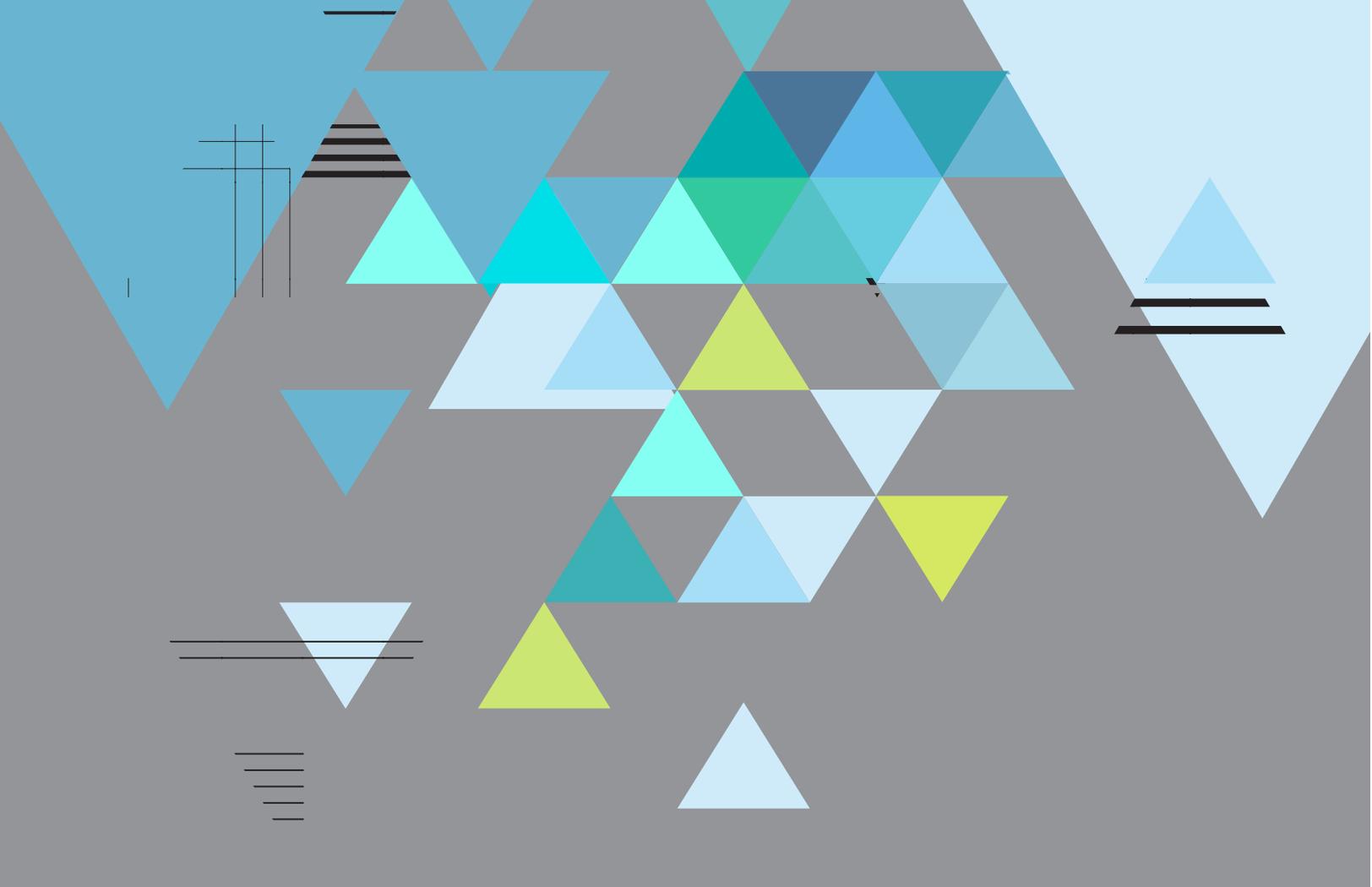
OBJECTIVE 1.1:

Ensure private property rights are considered in local decision-making. The following statements of private property rights shall be considered in local decision making:

Policies:

- 1.1.1. The City will consider in its decision-making the right of a property owner to physically possess and control his or her interests in the property, including easements, leases, or mineral rights.
- 1.1.2. The City will consider in its decision-making the right of a property owner to use, maintain, develop, and improve his or her property for personal use or for the use of any other person, subject to state law and local ordinances.
- 1.1.3. The City will consider in its decision-making the right of a property owner to privacy and to exclude others from the property to protect the owner's possessions and property.
- 1.1.4. The City will consider in its decision-making the right of a property owner to dispose of his or her property through sale or gift.





PLAN IMPLEMENTATION, EVALUATION & APPRAISAL

PLAN IMPLEMENTATION, EVALUATION & APPRAISAL

PLAN IMPLEMENTATION

The key implementation actions of the Comprehensive Plan are laid out in the strategies of each of the element. Some of these strategies will result in direct implementation tasks and others will result in the development and/or on-going management of programs. These programs will range from the revision of Dunedin's Land Development Code (LDC) to annual adoption of the Five-Year Capital Improvements Program (CIP). The development, implementation and management of Community Redevelopment Area (CRA) Plans, Special Area Plans (SAPs), corridor studies and the City's annual Business Plan Initiatives are also an important part of the implementation of the Comprehensive Plan goals, objectives and policies. Furthermore, the Comprehensive Plan will support the City's Five Epic! Goals.

MONITORING AND EVALUATION REQUIREMENTS

At least every seven years, pursuant to Rule Chapter 73C-49, Florida Administrative Code, the City will determine whether the need to amend the Comprehensive Plan exists to reflect changes in State requirements since the last time the Comprehensive Plan was updated. The City will notify the State Land Planning Agency by letter of this determination. If the City determines amendments to the Comprehensive Plan are necessary, the City will prepare and transmit the proposed amendments to the State Land Planning Agency within one year of such determination.

PUBLIC PARTICIPATION

Since 2008, the City studied and started implementing recommendations for all five of its commercial corridors. Dunedin also conducted its second citywide visioning exercise in August 2017. The exercise was an intensive community outreach with over 500 residents participating in 14 public sessions held over five days. The consultant-led visioning consisted of the following:

1. Preference survey,
2. SWOT analysis, and
3. Map exercise.

The public feedback and final Visioning Report recommendations are an important part of this Comprehensive Plan update.

City staff also engaged many of Dunedin's 28 advisory boards and committees to review and comment on relevant draft of the Comprehensive Plan elements. The technical expertise represented by the resident volunteers on the boards and committees will be used for any future amendment of the *Strive for 2035* Dunedin Comprehensive Plan and will also help represent the public in the evaluation and appraisal process.





The Dunedin Local Planning Agency (LPA) will have the responsibility of holding the initial public hearings on all amendments to the Comprehensive Plan and Future Land Use Map. All proposed Comprehensive Plan amendments will be heard by the LPA. In addition to hearing amendments, the LPA will be responsible for reviewing and making recommendations on changes to the LDC. The City Commission will review and make determinations on all proposed Comprehensive Plan amendments. The City Commission is the final decision-making authority for the implementation and amendment to the Comprehensive Plan.

Moving forward, the City staff will take a lead role in facilitating the implementation of the *Strive for 2035* Comprehensive Plan by recommending LDC amendments, implementing corridor plans, completing Business Plan initiatives, managing the CIP, and aligning development review with the Plan's goals, objectives and policies.

BASELINE DATA

The City utilized a wide variety of data, reports, studies and plans to generate the *Strive for 2035* Comprehensive Plan update. The list of resources can be found in Appendix A of the plan and are adopted by reference to support the goals, objectives, and policies of each plan element. The City staff will maintain and update the resources in Appendix A on a regular basis to prepare future plan amendments as necessary from changing conditions.

MEASURABLE OBJECTIVES

Based on the public participation process described previously, the City will review the implementation of the objectives through the measurements provided on an annual basis. This will allow the City to periodically evaluate its progress toward meeting community needs and desires. It is important for the City to understand its areas of successes and problems in the planning and implementation process and, based upon this appraisal, to make any necessary adjustments to either the Comprehensive Plan or the implementation process.

ACCOMPLISHMENTS AND PROBLEMS

With an annual review process established, the City will be able to determine whether its goals, objectives and policies are being accomplished, or if there are problems in achieving them. In cases where problems have developed, the City will review the goals, objectives, and policies and the implementation process, and determine if they need to be amended or if the implementation strategies need to be modified to address the problems. By reviewing the Comprehensive Plan for both its positive and negative aspects, the City will be able to ensure that it is a relevant document to the community.

AMENDMENTS

The Comprehensive Plan is not a static document. Concerns will arise in implementing the Plan, and some of the concerns will be the result of changing community values or changing goals, objectives, and policies. To remedy the concerns and maintain the relevance of the Comprehensive Plan, amendments to the Plan will be necessary. Amendments to the Comprehensive Plan will be discussed and conducted in the appropriate public forum. Amendments to the Comprehensive Plan will focus on how to make the document better and continue to reflect Dunedin's vision moving forward.

CONTINUOUS MONITORING

To ensure continuous monitoring, the City will link the goals, objectives and policies of the Comprehensive Plan to the annual City Commission strategic planning meeting, the annual list of business plan initiatives, the CIP, corridor study implementation and other City plans. By requiring this linkage, the Comprehensive Plan will be institutionalized into the City's decision-making process.



APPENDIX

Maps
Definitions/Acronyms
Related Plans & Programs
Related Agencies and Organizations
FY 2020 Capital Improvements Plan by Fund





APPENDIX MAPS



Maps by Element

Future Land Use Element

- Map 1-1: Planning Area
- Map 1-2: Future Land Use Designations
- Map 1-3: Visioning Corridors & Areas
- Map 1-4: Vacant Lands

Transportation Element

- Map 2-1: Functional Classifications
- Map 2-2: Jurisdictions
- Map 2-3: Existing and Proposed Bicycle and Pedestrian Facilities
- Map 2-4: Golf Cart Routes/Crossings
- Map 2-5: Level of Service
- Map 2-6: PSTA Network (Routes and Facilities)

Conservation Element

- Map 4-1: Significant Wetlands

Coastal Management Element

- Map 5-1: Coastal Storm Area
- Map 5-2: Hurricane Storm Surge Areas\ Routes and Shelters
- Map 5-3: Topographic Map
- Map 5-4: Soil Map
- Map 5-5: Hydrological Features
- Map 5-6: Public Waterways and Coastal Access Areas

Recreation & Open Space Element

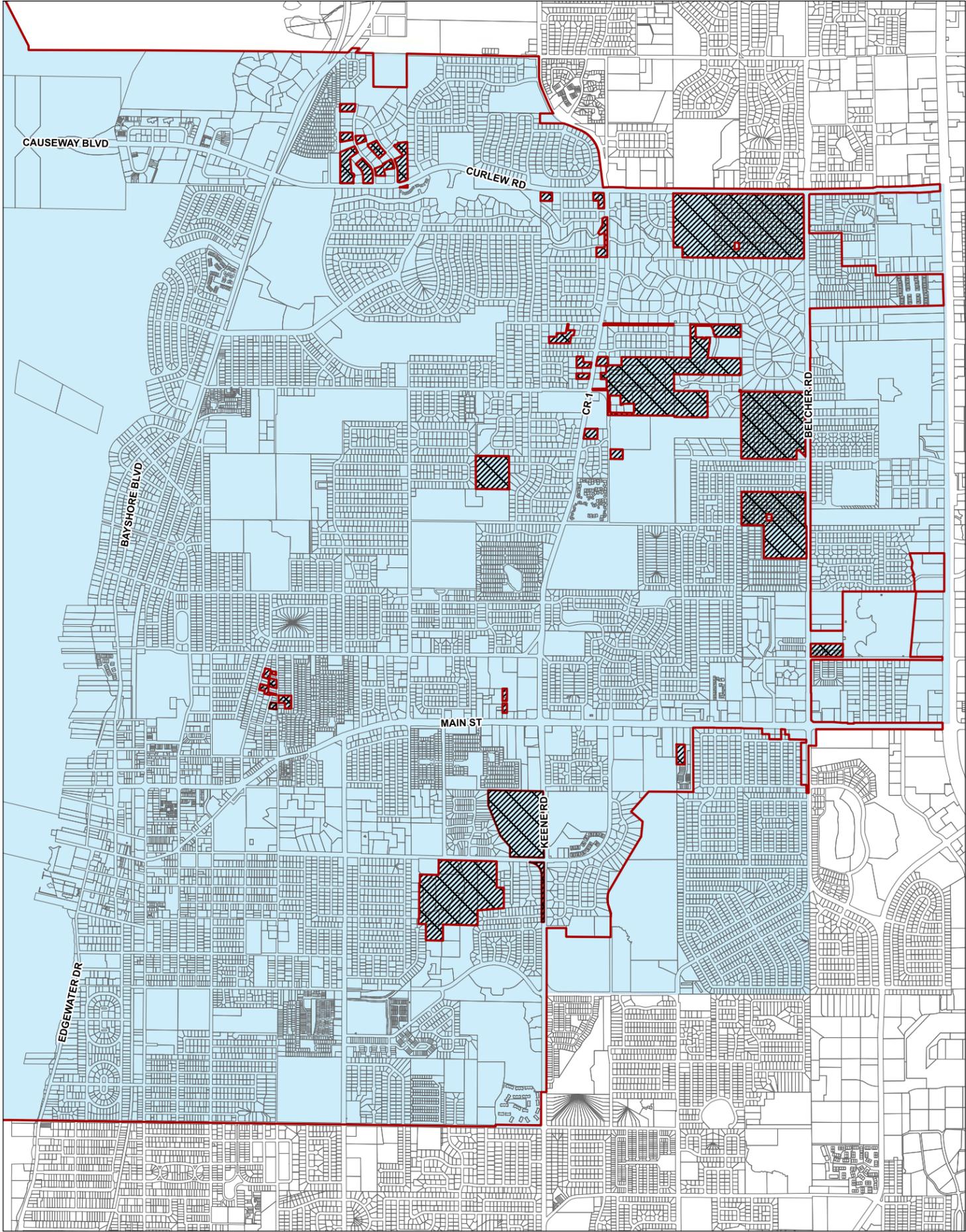
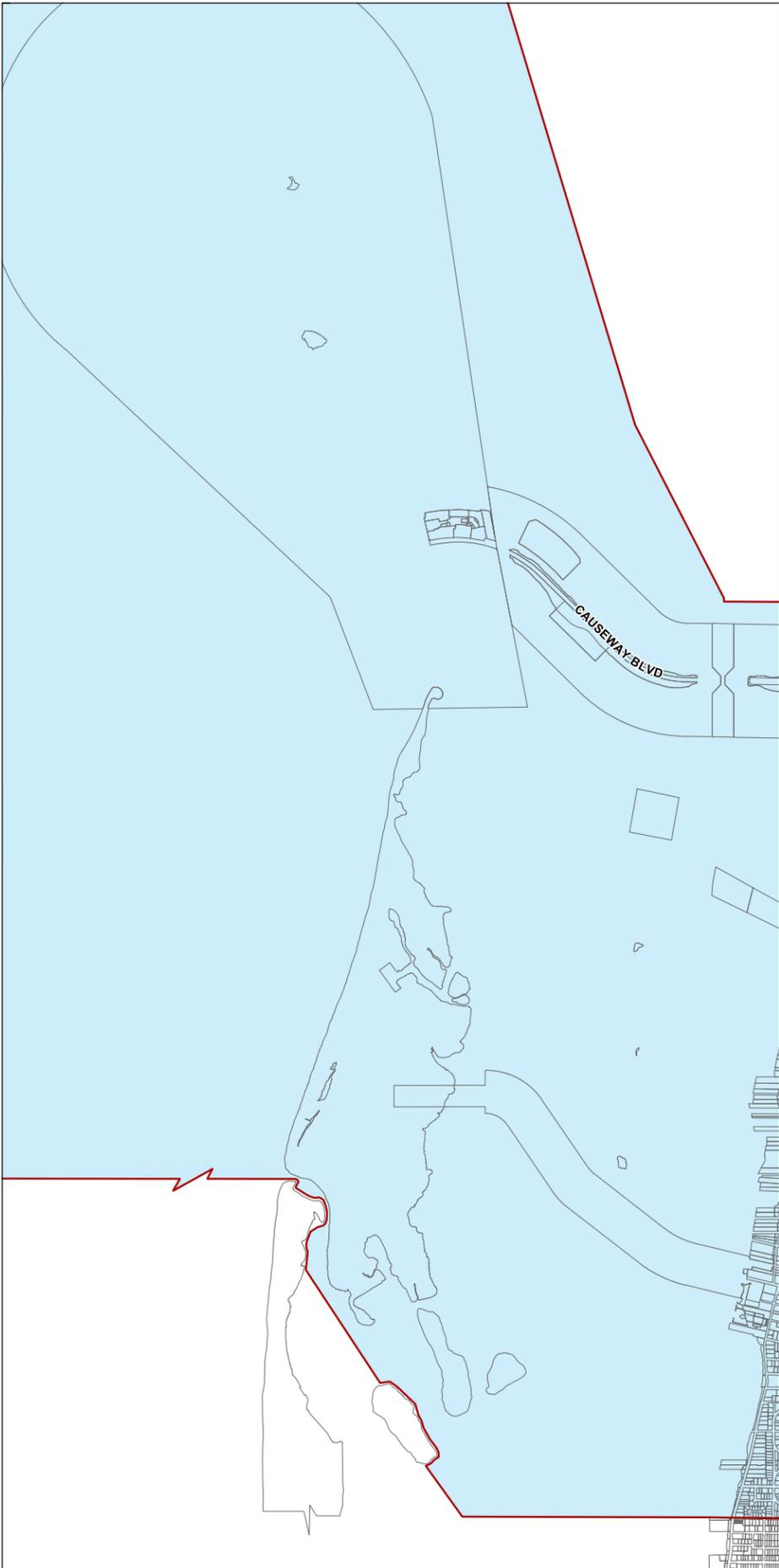
- Map 6-1: Parks, Open Space, Trails

Support Services Element

- Map 7-1: Wastewater System
- Map 7-2: Potable Water System
- Map 7-3: Wellhead Protection Areas
- Map 7-4: Reclaimed Water System
- Map 7-5: Major Drainage Basins and Facilities



Map 1-1 Future Land Use Element: Planning Area



DUNEDIN
Home of Honeymoon Island
**STRIVE FOR
2035**

- Legend**
- Planning Area
 - Dunedin City Limits
 - City Limits Enclave

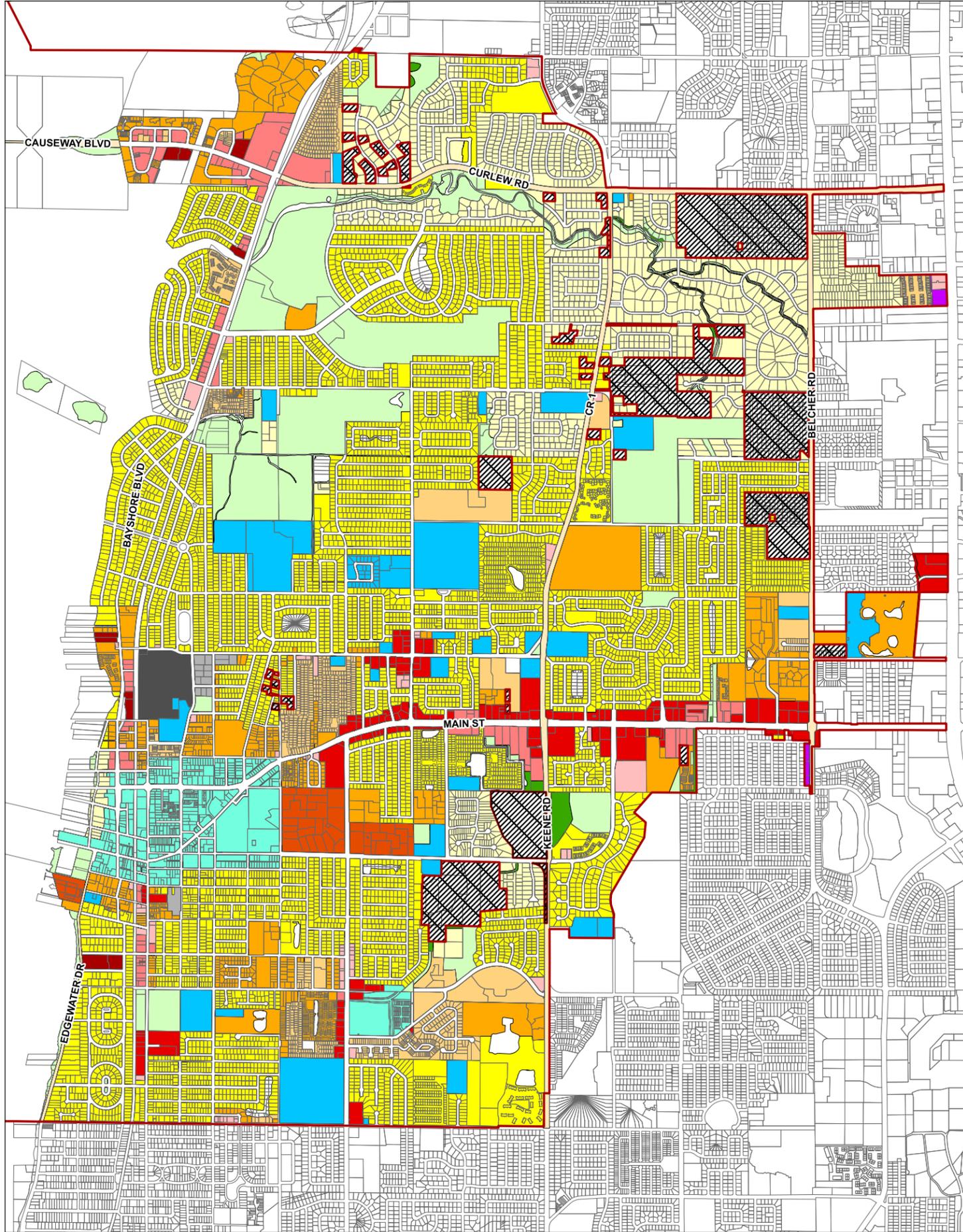
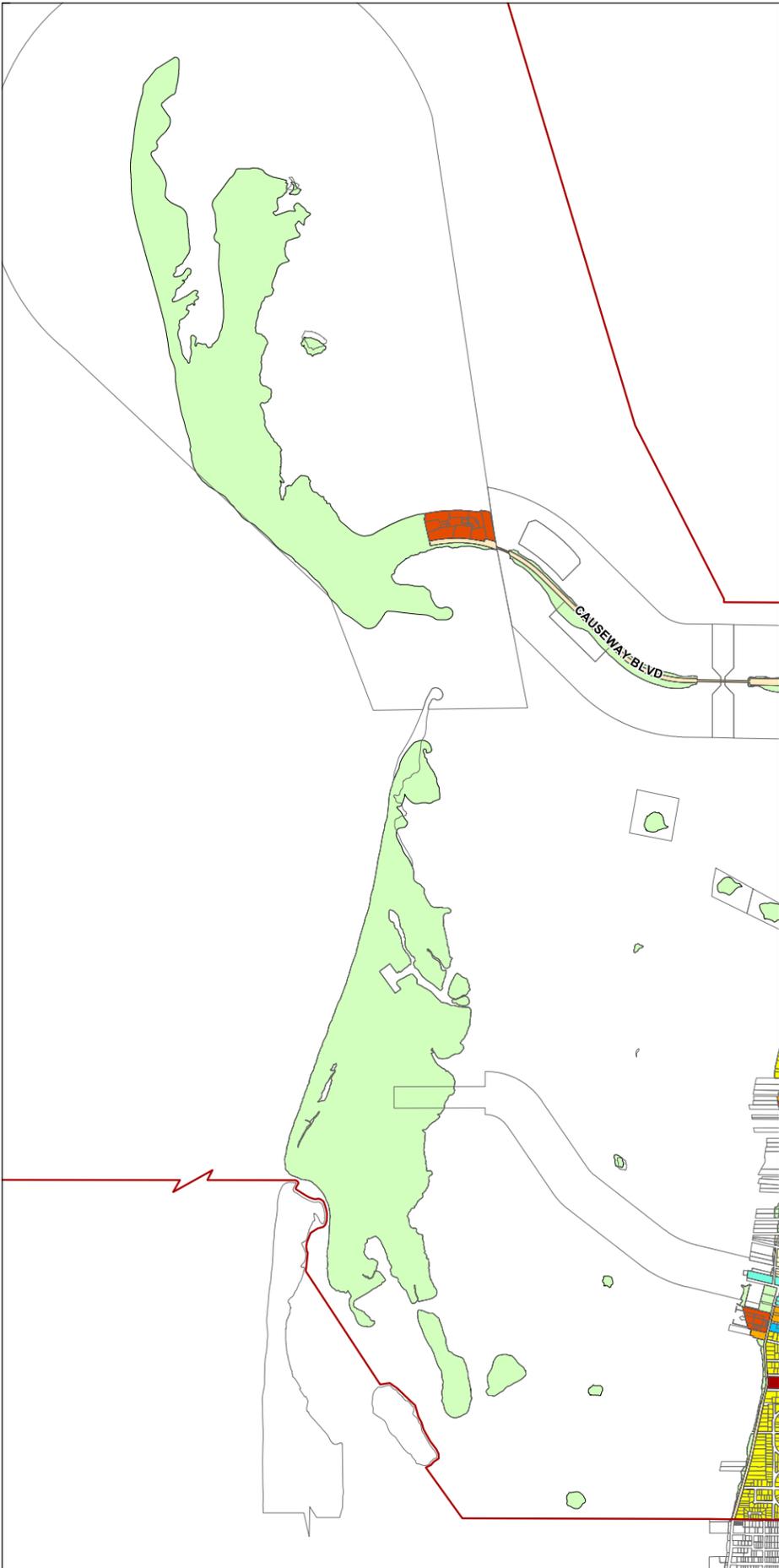
Source: City of Dunedin 2019

Map 1-2 Future Land Use Element: Future Land Use Designations

1,300 650 0 1,300 Feet



DUNEDIN
Home of Honeymoon Island
STRIVE FOR 2035

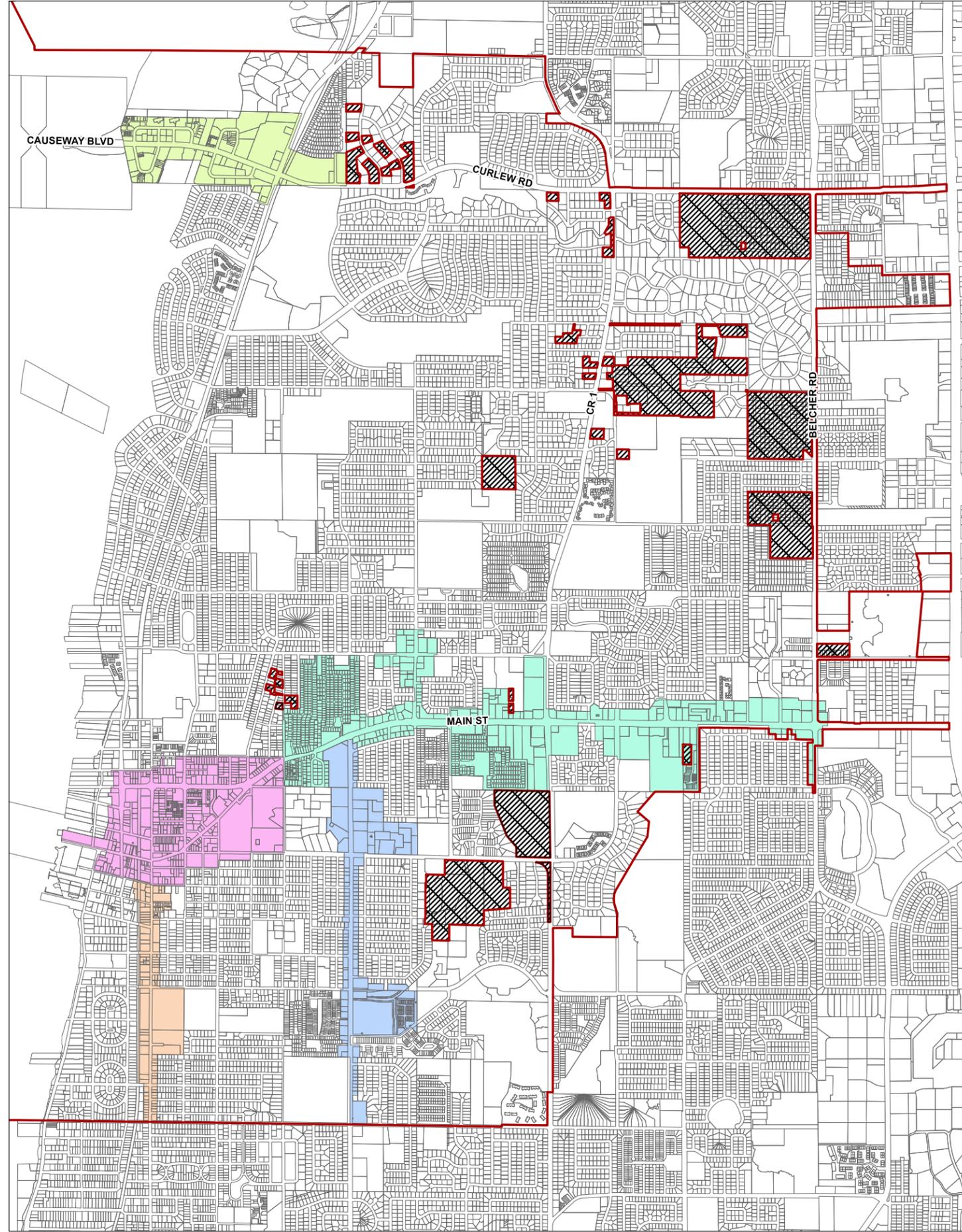
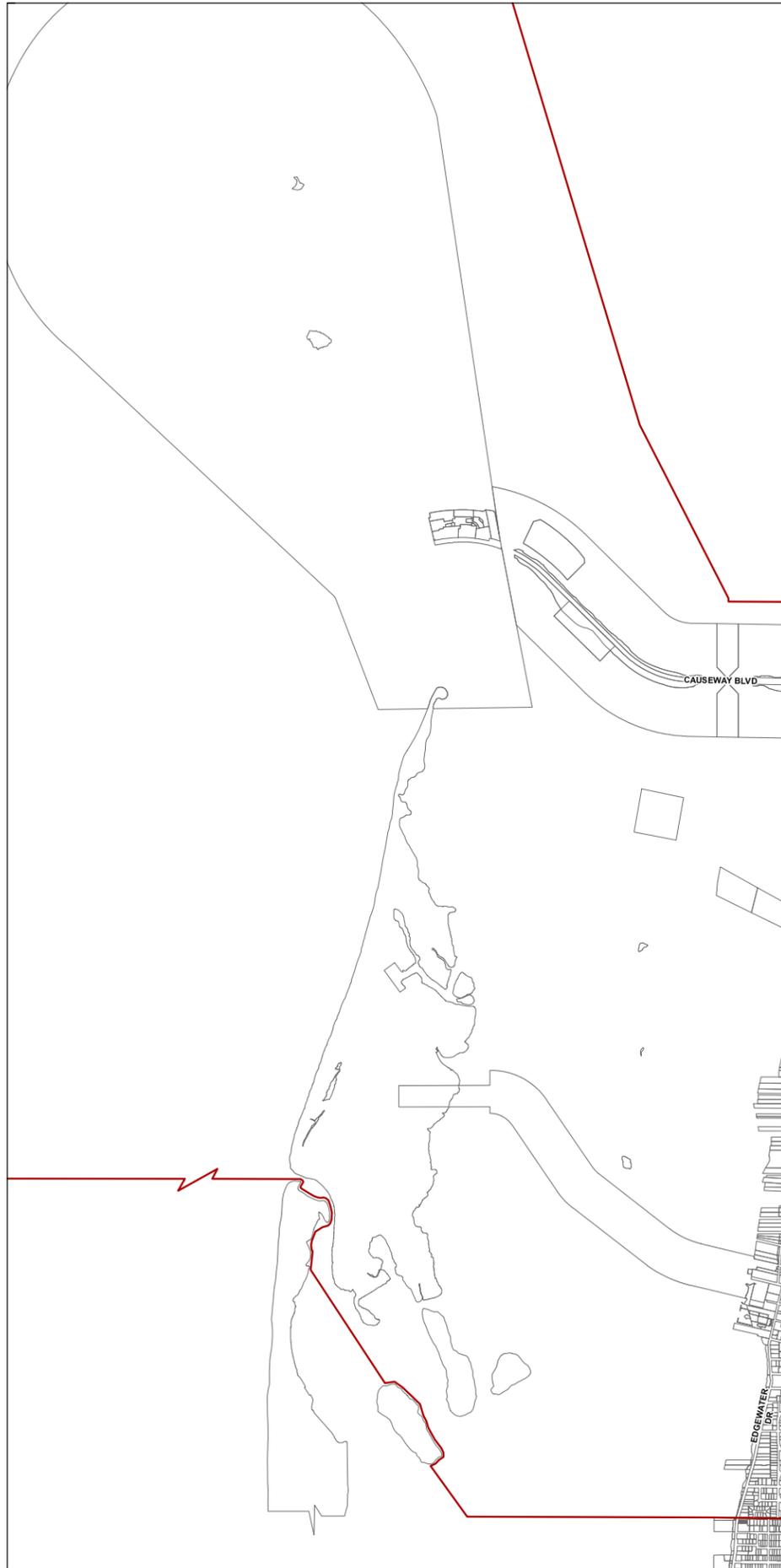


Legend

- Industrial (I)
- Employment (E)
- Public/Semi-Public (P/SP)
- Retail & Services (R&S)
- Coastal High Hazard Area- Retail & Services (CHHA-R&S)
- Retail & Services-Limited (R&S-L)
- Activity Center (AC)
- Office (O)
- Coastal High Hazard Area- Resort (CHHA-R)
- Residential High (RH)
- Residential Medium (RM)
- Residential Low Medium (RLM)
- Residential Urban (RU)
- Residential Low (RL)
- Scenic/Non-Commercial Corridor (SNCC)
- Recreation/Open Space (R/OS)
- Preservation (P)
- Dunedin City Limits
- City Limits Enclave

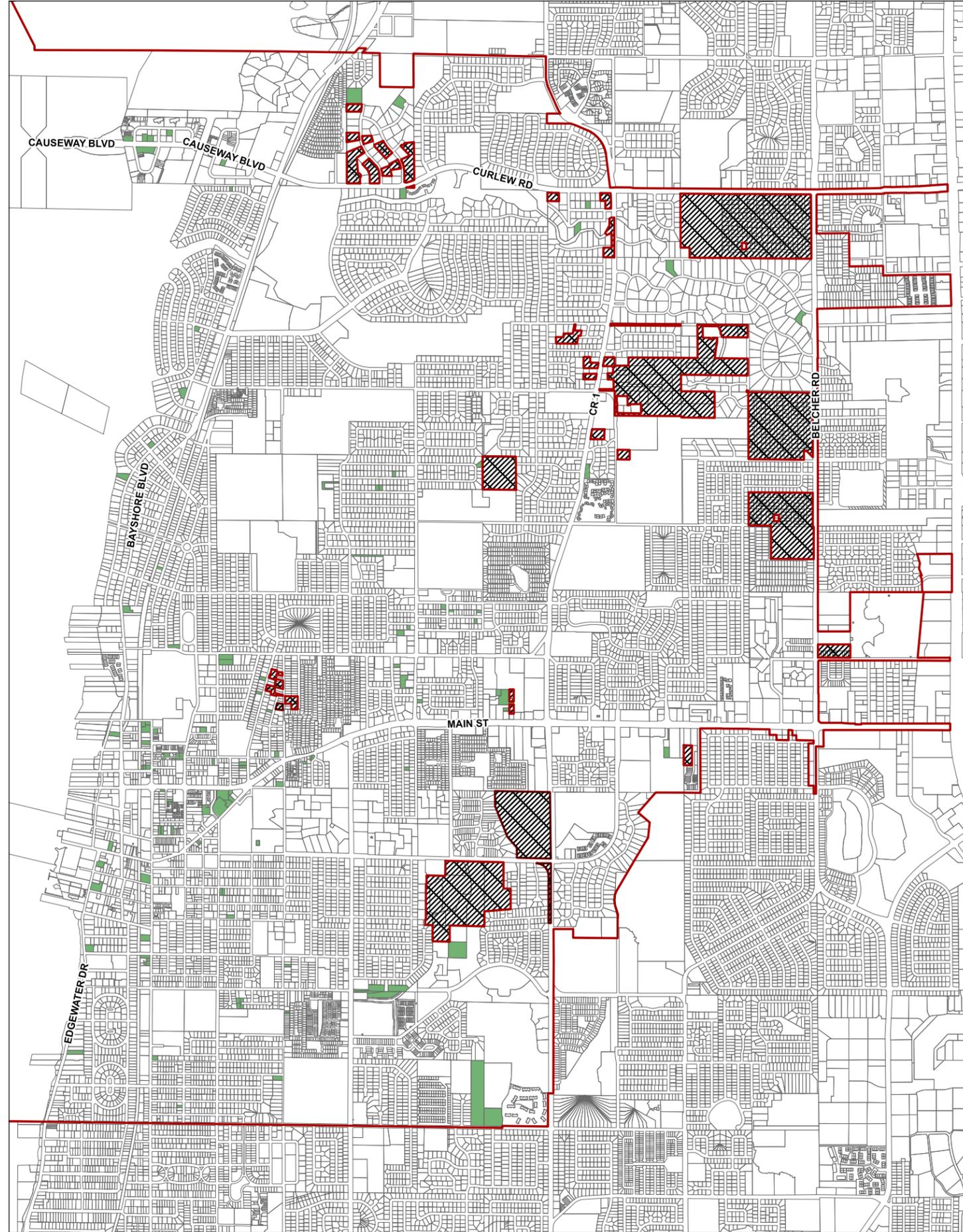
Source: City of Dunedin 2019

Map 1-3 Future Land Use Element: Visioning Corridors & Areas



- Legend**
- CRA
 - Causeway Blvd
 - Douglas Ave
 - Patricia Ave
 - S.R. 580
 - Dunedin City Limits
 - City Limits Enclave
- Source: City of Dunedin 2019

Map 1-4 Future Land Use Element: Vacant Lands



Legend

-  Vacant Land
-  Dunedin City Limits
-  City Limits Enclave

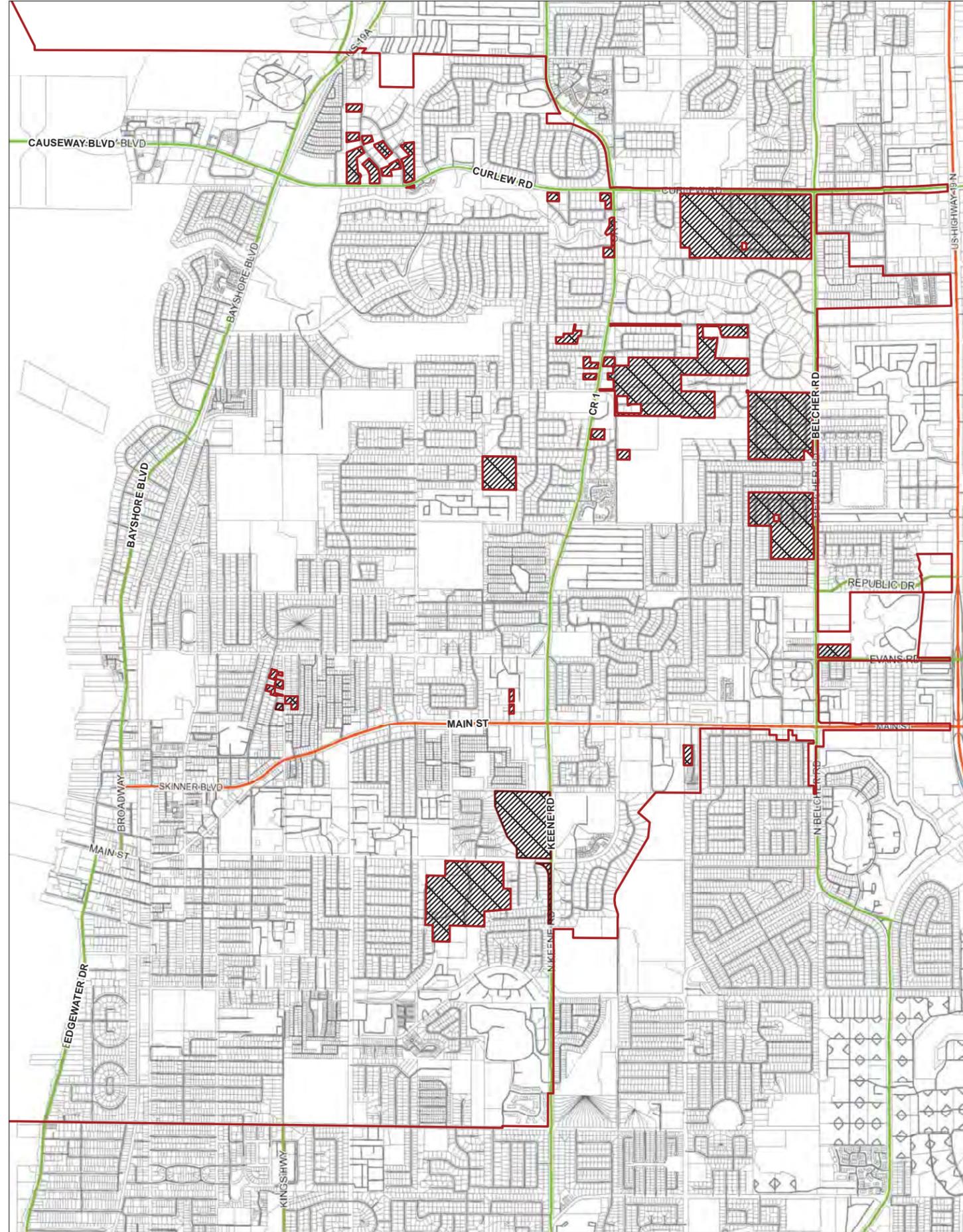
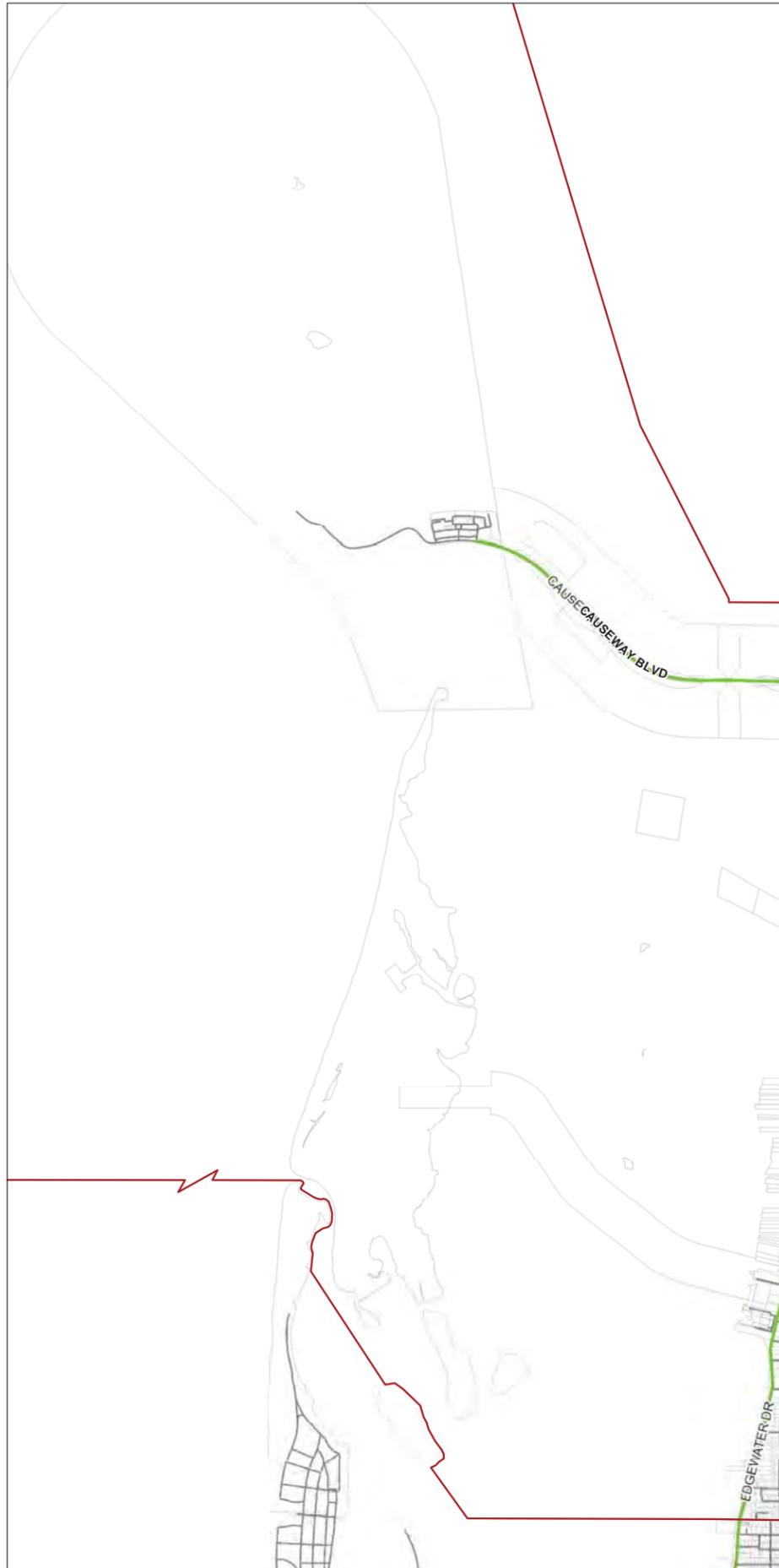
Total Parcel Area: 199591391 SF (4582 Acres)

Vacant Parcel Area: 2456745 SF (56.4 Acres)

Vacant Parcel Percentage: 1.23%

Source: Pinellas County & City of Dunedin 2019

Map 2-1 Transportation Element: Functional Classifications

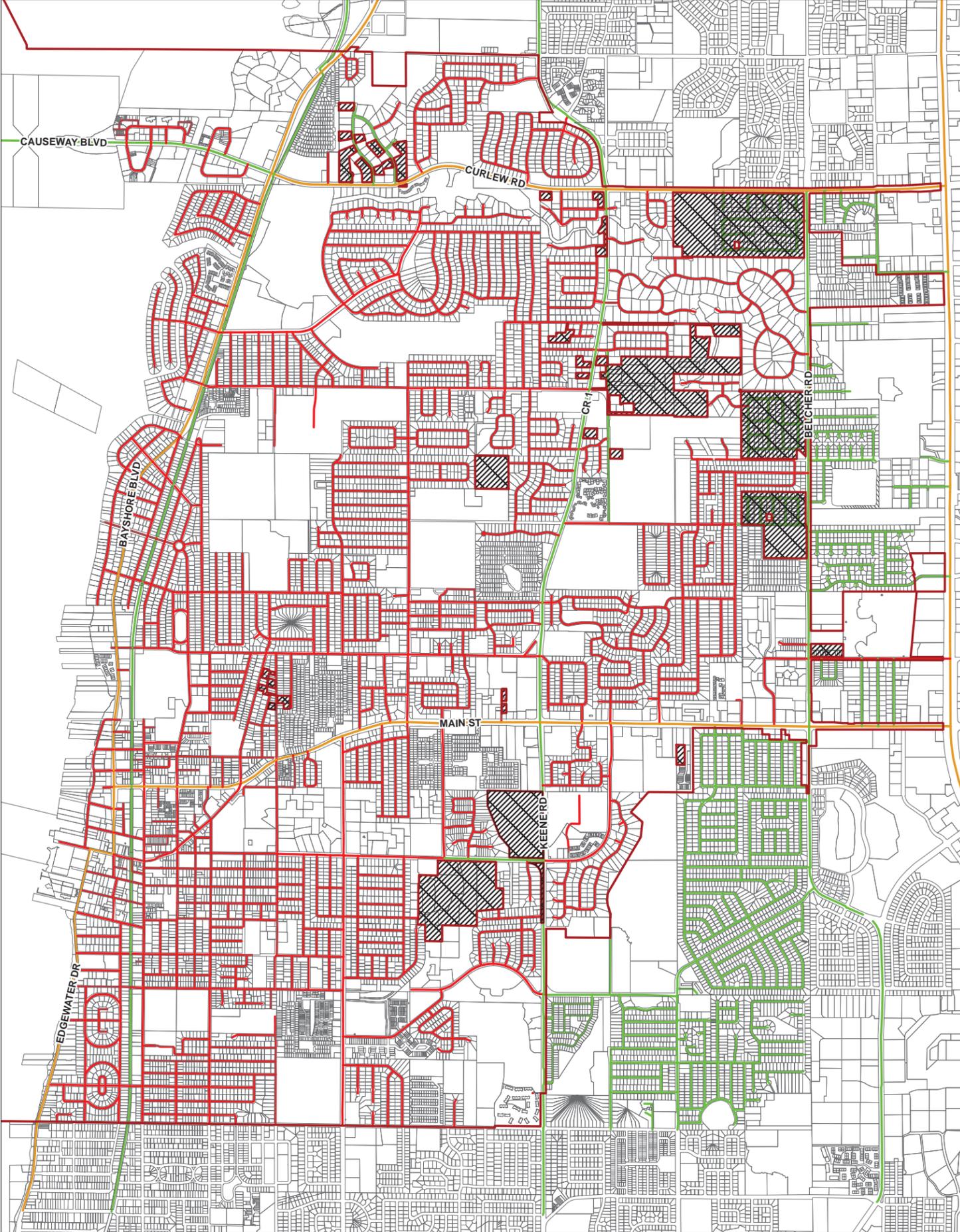


Legend

- Major Road
- Arterial
- Local Roads

Source: Pinellas County 2019

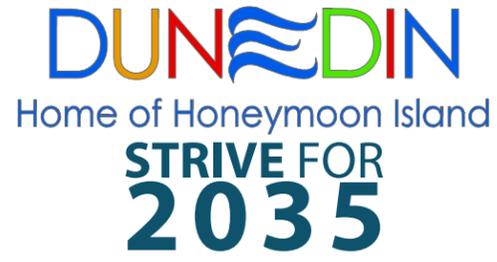
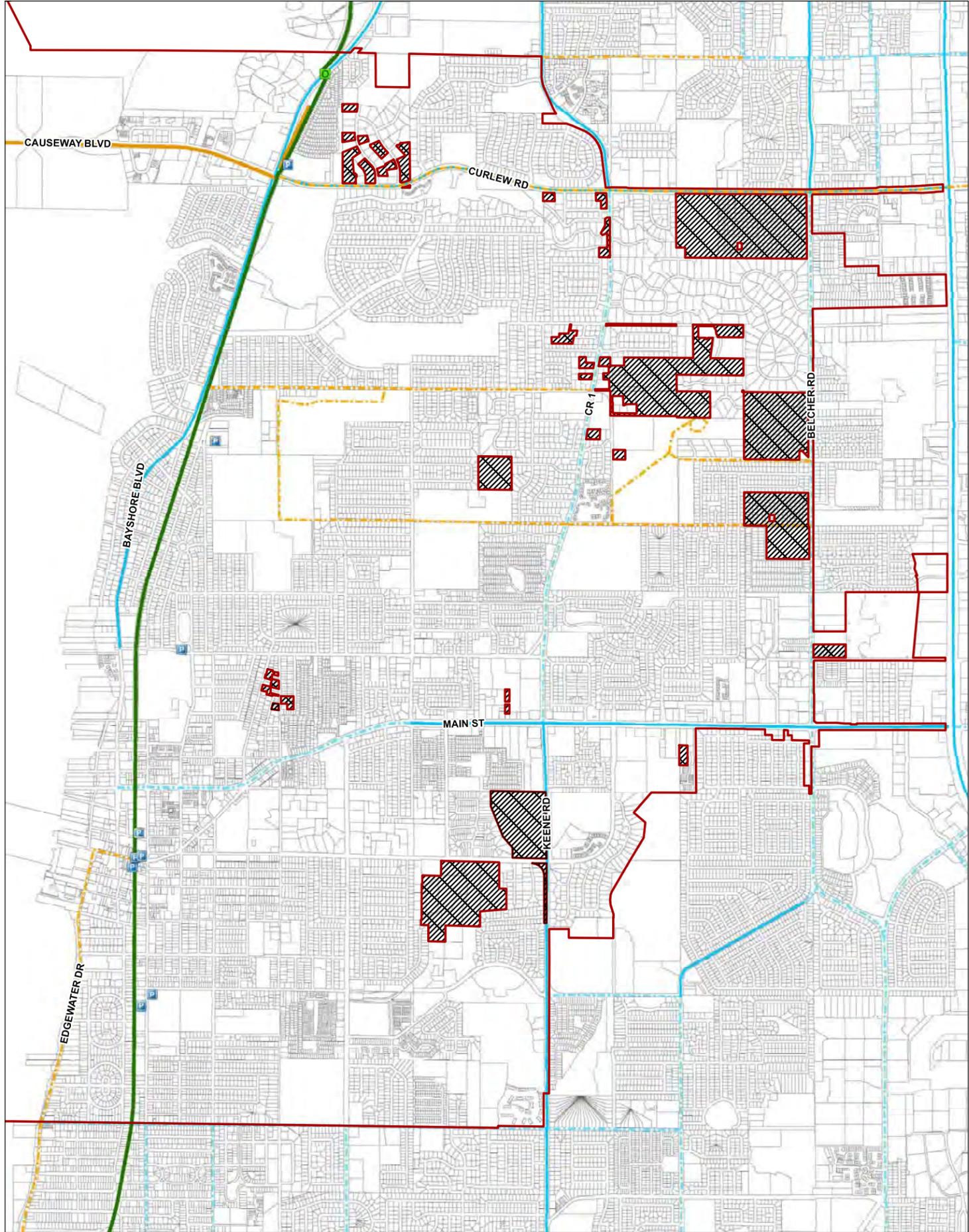
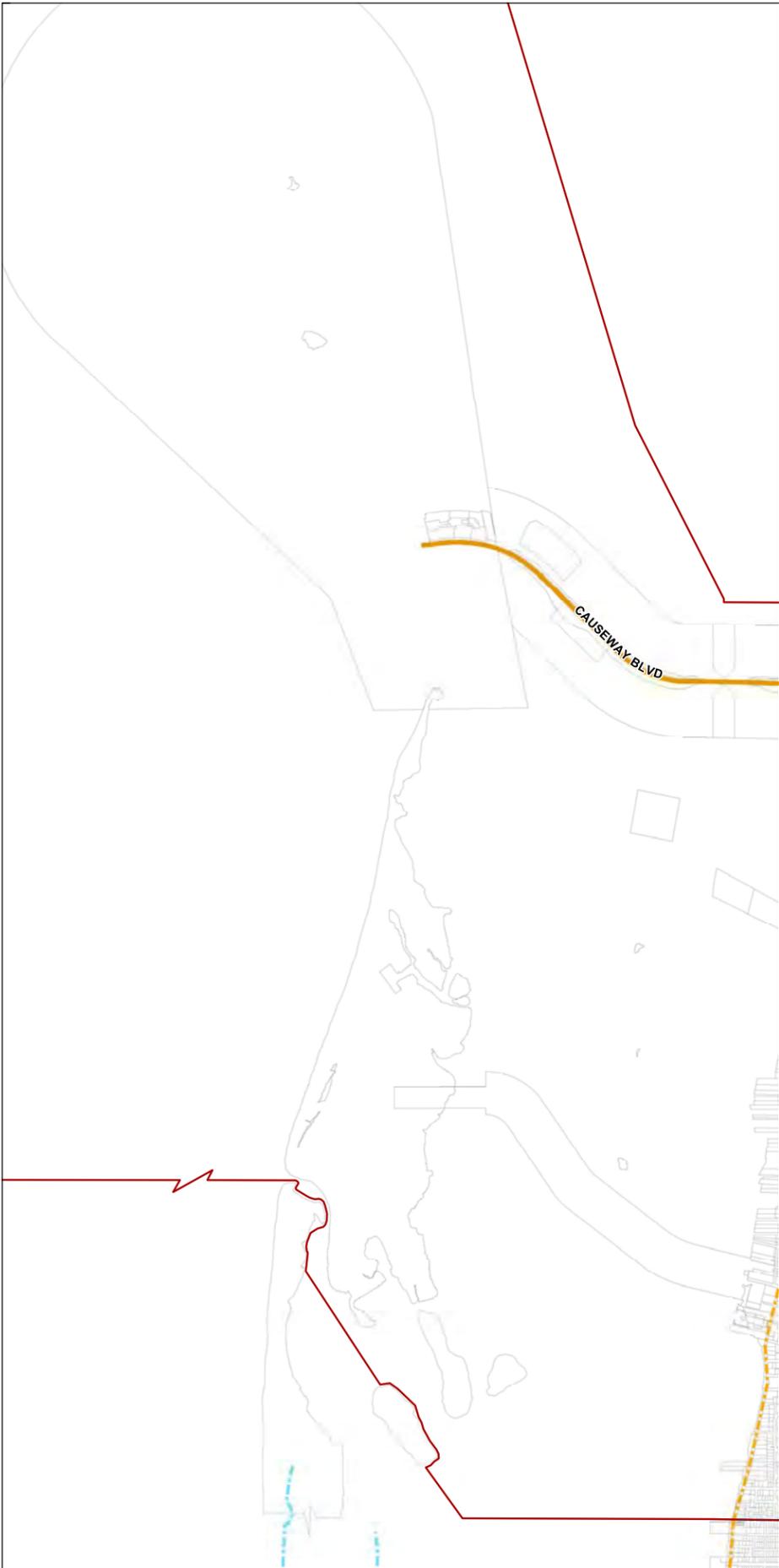
Map 2-2 Transportation Element: Jurisdictions



DUNEDIN
Home of Honeymoon Island
STRIVE FOR
2035

- Legend**
- City of Dunedin
 - Pinellas County
 - State of Florida
 - Dunedin City Limits
 - City Limits Enclave
- Source: Pinellas County & City of Dunedin 2019

Map 2-3 Transportation Element: Existing & Proposed Bicycle and Pedestrian Facilities

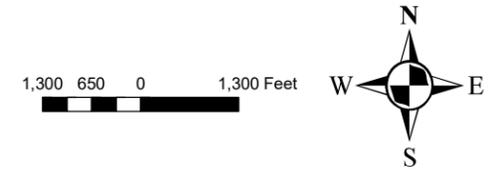
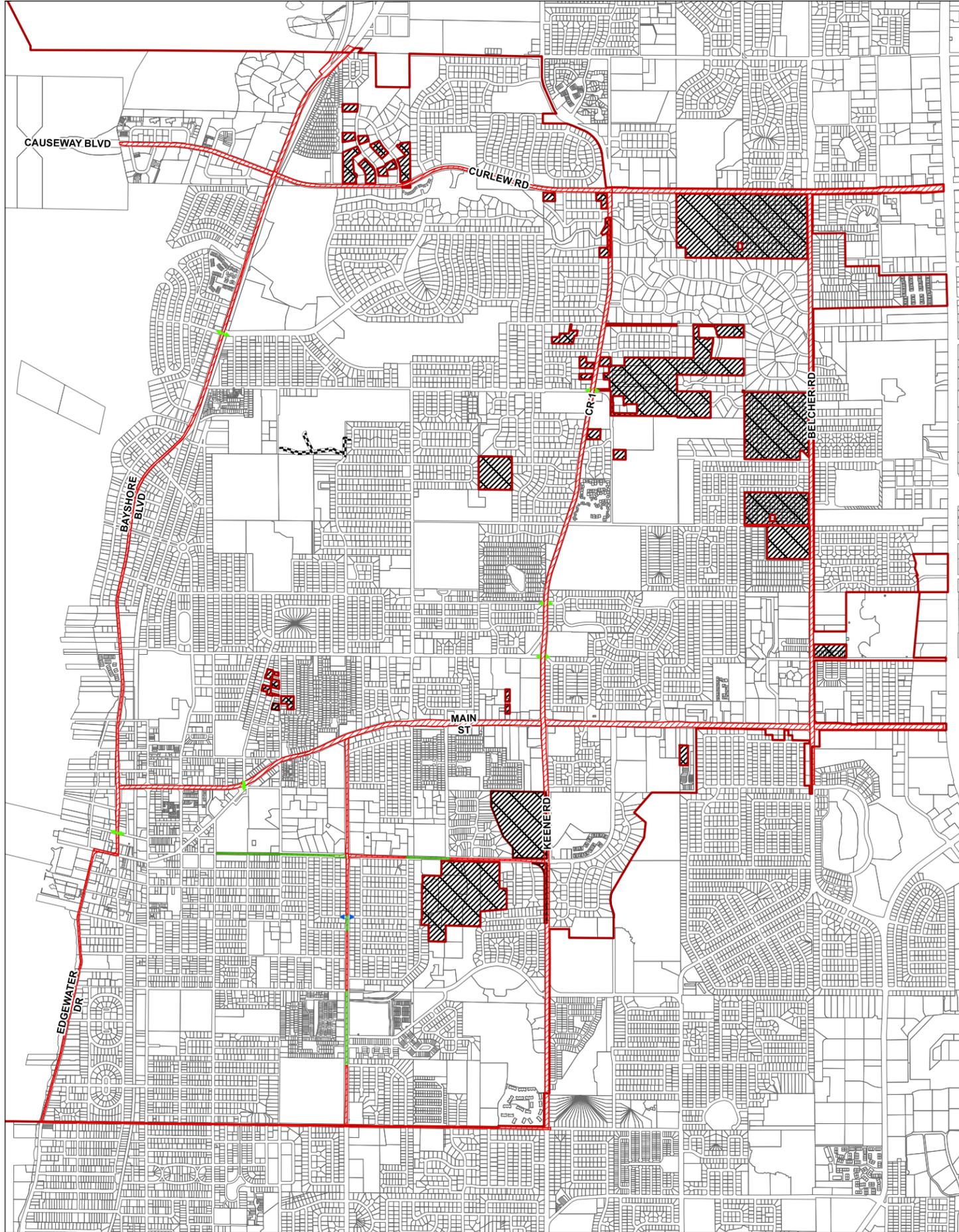
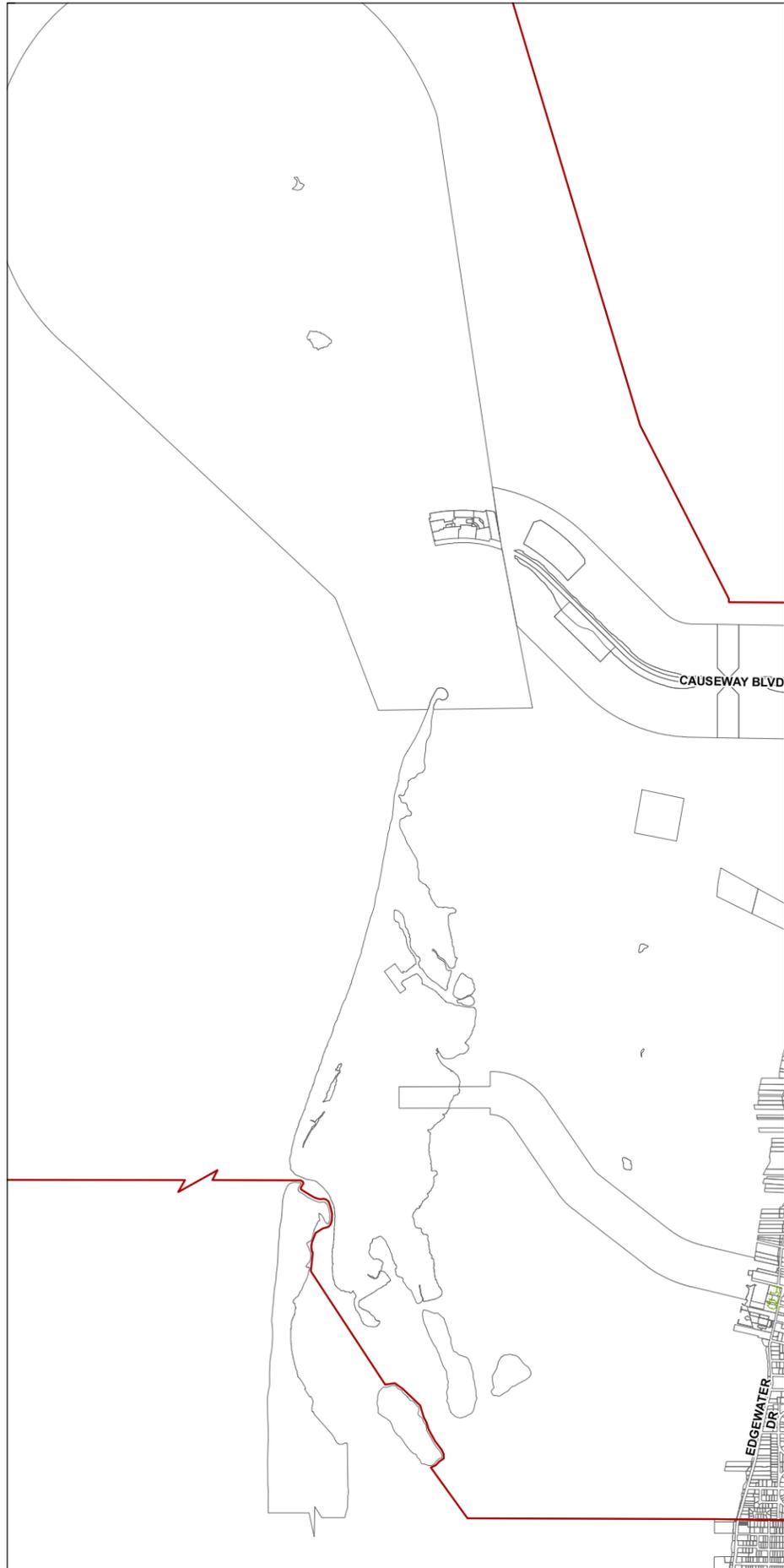


- Legend**
- OVERPASS
 - UNDERPASS
 - Trail Parking Areas
 - Equestrian Trail
 - Pinellas Trail
 - Bike Lanes
 - Bike Lanes Proposed
 - Community Trails

- Community Trails Proposed
- Dunedin City Limits
- City Limits Enclave

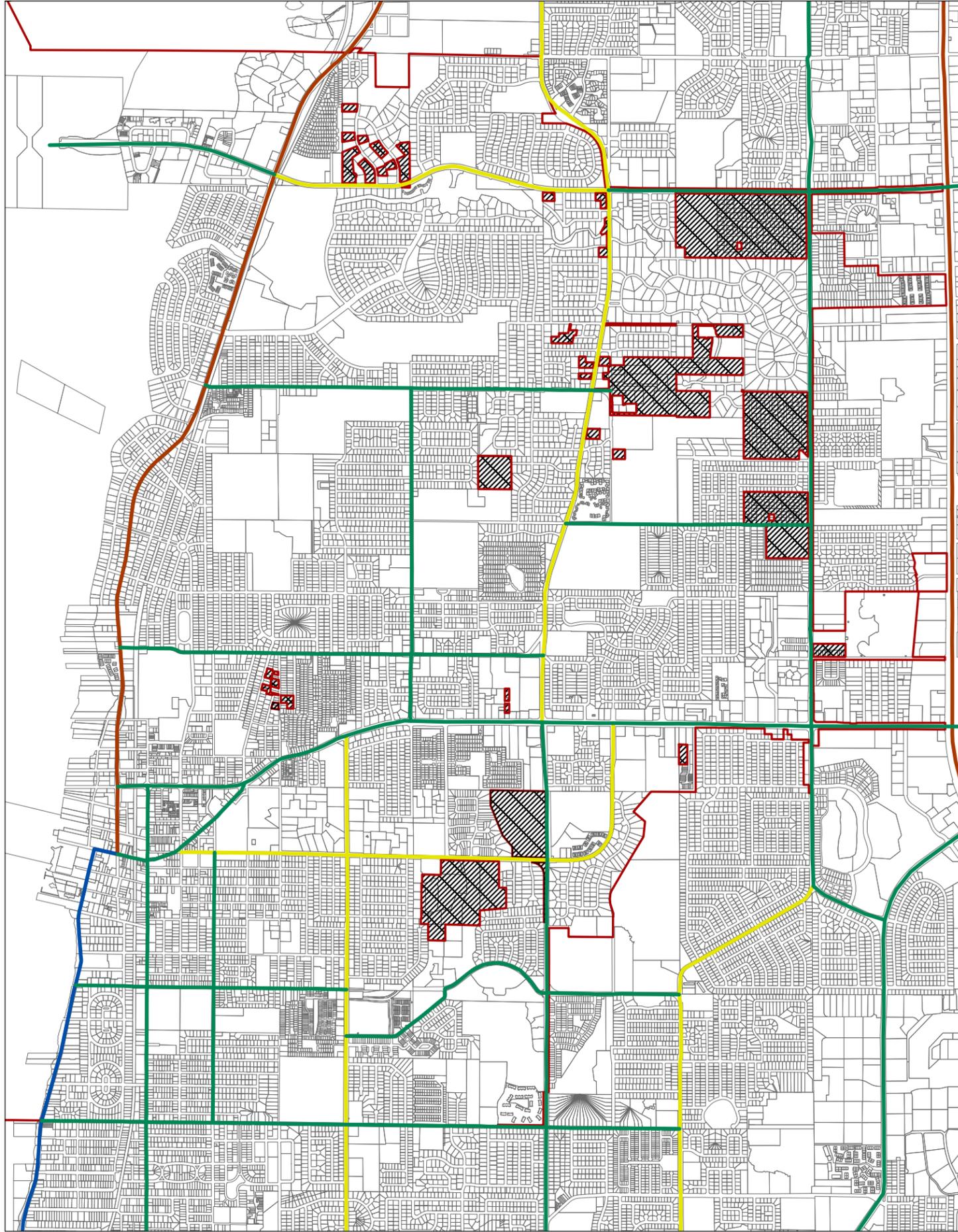
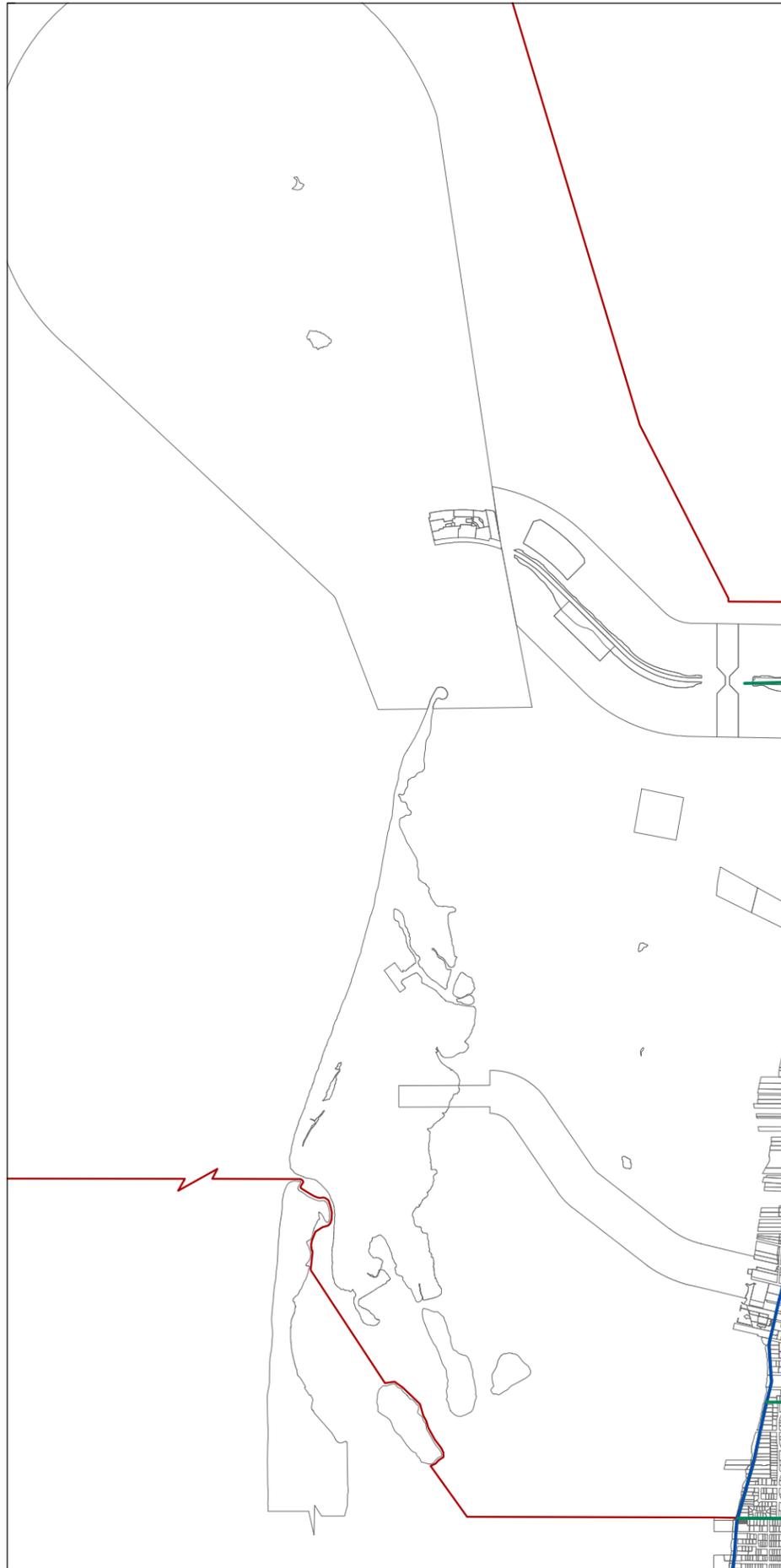
Source: Pinellas County 2019

Map 2-4 Transportation Element: Golf Cart Routes/Crossings



- Legend**
- High Volume Road Crossing
 - Low Volume Road Crossing
 - Trail Accessibility
 - Prohibited Access
 - High Volume Road Share 25 MPH
- Source: City of Dunedin 2019

Map 2-5 Transportation Element: Level of Service



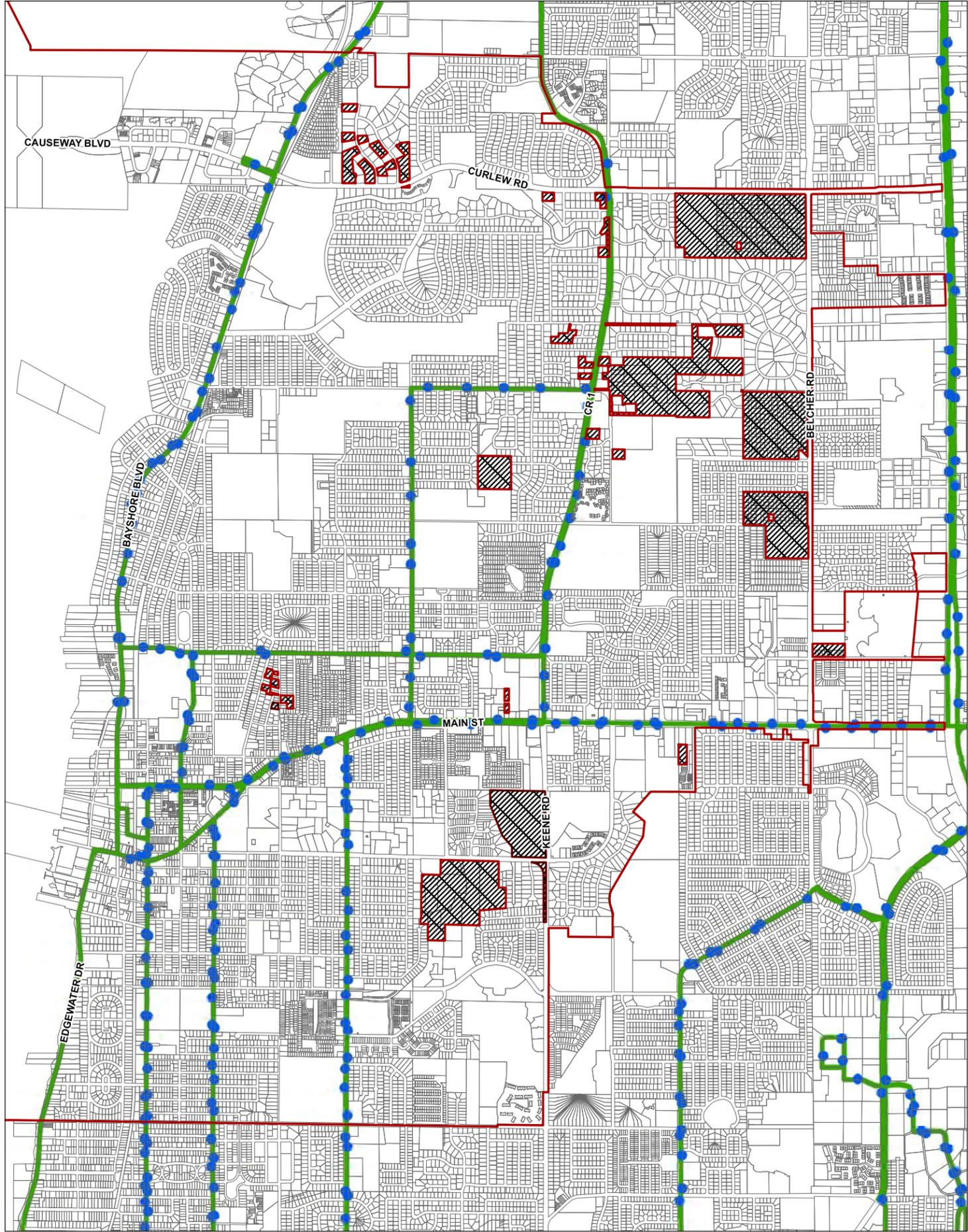
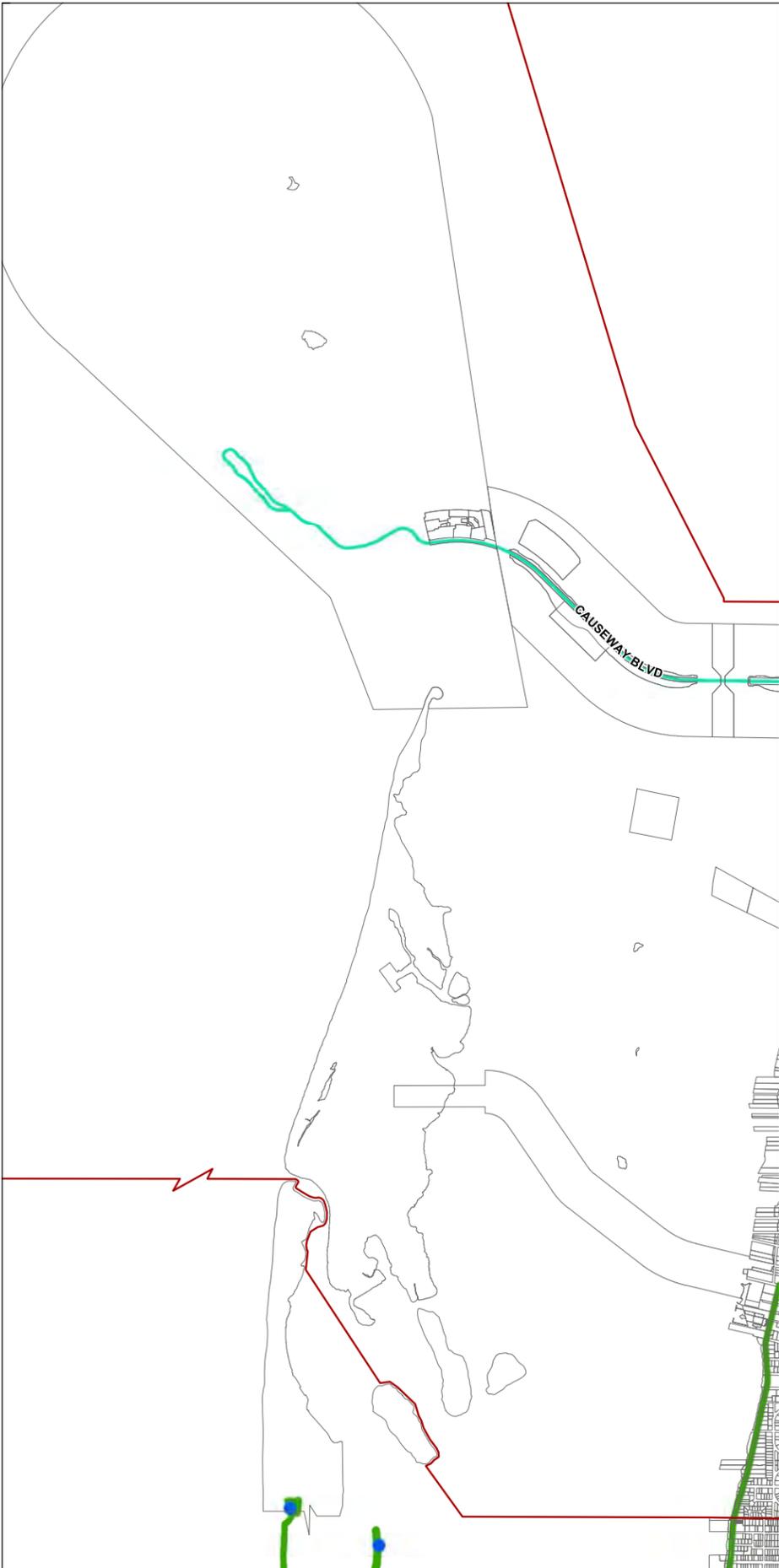
DUNEDIN
Home of Honeymoon Island
STRIVE FOR
2035

Legend

- Level of Service B,C
- Level of Service D
- Level of Service E
- Level of Service F
- Dunedin City Limits
- City Limits Enclave

Source: Data Source: Forward Pinellas, 2018.

Map 2-6 Transportation Element: PSTA Network (Routes and Facilities)



1,300 650 0 1,300 Feet



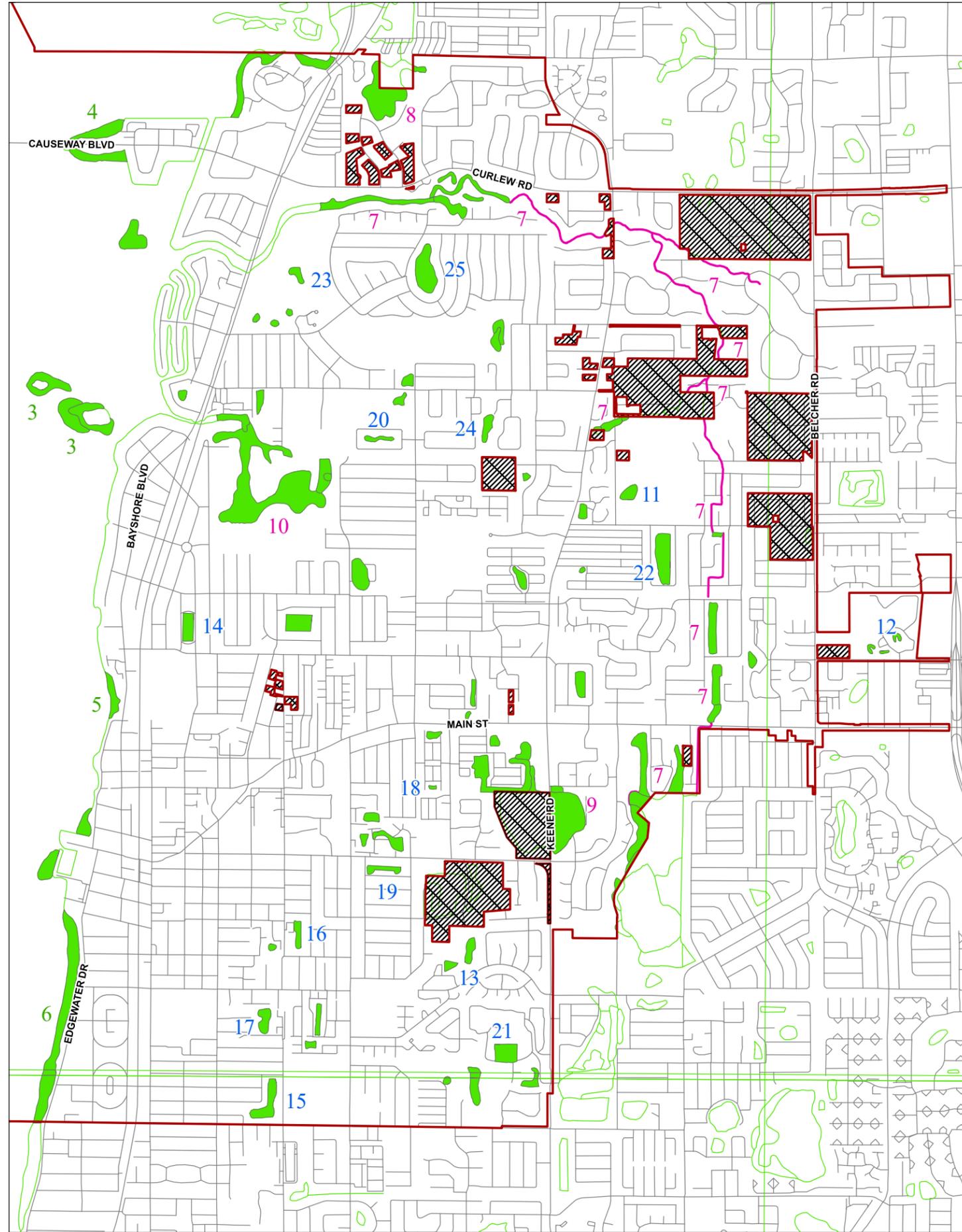
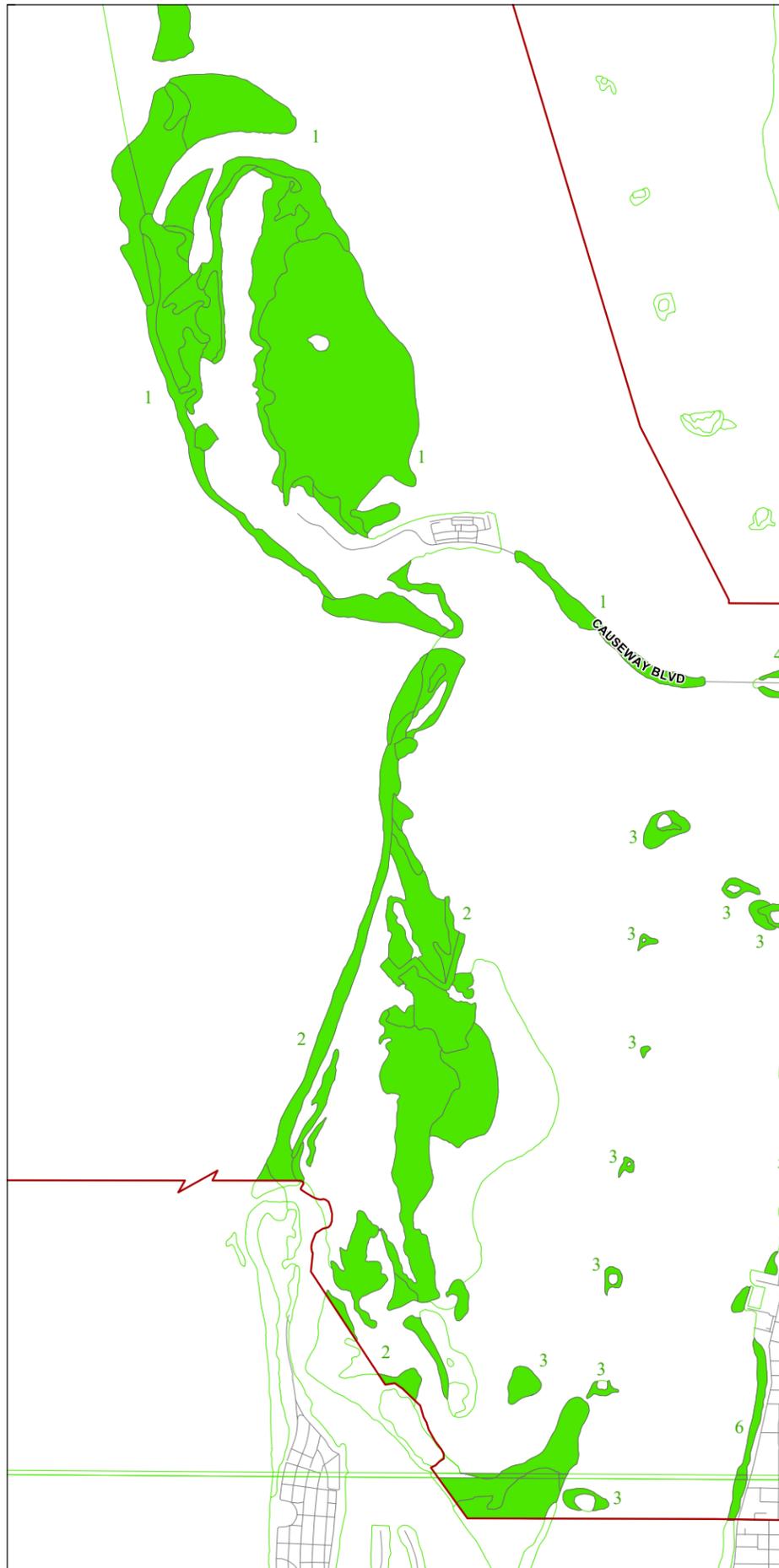
DUNEDIN
Home of Honeymoon Island
STRIVE FOR
2035

Legend

- Bus Stops
- Routes
- Dunedin City Limits
- ▨ City Limits Enclave

Source: Pinellas County 2019

Map 4-1 Conservation Element: Significant Wetlands



Legend

- Significant Wetlands
- Dunedin City Limits
- City Limits Enclave

- 1.) HONEYMOON ISLAND/CAUSEWAY
- 2.) CALADESI ISLAND/MALONE ISLAND/ MOONSHINE ISLAND/CORE ISLAND
- 3.) SPOIL ISLANDS
- 4.) NORTH SHORELINE
- 5.) CENTRAL SHORELINE
- 6.) SOUTH SHORELINE
- 7.) CURLEW CREEK
- 8.) SALT MARSH
- 9.) HOWELL SWAMP
- 10.) HAMMOCK PARK/YOUTH GUILD PARK /CEDAR CREEK
- 11.) VANECH RECREATIONAL COMPLEX
- 12.) CHESAPEAKE APARTMENTS
- 13.) SCOTSDALE PARK
- 14.) LAKE PALOMA
- 15.) LAKE DIANE
- 16.) LAKE EARLE
- 17.) LAKE LOGARTO
- 18.) LAKE HAVEN MHP
- 19.) DUNEDIN LAKEWOOD ESTATES
- 20.) LAKE RHONDA
- 21.) NORTH LAKE
- 22.) SCOTS LAKE
- 23.) DUNEDIN COUNTRY CLUB
- 24.) STIRLING HEIGHTS
- 25.) LAKE SAUNDRA

Source: City of Dunedin and U.S. Fish & Wildlife Service 2019

Map 5-1 Coastal Management Element: Coastal Storm Area

1,300 650 0 1,300 Feet



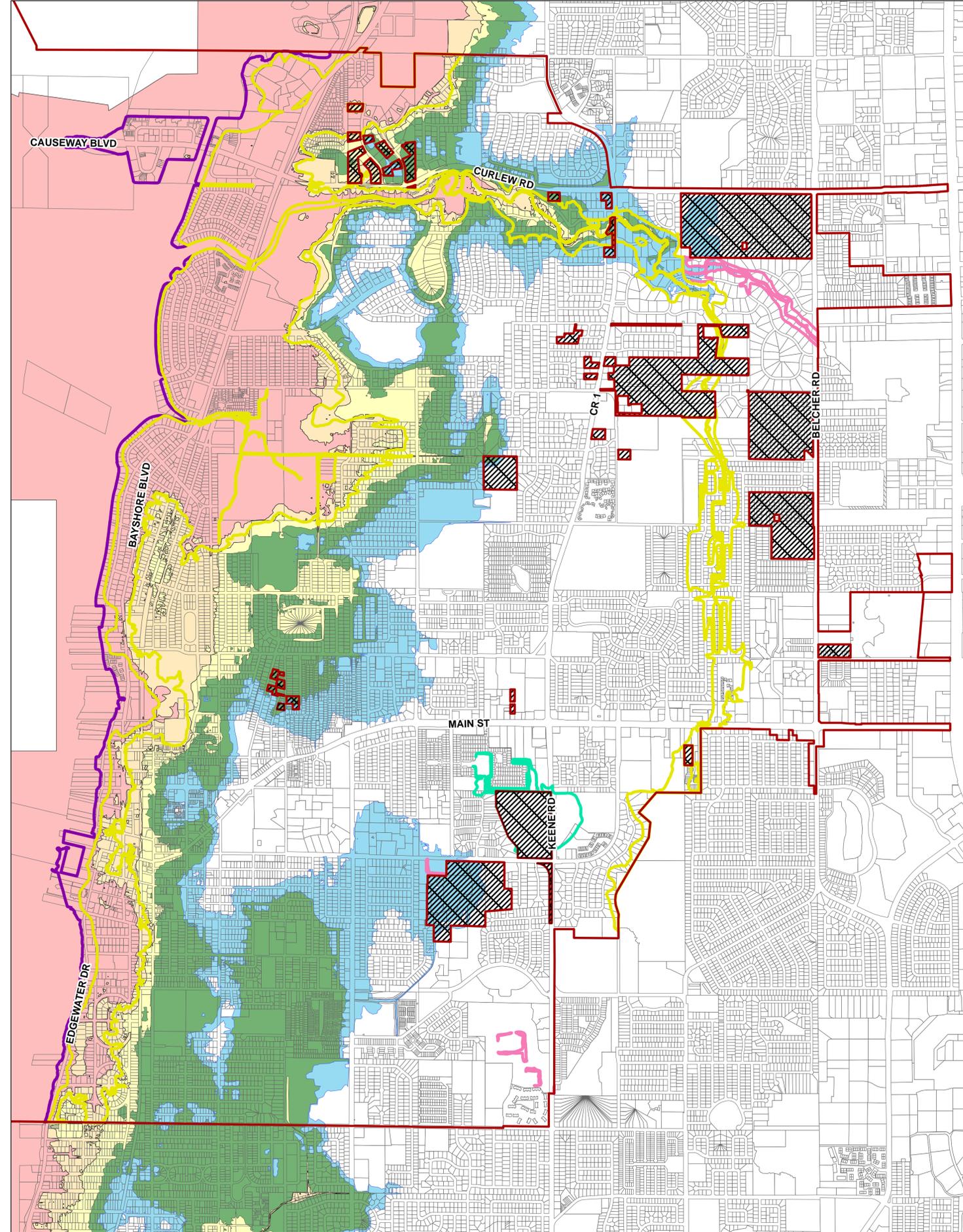
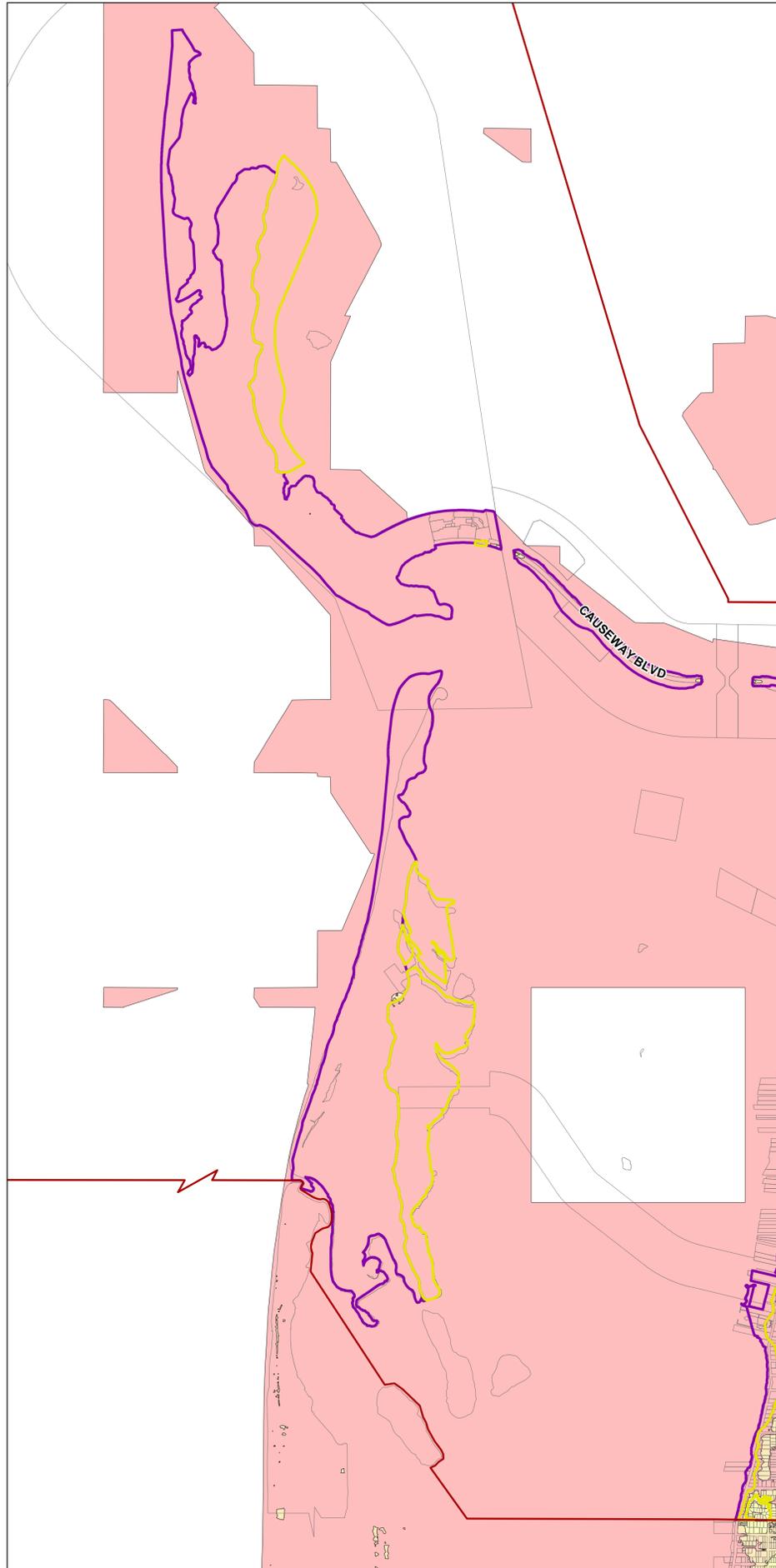
DUNEDIN
Home of Honeymoon Island
STRIVE FOR 2035

Legend

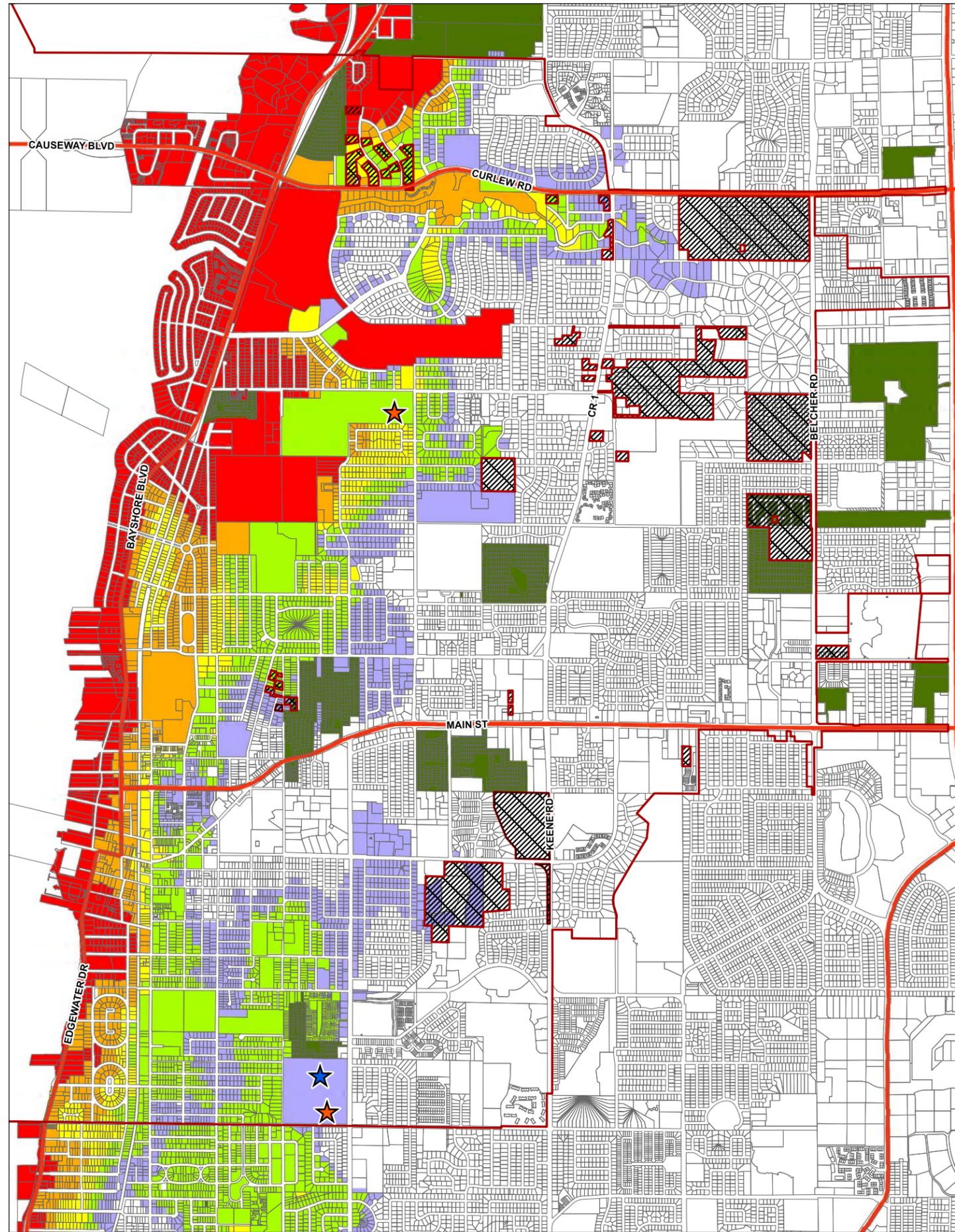
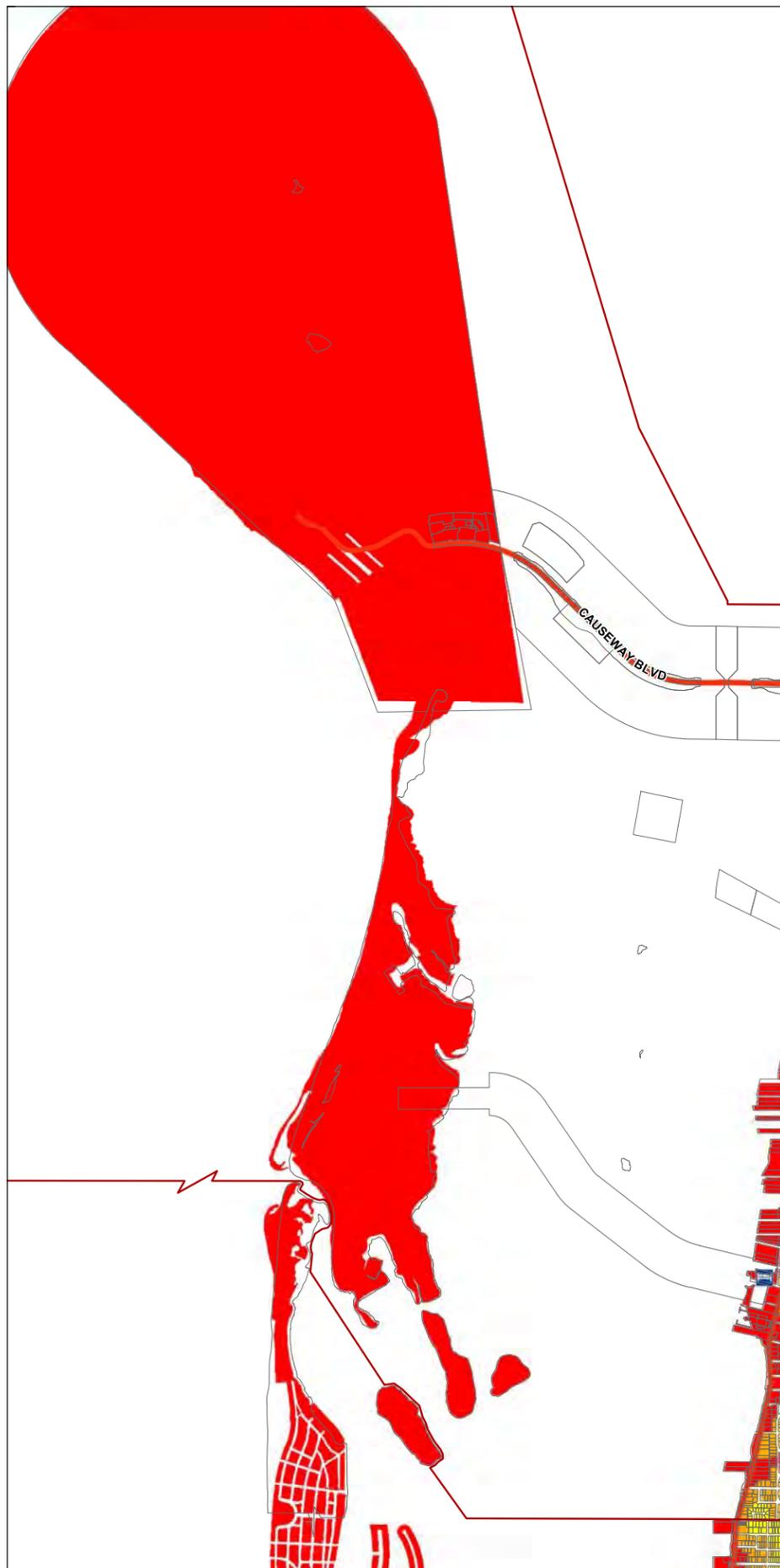
- Dunedin City Limits
- City Limits Enclave
- FEMA SFHA**
- SFHA Zone A
- SFHA Zone AE
- SFHA Zone AO
- SFHA Zone VE
- Storm Surge 2016**
- CHHA Category 1
- Storm Surge Category 2
- Storm Surge Category 3
- Storm Surge Category 4
- Storm Surge Category 5

Source: FEMA & Pinellas County 2016

Note: The Coastal High Hazard Area (CHHA) is defined by Section 163.178 (2)(h), Florida Statutes, as the area below the elevation of the category 1 storm surge line as established by a Sea, Lake, and Overland Surges from Hurricanes(SLOSH) computerized storm surge model. Map represents 2016 data.



Map 5-2 Coastal Management Element: Hurricane Evacuation Zones \ Routes and Shelters



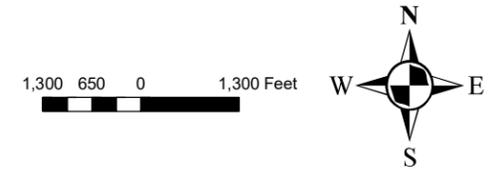
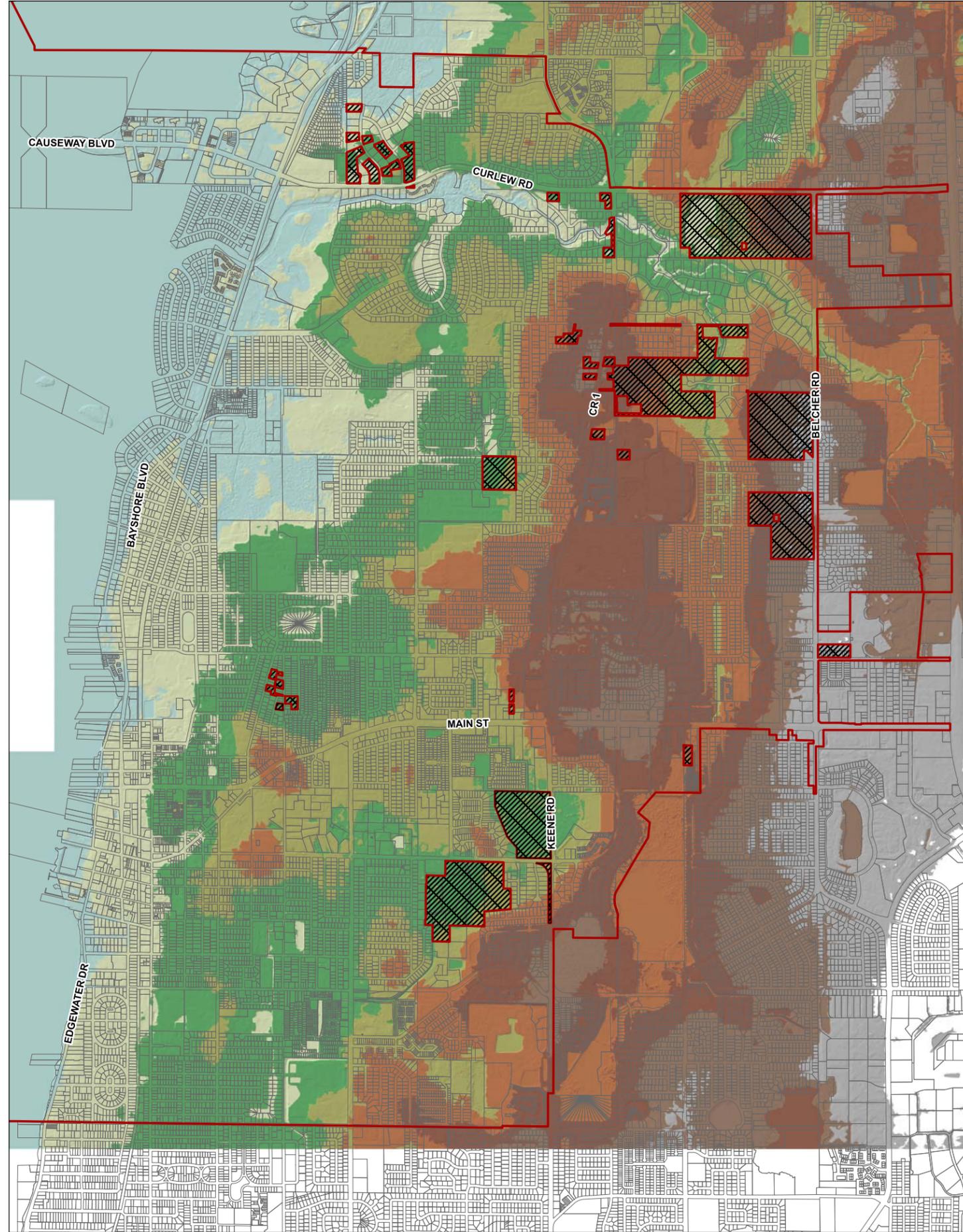
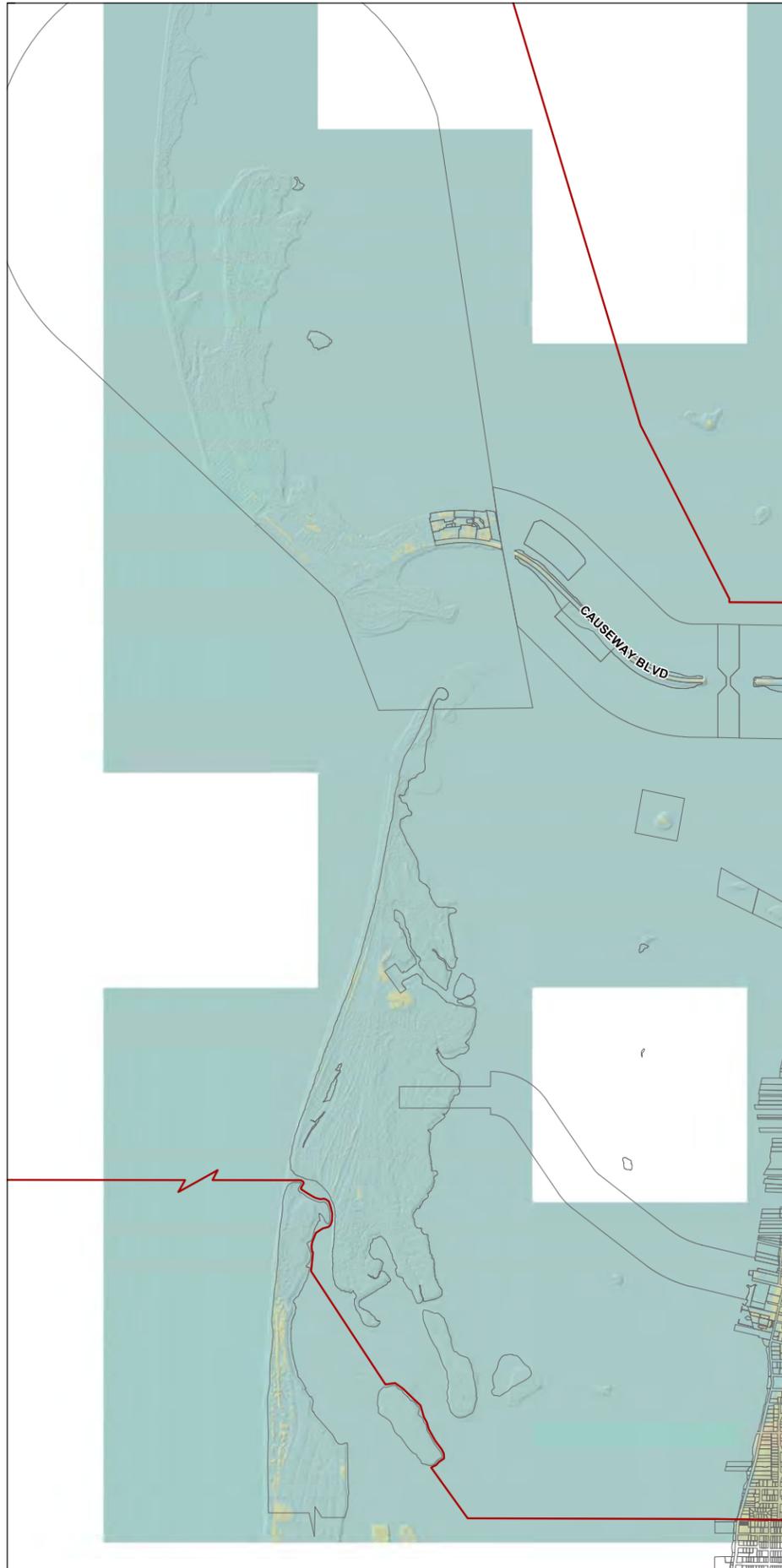
DUNEDIN
 Home of Honeymoon Island
STRIVE FOR 2035

Legend

- Shelter
- Special Needs Shelter
- Evacuation Routes
- A; MOBILE HOME
- B
- C
- D
- E
- Dunedin City Limits
- City Limits Enclave

Source: Pinellas County & City of Dunedin 2019

Map 5-3 Coastal Management Element: Topographic Map

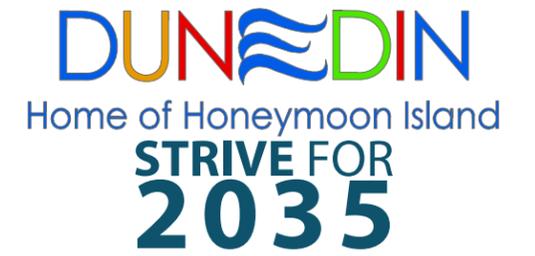
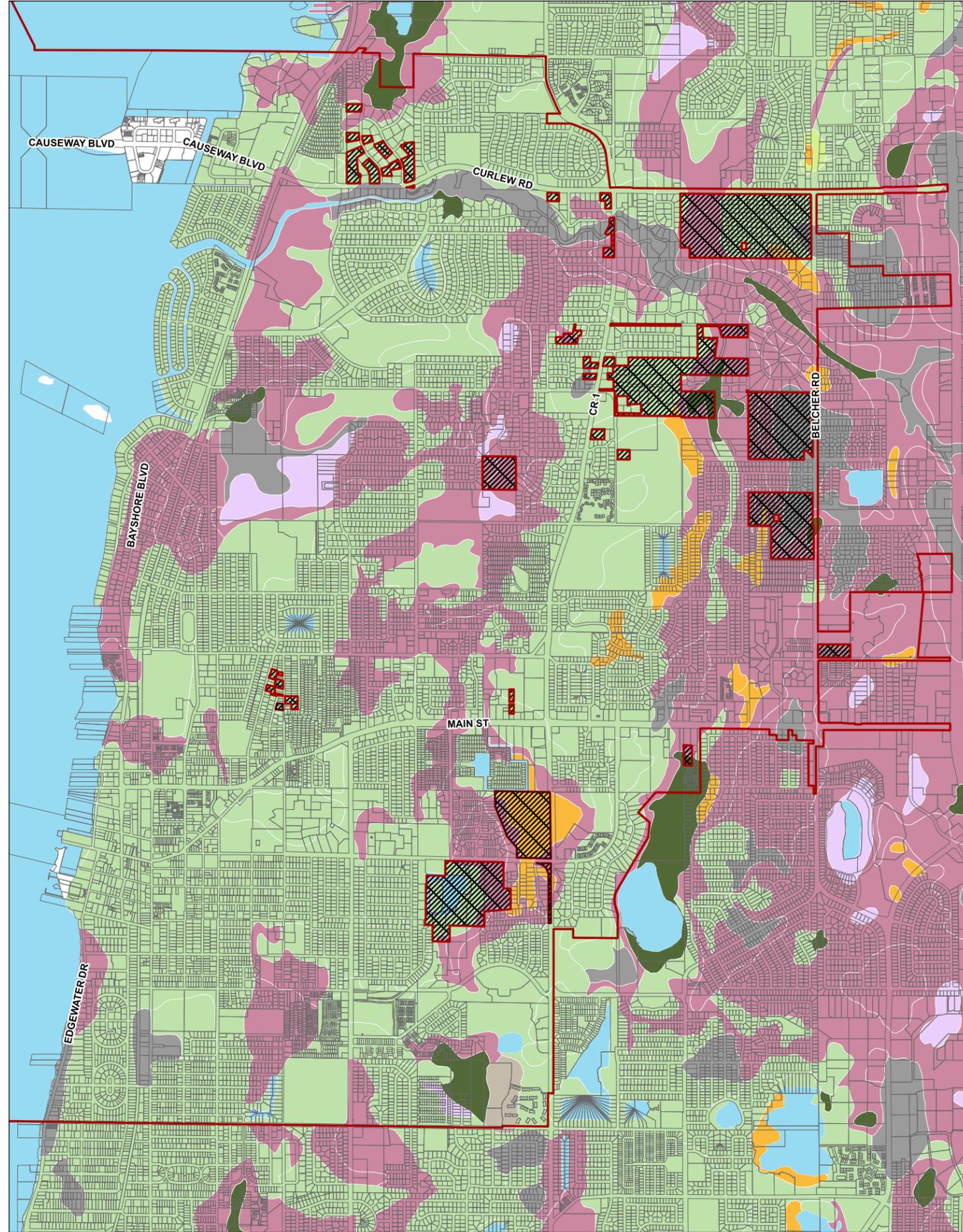


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STRIVE FOR 2035

- Legend**
- Elevation**
- 3.599 - 6.481
 - 6.481 - 16.562
 - 16.562 - 26.642
 - 26.642 - 36.722
 - 36.722 - 46.802
 - 46.802 - 56.883
 - 56.883 - 66.963
 - 66.963 - 77.043
 - 77.043 - 87.123
- Dunedin City Limits
 - City Limits Enclave

Source: Pinellas County 2019

Map 5-4 Coastal Management Element: Soil Map

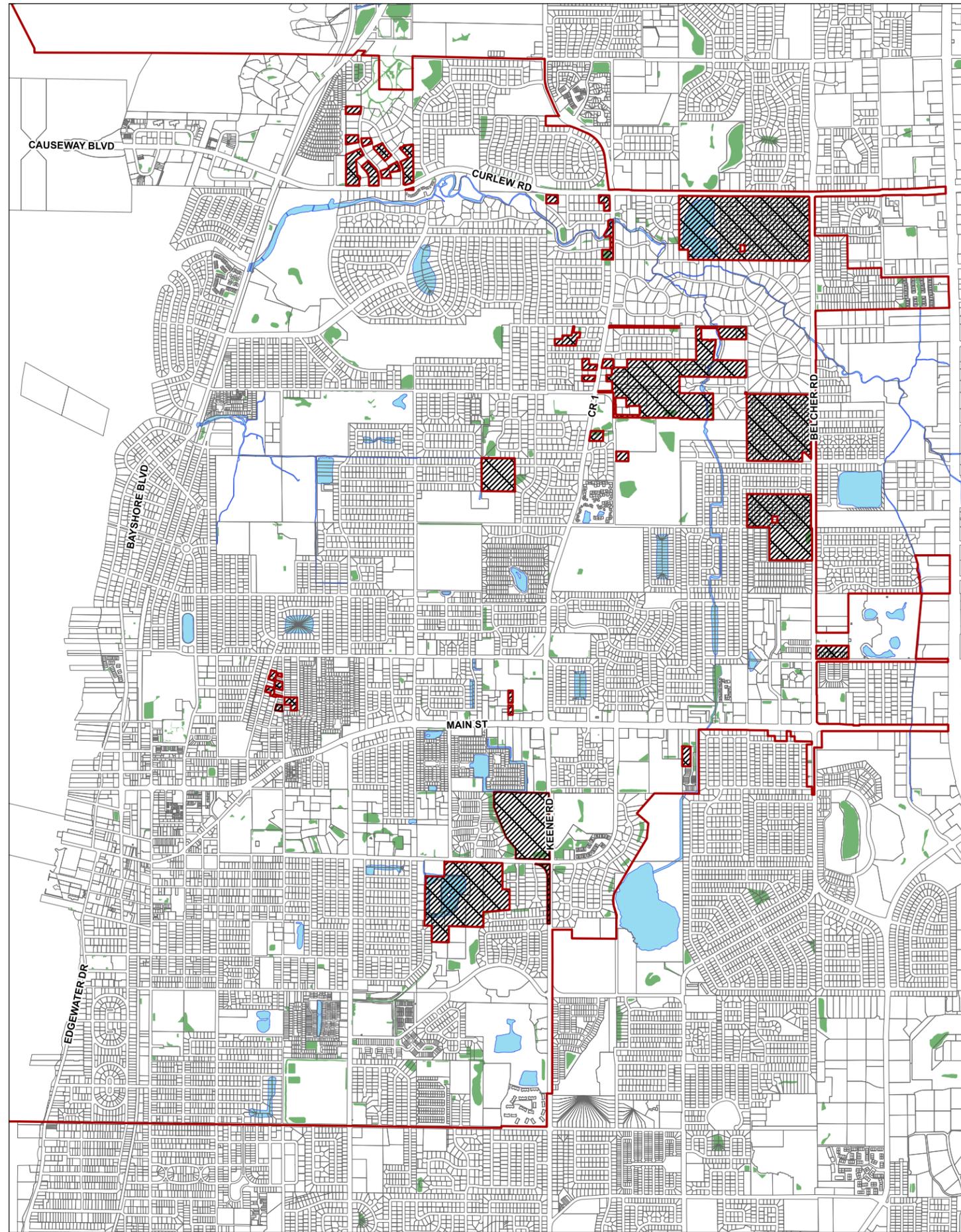
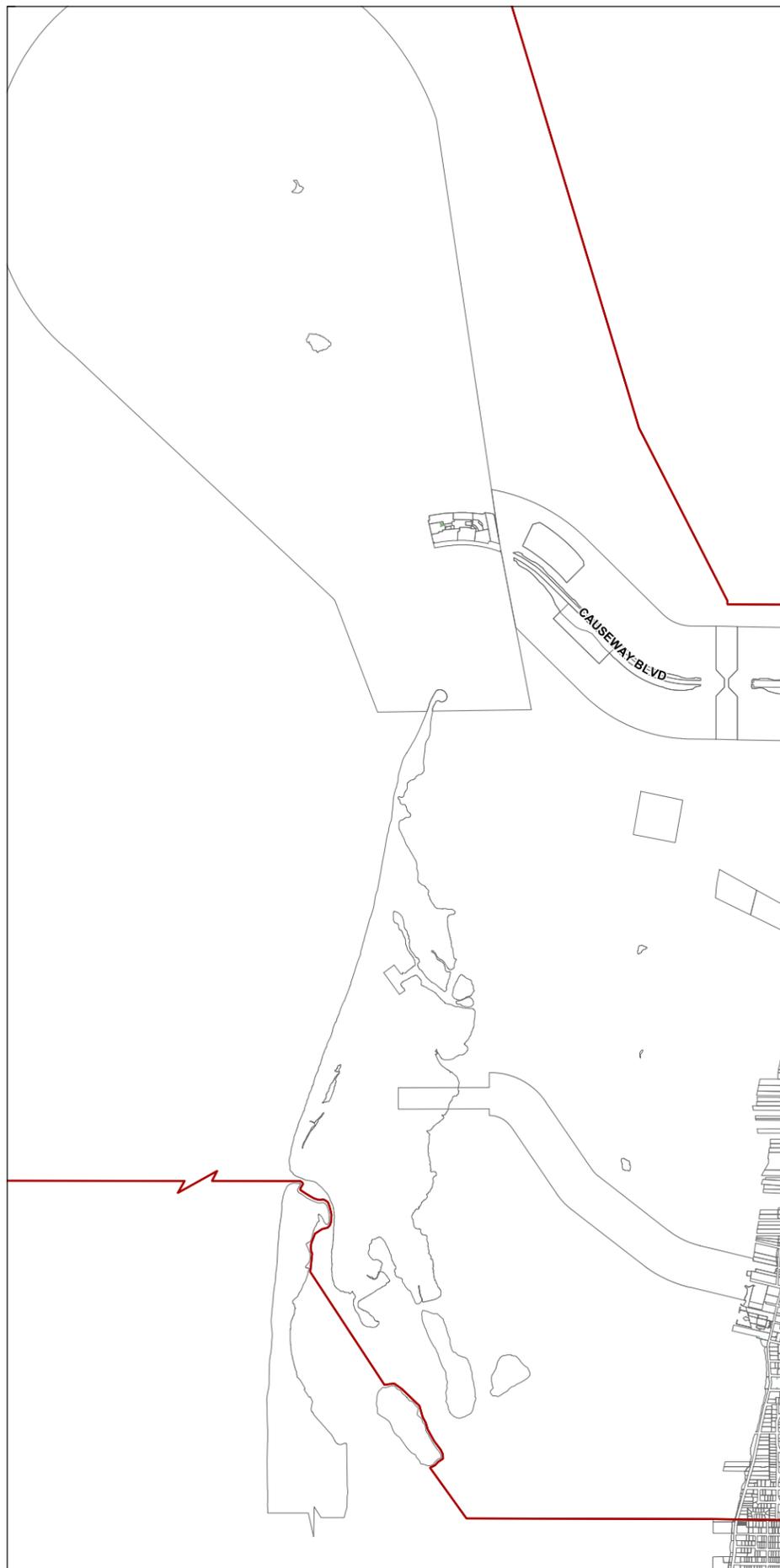


Legend

- Alfisols
- Bodies of Water
- Data Not Available
- Entisols
- Histosols
- Inceptisols
- Mollisols
- No Soil
- Spodosols
- Ultisols
- Vertisols
- Dunedin City Limits
- City Limits Enclave

Source: Florida Department of Environmental Protection 2019

Map 5-5 Coastal Management Element: Hydrological Features



1,250 625 0 1,250 Feet



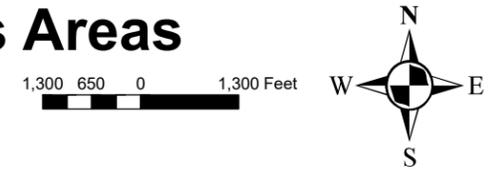
DUNEDIN
Home of Honeymoon Island
STRIVE FOR
2035

Legend

-  Stormwater Ponds
-  Waterbody
-  Dunedin City Limits
-  City Limits Enclave

Source: City of Dunedin 2019

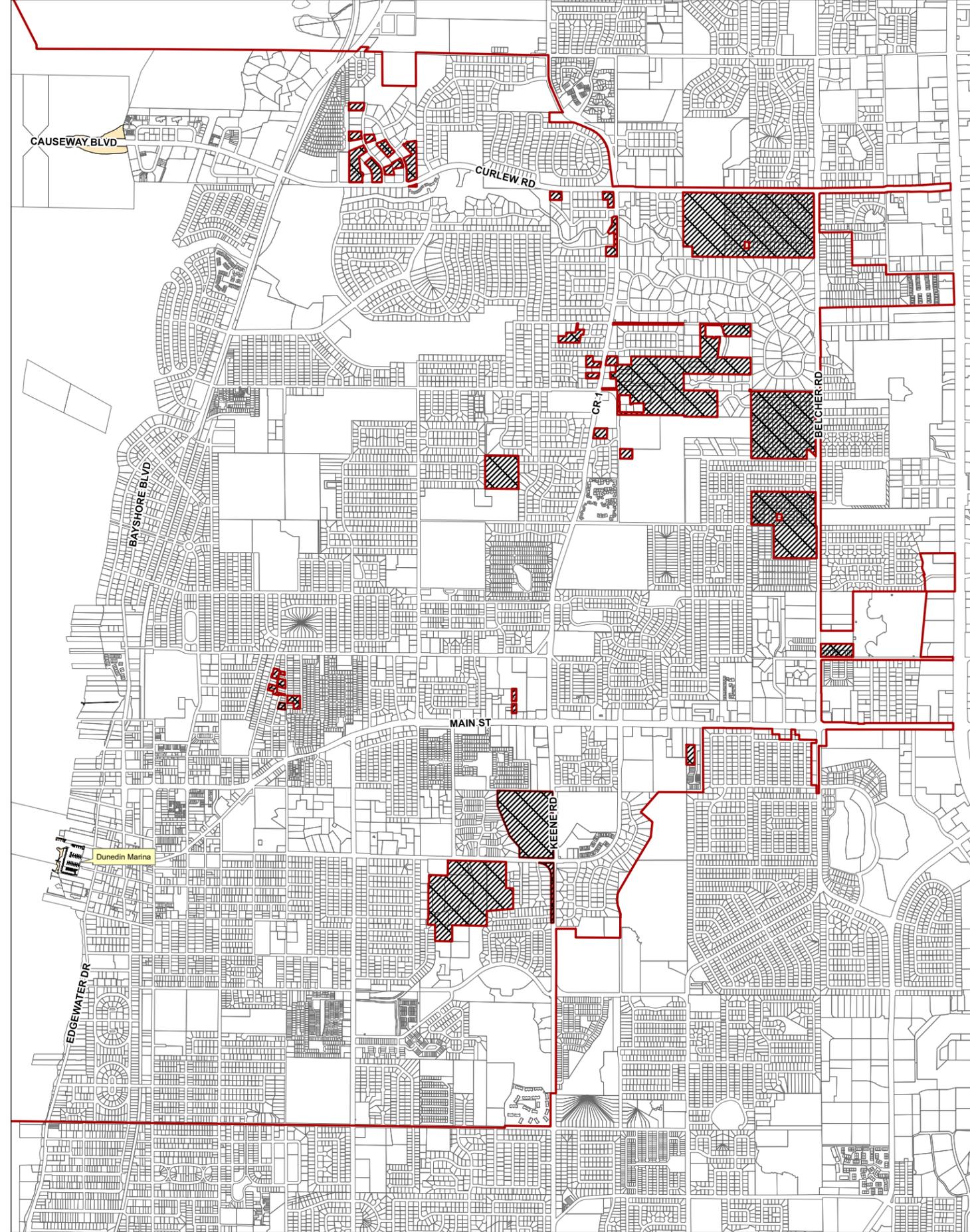
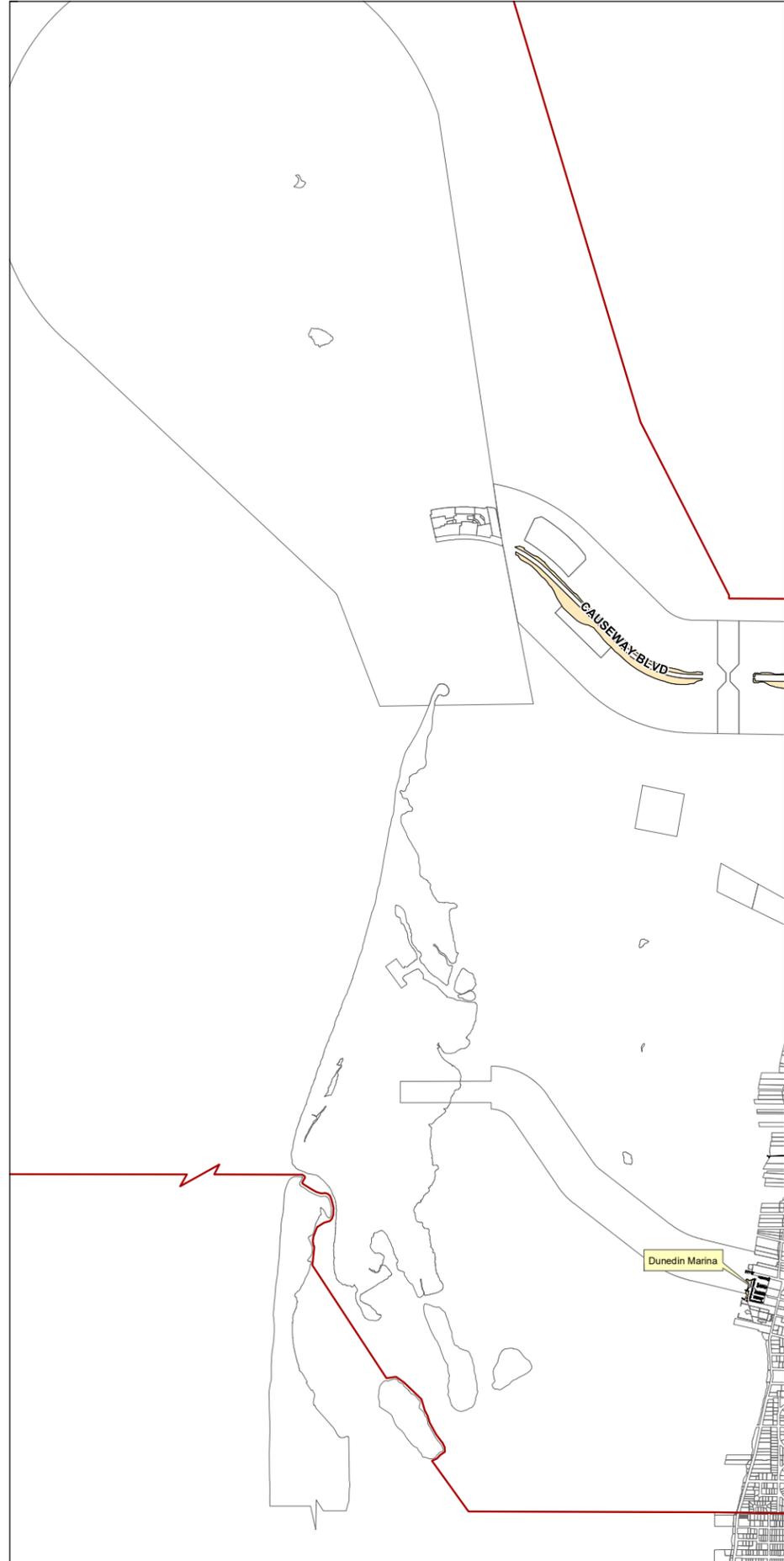
Map 5-6 Coastal Management Element: Public Waterways and Coastal Access Areas



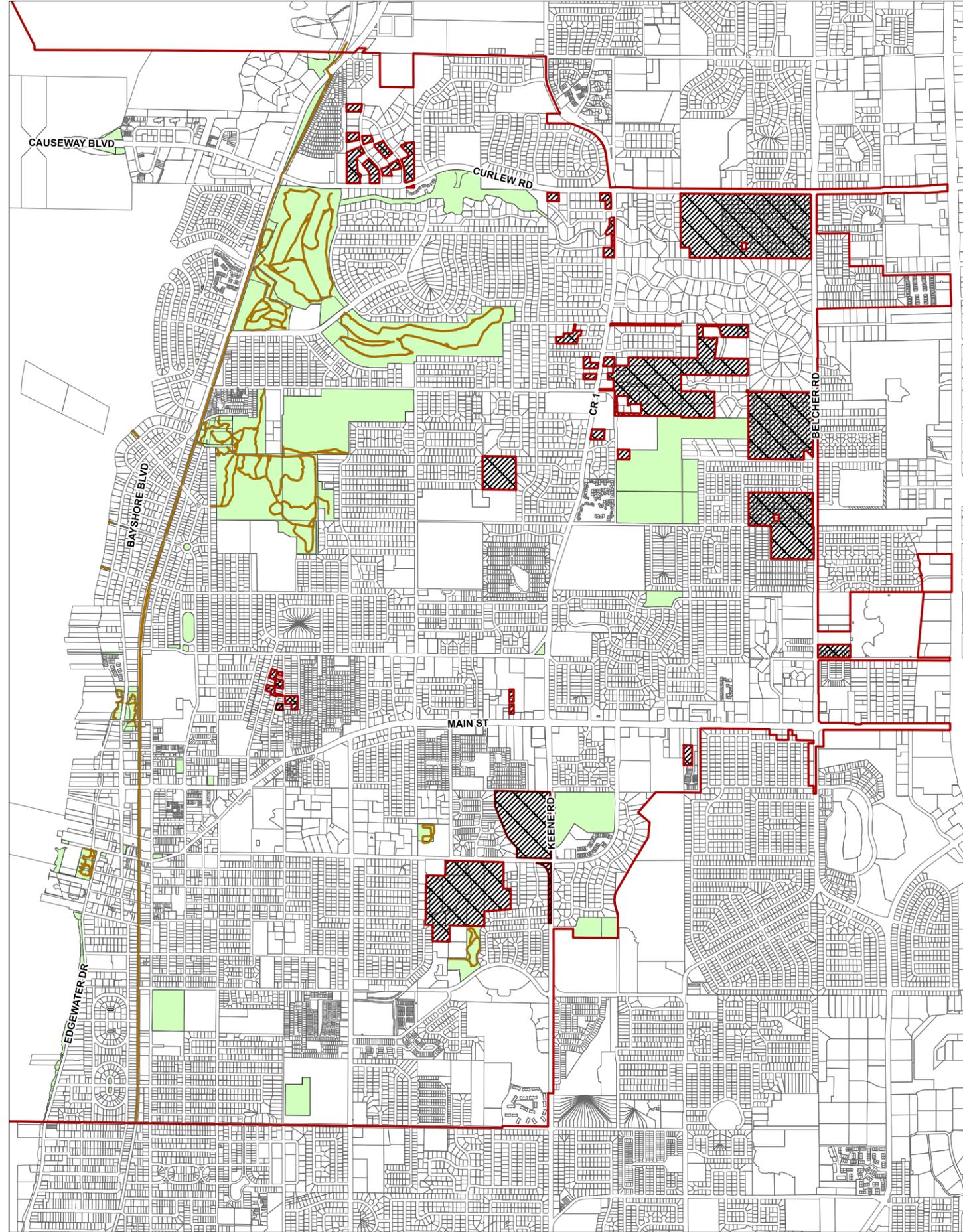
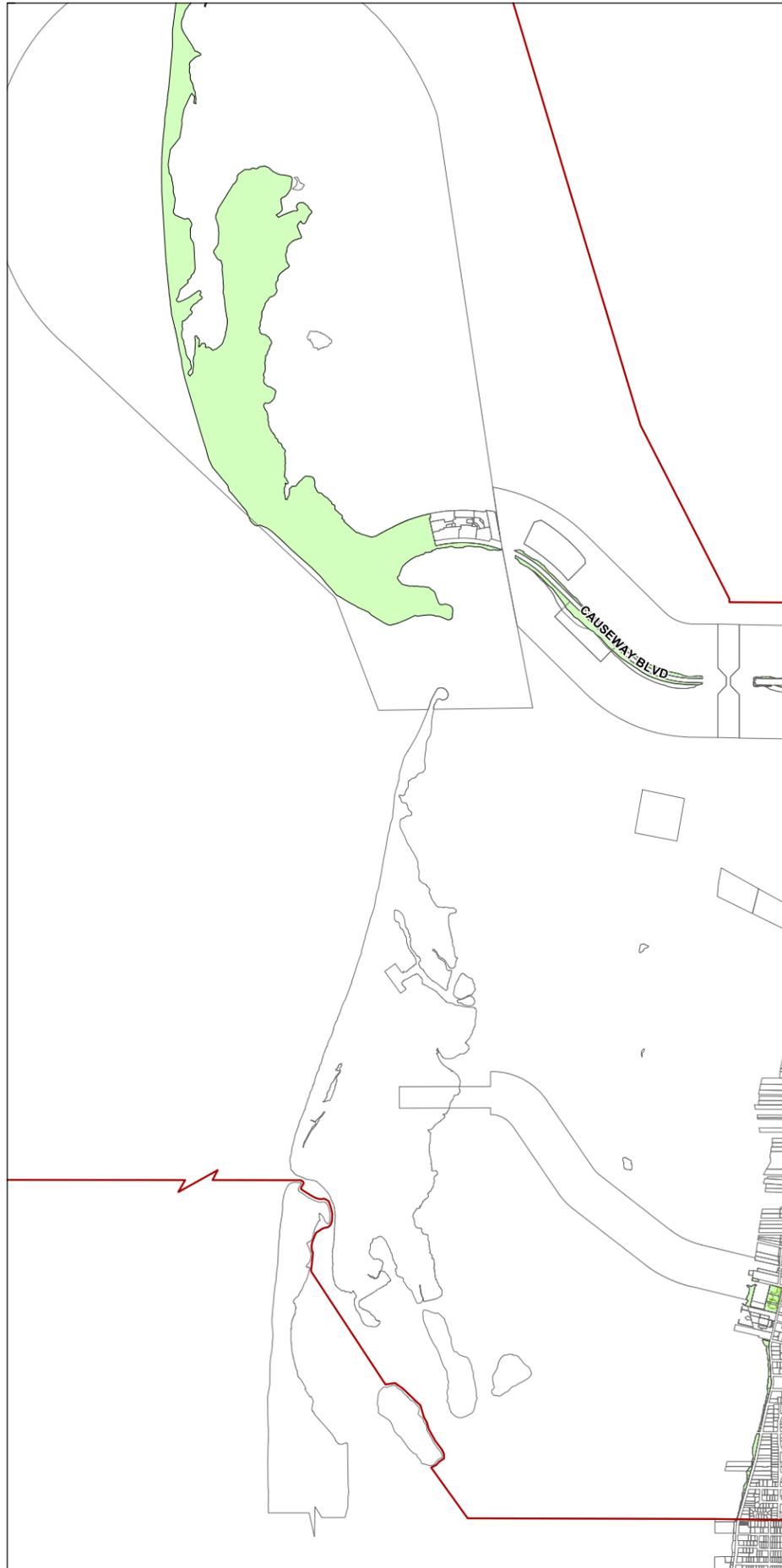
Legend

-  Boat Ramp
-  Beach/Sand
-  Docks/Decking
-  Dunedin City Limits
-  City Limits Enclave

Source: City of Dunedin 2019



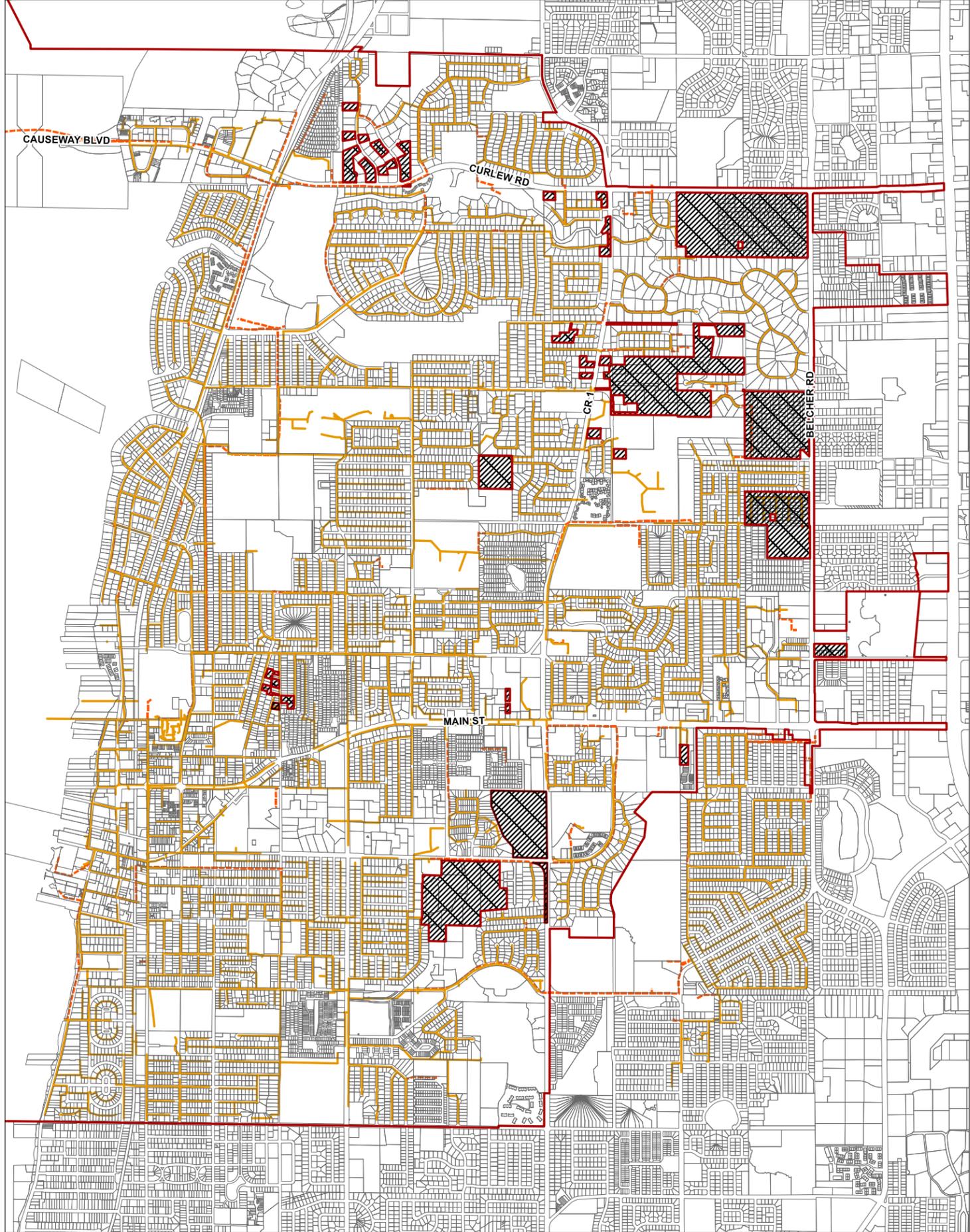
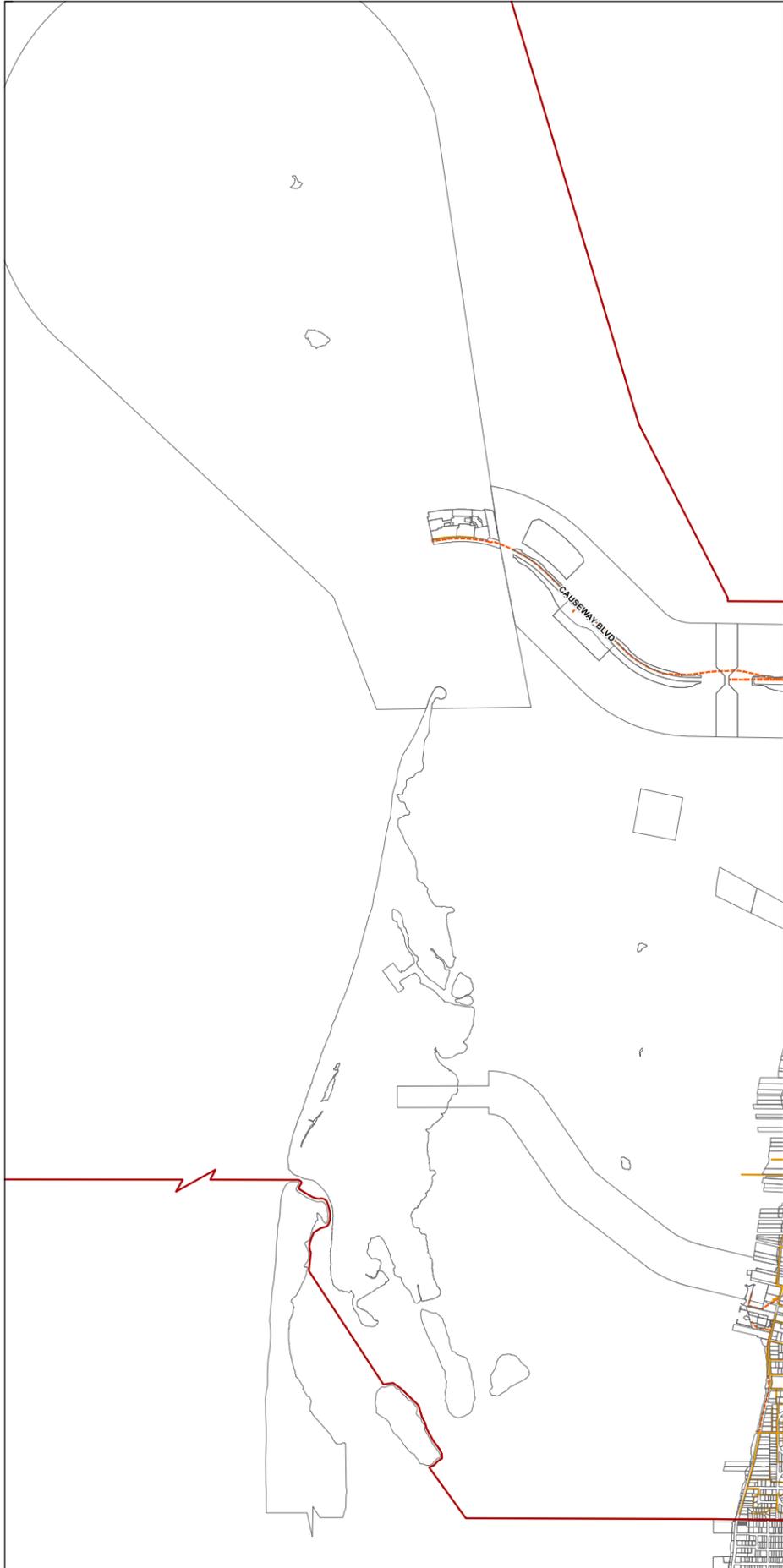
Map 6-1 Recreation & Open Space Element: Parks, Open Space, Urban Trails



DUNEDIN
Home of Honeymoon Island
**STRIVE FOR
2035**

- Legend**
- Trail
 - Park
 - Dunedin City Limits
 - City Limits Enclave
- Source: City of Dunedin 2019

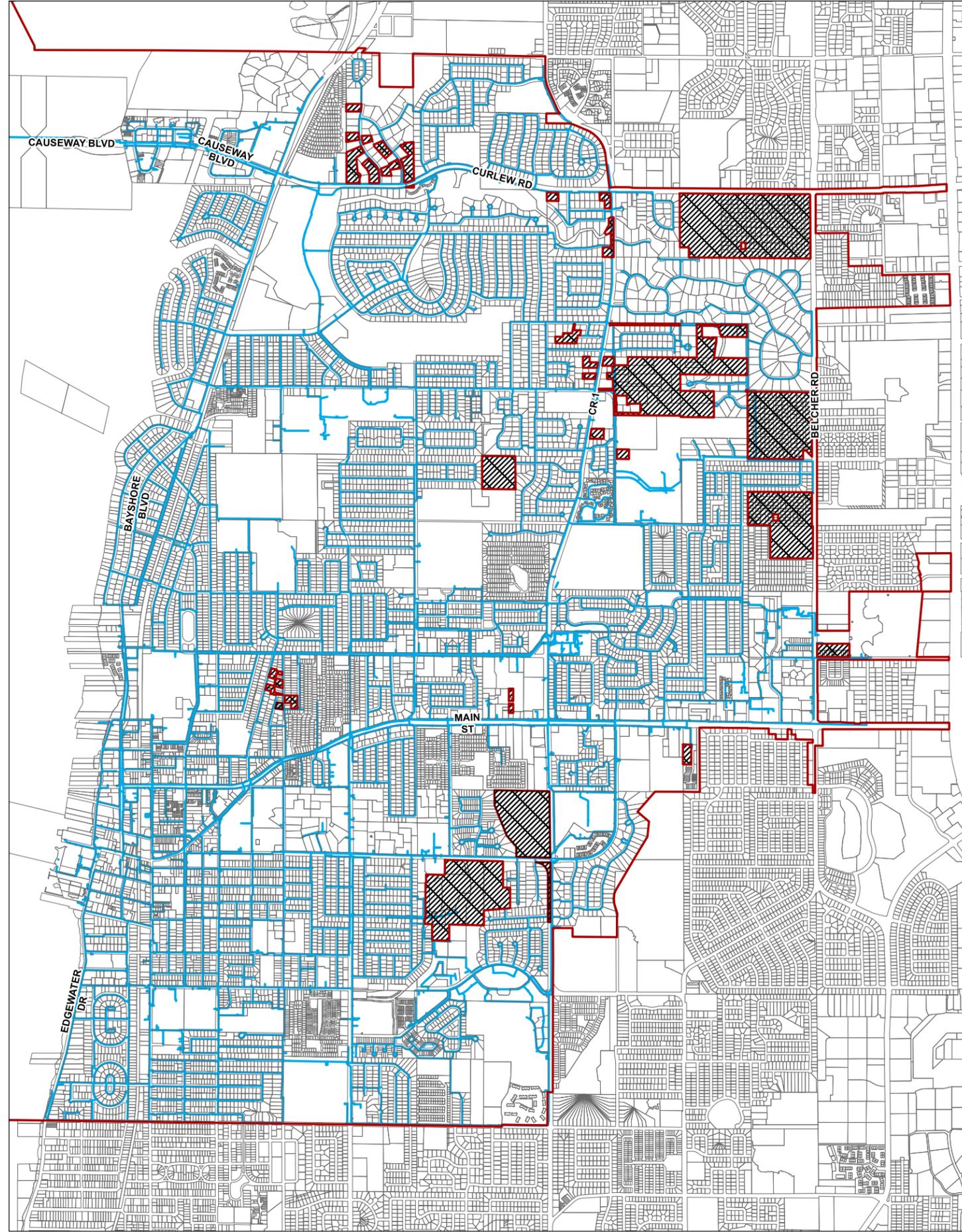
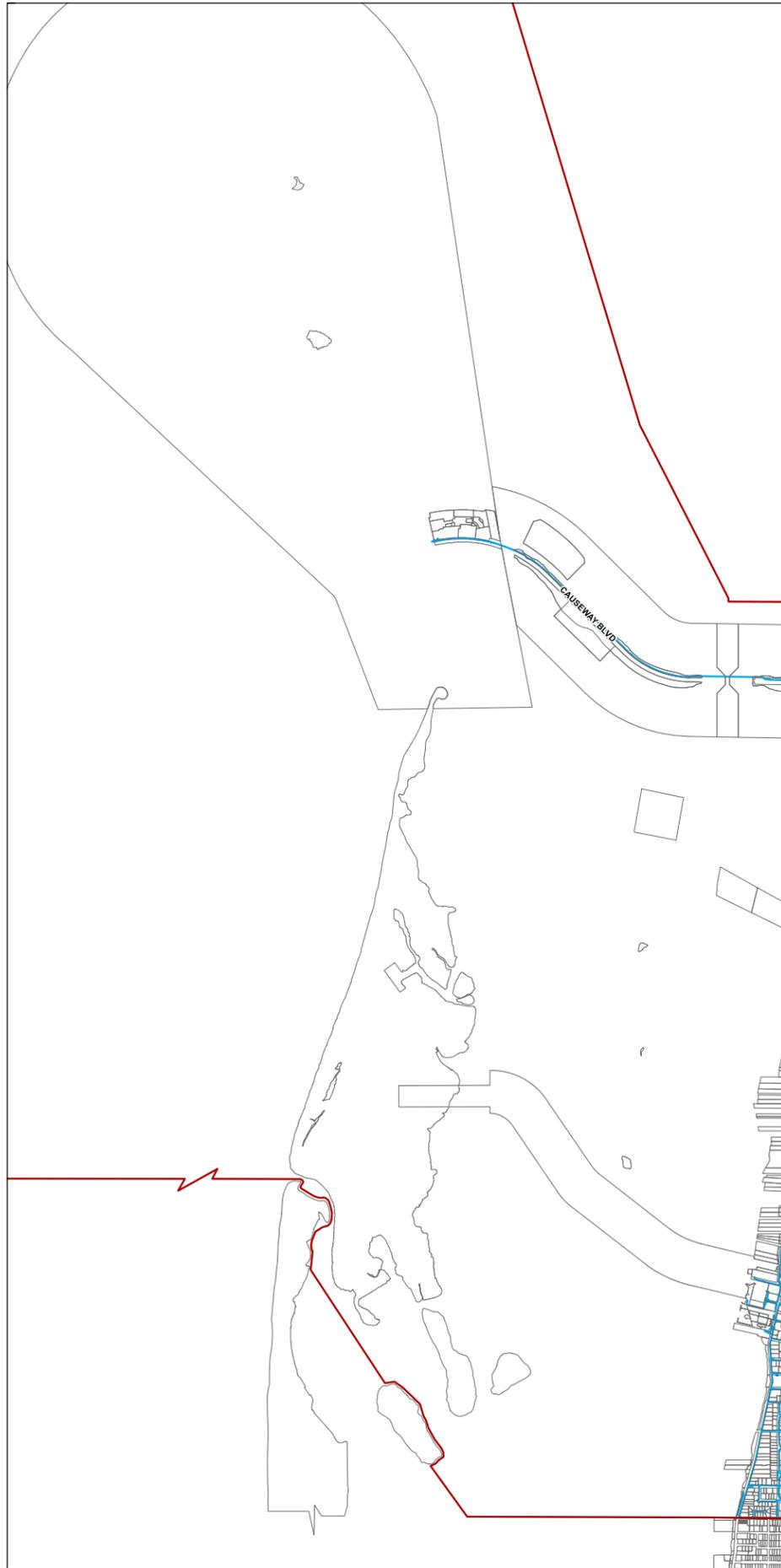
Map 7-1 Support Services Element: Wastewater System



- Legend**
- Gravity Main
 - Pressurized Main
 - Dunedin City Limits
 - City Limits Enclave

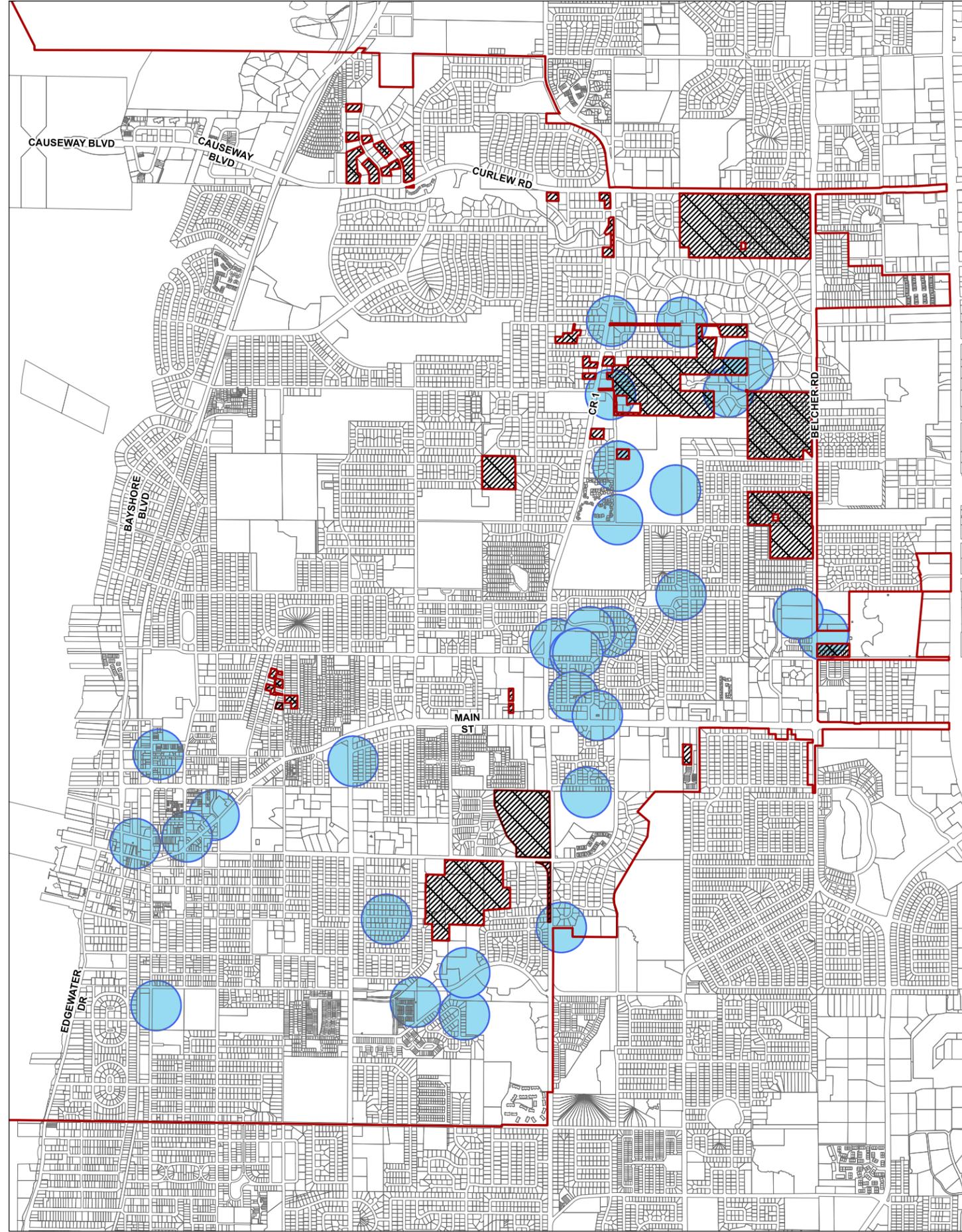
Source: City of Dunedin 2019

Map 7-2 Support Services Element: Potable Water System



- Legend**
- Main
 - Dunedin City Limits
 - City Limits Enclave
- Source: City of Dunedin 2019

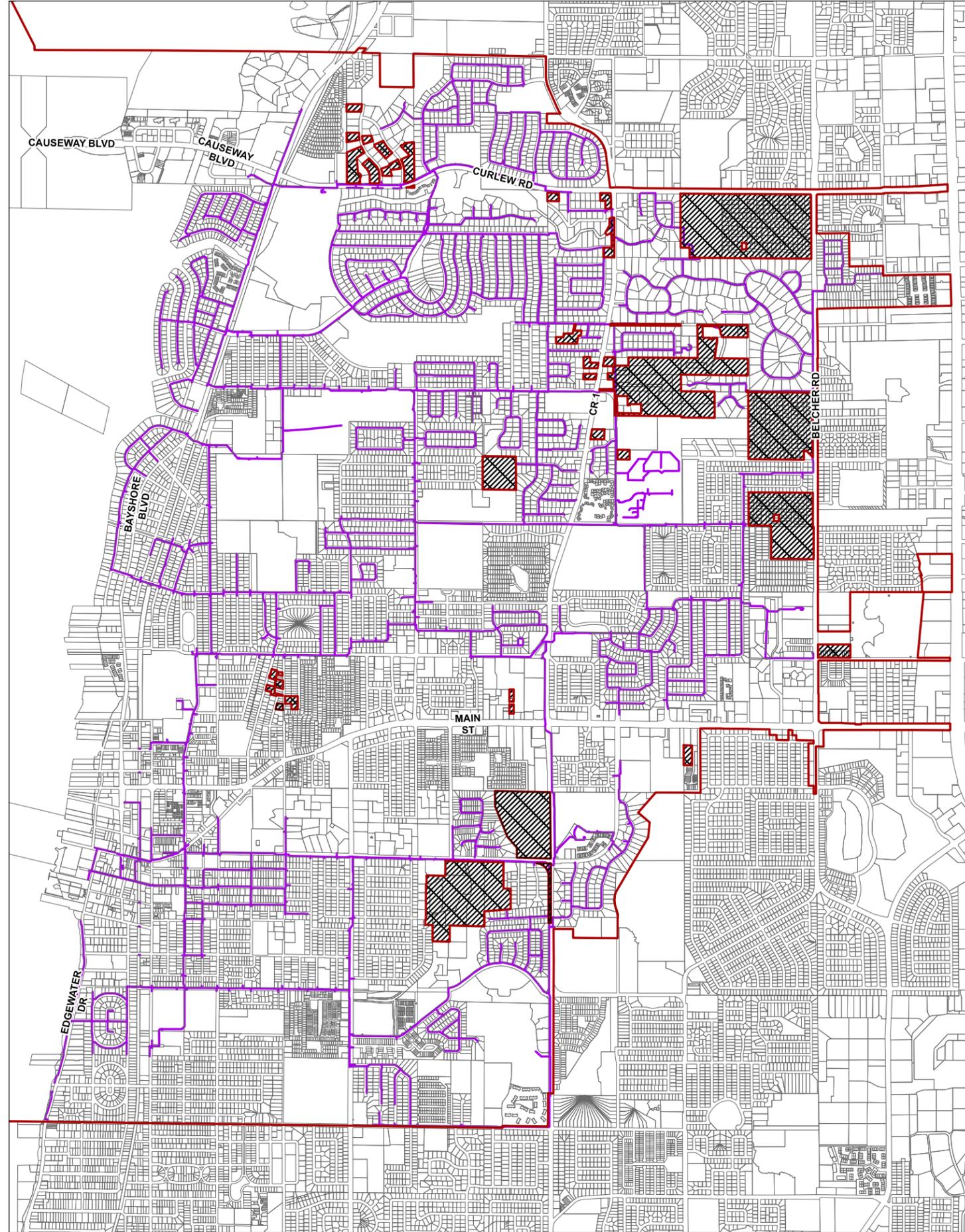
Map 7-3 Support Services Element: Wellhead Protection Areas



- Legend**
- Production Well 500' Graphic Buffer
 - Dunedin City Limits
 - City Limits Enclave

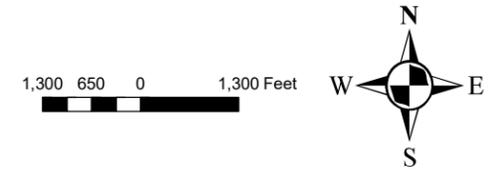
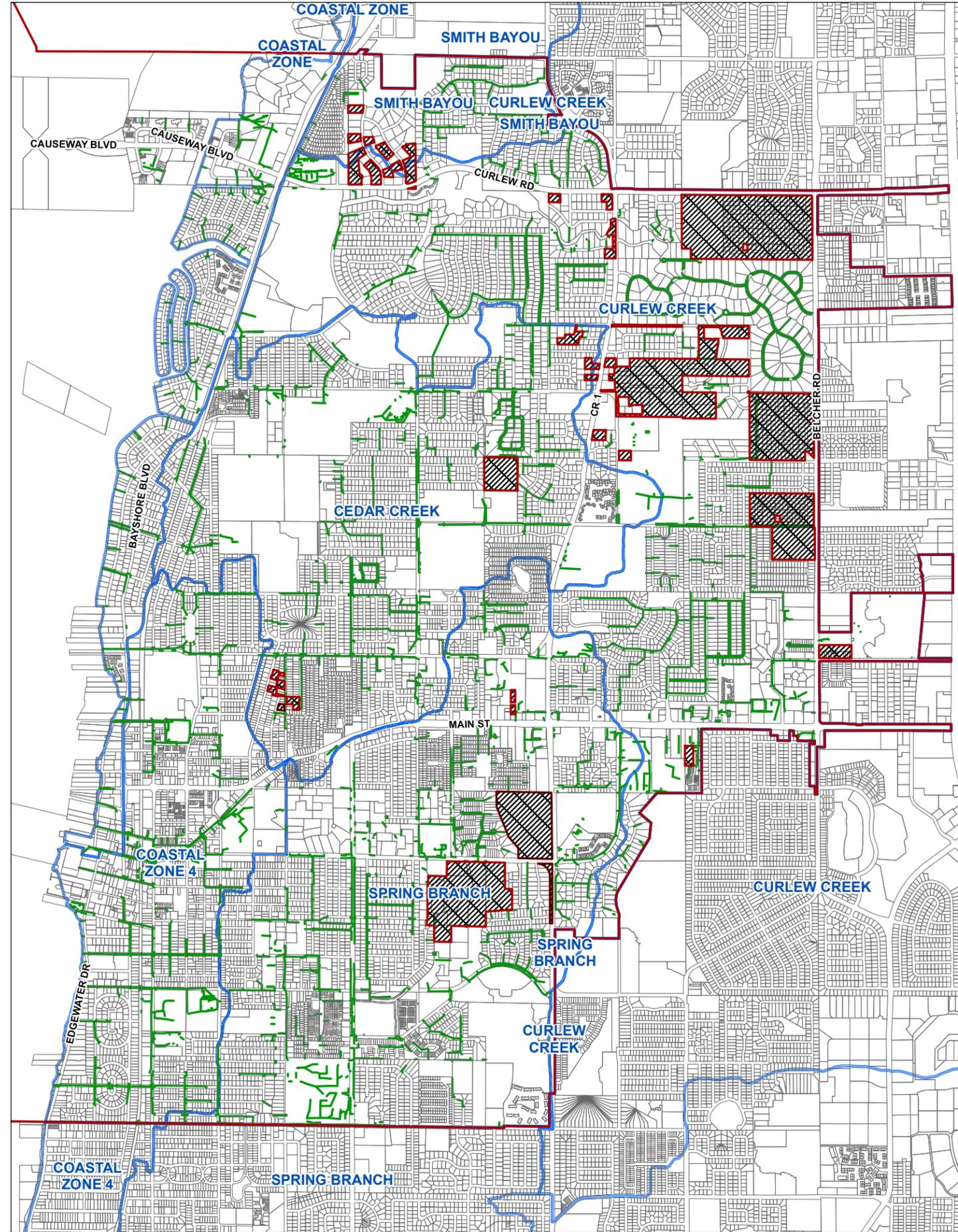
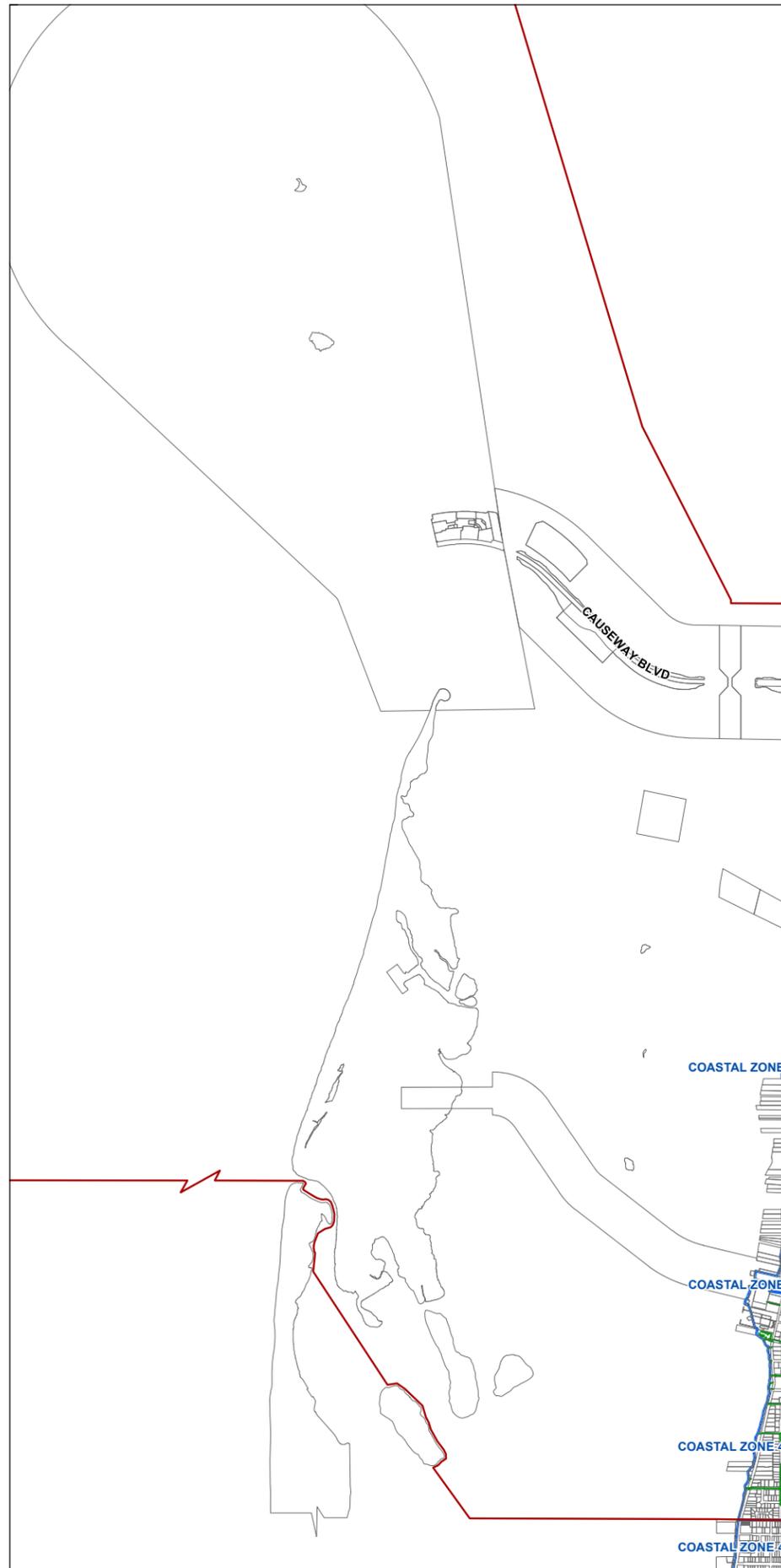
Source: City of Dunedin 2019

Map 7-4 Support Services Element: Reclaimed Water System



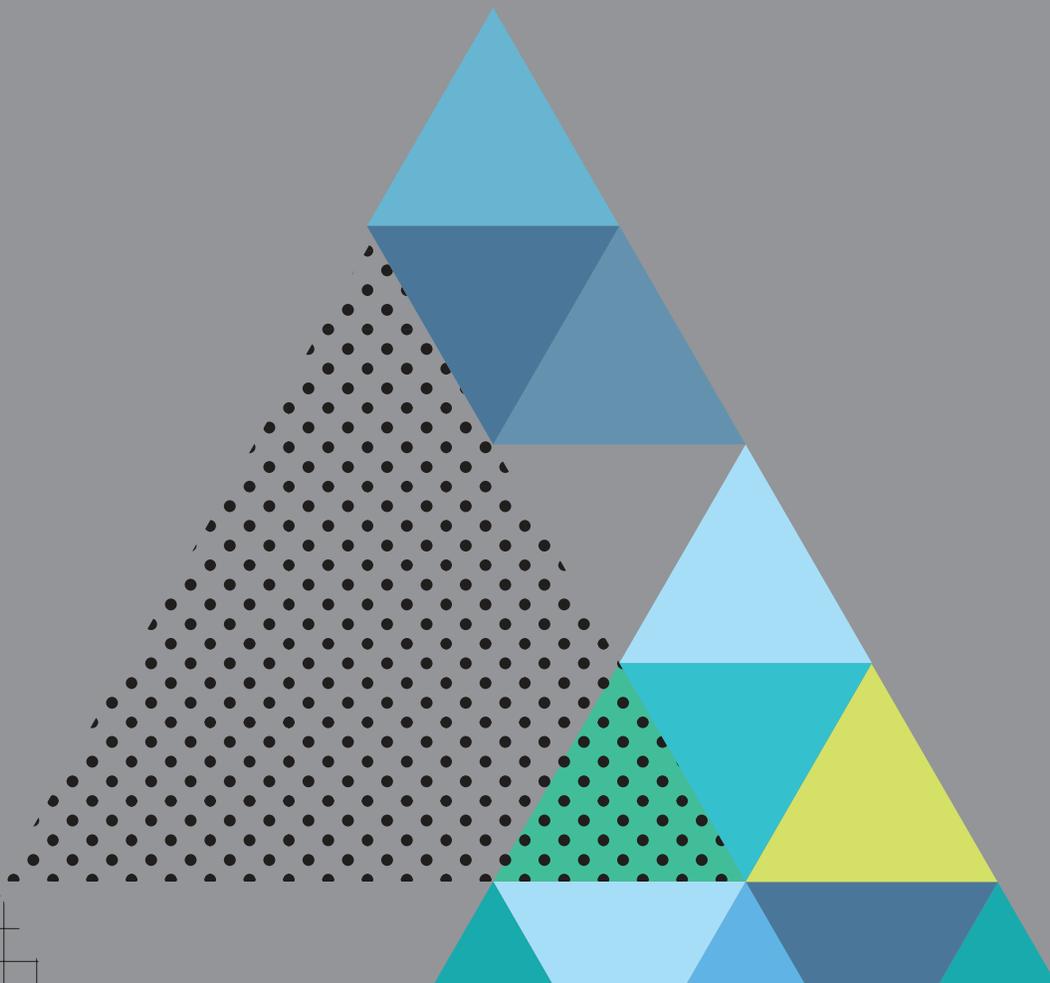
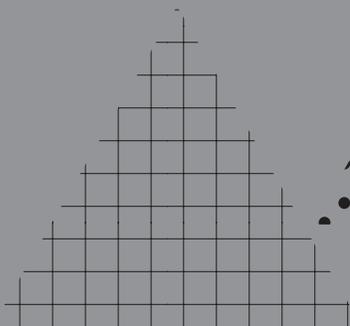
- Legend**
- Main
 - Dunedin City Limits
 - City Limits Enclave
- Source: City of Dunedin 2019

Map 7-5 Support Services Element: Major Drainage Basins and Facilities



- Legend**
- Watershed
 - GravityMain
 - Dunedin City Limits
 - City Limits Enclave

Source: Pinellas County & City of Dunedin 2019





APPENDIX
**DEFINITIONS/
ACRONYMS**



Definitions

A

Abutting: Having common borders or edges.

Accessory Dwelling Unit (ADU): A dwelling unit that is accessory, supplementary, and secondary to the principal dwelling, which may be constructed as an addition to the principal structure or as an accessory to the principal structure. An accessory dwelling unit is detached from the principal dwelling.

Activity Center: A contiguous area designated with the Activity Center category on the Countywide Plan Map, which is governed by locally-adopted plan or code provisions that identify the area as a unified location, and which serves as an important, identifiable center of business, public, and residential activity that is the focal point of a community, designed to accommodate multiple modes of transportation including enhanced transit.

Adaptive Reuse: The process of converting a building to a use other than which it was originally designed, e.g., changing a factory into commercial, retail use or residential use. Such conversions are accomplished with varying alterations to the building.

Adjacent: To have property lines or portions thereof in common or facing each other across a right-of-way, street or alley.

Adverse Impact (upon a natural resource): Direct contamination, alteration or destruction, or that contributes to the contamination, alteration or destruction of a natural resource, or portion thereof, to the degree that its present and future environmental benefits are, or will be, eliminated, reduced, or impaired.

Affordable Housing: Safe, sanitary and quality designed housing that is available to a household earning 120 percent or less of area median income, adjusted for family size, which can be rented or purchased at no more than 30 percent of household income.

Annexation: The legal method of attaching an area into an area controlled by another form of government.

Aquifer: A water-bearing layer of rock or soil that will yield water in usable quantity to a well or spring.

Aquifer Recharge: *See surficial aquifer and groundwater recharge.*

Arterial Roadway: A roadway providing service which is relatively continuous and of relatively high traffic volume, long trip length and high operating speed. In addition, every United States numbered highway is an arterial road.



B

Barrier Island: In Florida, a natural and usually vegetated sand island roughly paralleling the mainland shore and separated from it by a protected waterway or bay.

Beach: The zone of unconsolidated material that extends landward from the mean low water line to the place where there is marked change in material or physiographic form, or to the line of permanent vegetation, usually the effective limit of storm waves.

Benthic: Anything associated with or occurring on the bottom of a body of water.

Best Management Practices (BMPs): Design, construction, operational or maintenance techniques which are accepted by research institutes, professional societies or regulatory agencies as the most advanced and effective for any given application. For example, BMPs have been developed for stormwater pollution control, agricultural activities, silvicultural management and construction practices.

Bike Lane: A portion of a roadway which has been designated by striping, signing and pavement markings for the preferential or exclusive use of bicyclists.

Buffer: An area or zone between two land uses which is intended to ameliorate, reduce or mitigate the adverse effects one may have on the other. Examples include the placement of office parks between industrial uses and nature preserves, or the preservation of a naturally vegetated upland zone around wetlands as a part of a development.

Building: A structure created to shelter any form of human activity, such as an office, house, church, hotel or similar structure. Buildings may refer to a historically related complex such as courthouse and jail.

Built-out: Refers to a situation where every property in the City has some type of use and there is no vacant land left to develop. Built-out based on the Regulatory Land Use Plan means that every property is developed to the intensity allowed under the regulatory land use category assigned to the property.

C

Capacity (traffic): The measure of the ability of a traffic facility to accommodate a stream of moving vehicles, expressed as a rate. Thus, it is the maximum number of vehicles that have a reasonable expectation of passing over a given roadway in a given time period under the prevailing roadway and traffic conditions.

Capital Budget: The portion of each local government's budget which reflects capital improvements scheduled for a fiscal year.



Capital Improvement: Physical assets constructed or purchased to improve or replace a public facility and which are large scale, and which exceed \$50,000. The cost of a capital improvement is generally non-recurring and may require multi-year financing.

Capital Improvements Program (CIP): A five-year planning document that is reviewed annually for the construction of physical assets, such as buildings, streets, sewers and purchase of vehicles and equipment to meet capacity needs, maintain infrastructure, or enforce Levels-of-Service. The CIP includes all capital projects with costs of \$100,000 or greater.

Carbon Footprint: The amount of carbon dioxide or other carbon compounds emitted into the atmosphere by the activities of an individual or jurisdiction.

Character: An attribute, quality, or property of a place, space or object; its distinguishing features.

Clustering: The practice of grouping permitted types of residential and/or non-residential uses close together rather than distributing them evenly throughout the site while remaining at/or below the appropriate gross density ceiling in order to encourage creative site planning and/or protect natural resources.

Coastal Construction Control Line (CCCL): An imaginary line depicting the landward extent of that portion of the beach-dune system which is subject to severe fluctuations based upon predictable weather conditions as established by the Florida Department of Natural Resources (now the Florida Department of Environmental Protection).

Coastal High Hazard Area (CHHA): The area below the elevation of the Category 1 storm surge line as established by the Sea, Lake, and Overland Surges from Hurricanes (SLOSH) computerized storm surge model.

Coastal Zone: The coastal waters (containing a measurable percentage of sea water) and the adjacent shore lands, strongly influenced by each other.

Collector Roadway: A roadway classification that provide both land access and traffic circulation service within residential, commercial and industrial areas. The primary function is to move traffic from local roads to the arterial highway system, while providing some direct access to abutting property. While not dominated by signalized intersection traffic control, these facilities do tend to have more frequent intersection control such as stop and yield signs.

Commercial Uses: Activities within land areas which are predominantly connected with the sale, rental and distribution of products or performance of services.





Community Development Block Grant (CDBG): Program provides annual grants on a formula basis to states, cities, and counties to develop viable urban communities by providing decent housing and a suitable living environment, and by expanding economic opportunities, principally for low- and moderate-income persons. The program is authorized under Title 1 of the Housing and Community Development Act of 1974, Public Law 93-383, as amended 42 U.S.C.-530.1 et seq.

Community Redevelopment Agency (CRA): *See Community Redevelopment District.*

Community Redevelopment Area: A blighted area or an area in which there is a shortage of housing that is affordable to residents of low or moderate income, including the elderly, or a combination of which the City designates as a redevelopment area.

Community Redevelopment District (CRD): In accordance with Chapter 163, F.S., Part III, an area of the City in which a formal designation by the City Commission has been made, with concurrence by the Board of County Commissioners, that the area has been determined to be either a slum area, a blighted area, or an area with a shortage of affordable housing, and a CRD plan has been adopted.

Community Trails: Paths that provide both recreation and alternative transportation opportunities for residents to gain public access to the urban trail network.

Compact Infill Development: The addition of new housing or other buildings on scattered vacant sites or platted lots in a developed areas or subdivision.

Compatibility: Compatibility is defined as the characteristics of different uses or activities or design which allow them to be located near or adjacent to each other. Some elements affecting compatibility include the following: height, scale, mass and bulk of structures, pedestrian or vehicular traffic, circulation, access and parking impacts, landscaping, lighting, noise, odor and architecture. Compatibility does not mean “the same as.” Rather, it refers to the sensitivity of development proposals in maintaining the character of existing development.

Complete Streets: Roadways that are designed with the safety of all users in mind, including but not limited to motorists, pedestrians, bicyclists, and transit users.

Comprehensive Plan: An official document in ordinance form adopted by the local government setting forth its goals, objectives and policies regarding the long-term development of the area within its jurisdiction pursuant to Chapter 163.3161, et seq, Florida Statutes, as amended.

Concurrency: The legal requirement that specified public facilities (recreation and open space, potable water, sanitary sewer, solid waste, stormwater management, transportation) to be provided for, by an entity to an adopted level of service.

Concurrent: Occurring at the same time, i.e., support services shall be provided concurrently with development approval.

Conservation: The careful preservation, protection and planning for a natural resource to prevent exploitation, destruction or neglect.

Consistency: The regulatory requirement that local Comprehensive Plans not conflict with State or regional plans, and that the local plan furthers the goals and policies of the State and regional plans.

Consumptive Use: Withdrawal and use of water in such a way as to make the water unavailable for immediate reuse. The use of potable water by humans is a consumptive use since the result (wastewater) cannot be immediately reused. Use of water by powerplants usually is not a consumptive use since the water can be immediately recycled or returned to the source.

Context Sensitivity (Context Sensitive Design): Providing a transportation facility that fits its setting. It is an approach that leads to preserving and enhancing scenic, aesthetic, historic, community, and environmental resources, while improving or maintaining safety, mobility, and infrastructure conditions.

Corridor: The area within one-half mile of the road centerline and within a one-half mile arc radius beyond the terminus of the road segment centerline, and includes properties that are subject to at least one of the following conditions:

- a) Sole Direct Access – A condition where the only means of site ingress/egress is directly onto the road facility, regardless of the distance of that site from the facility.
- b) Direct Access – A condition in which one or more existing or potential site ingress/egress points make a direct connection to the road facility and the site is within one-half mile of the road facility.
- c) Sole Indirect Access – A condition where the only point of site ingress/egress is onto a public non-arterial roadway which makes its first and shortest arterial level connection onto a road facility regardless of the distance of that site from the facility.

D

Deficient Roadway: A road operating at peak hour level of service E or F, and/or a volume-to-capacity (v/c) ratio of 0.9 or higher with no mitigating improvements scheduled within three years.

Demographic: Relating to the dynamic balance of a population regarding density, capacity, and distribution for expansion or decline.

Density: A measure of the intensity of development expressed as the average number of dwelling units per unit of area (acre, square mile, etc.). Density can also be expressed in terms of population density (i.e., people per acre). Used as a measurement of dwelling units per net land area- DU/AC.

Density Bonus: The allocation of development rights that allow a parcel to accommodate additional square footage or additional residential units beyond the maximum for which the parcel is zoned, usually in exchange for the provision or preservation of an amenity at the same site or at another location.

Detention: The collection and temporary storage of stormwater in such a manner as to provide for control of peak runoff discharges and velocities to levels permitted by the appropriate water management district. Where detention is used as a method of controlling stormwater pollution, water is detained in such a manner as to provide for treatment through physical, chemical or biological processes, as well as whatever water quantity control is needed. "Detention" implies that a surface water outlet from the system exists except in the case of "detention with filtration," in which stormwater is passed through a layer of suitable fine textured granular medium, such as porous soil, which may be used in conjunction with filter fabric and/or underdrain pipe. *See also retention.*

Deterioration: The process by which structures and their components wear, age and decay in the absence of regular repairs and/or replacement or components which are worn or obsolete.

Development: The construction, reconstruction, conversion, structural alteration, relocation, or enlargement of any structure; the making of any material change in the use or appearance of any structure or land, or the dividing of land into three or more parcels; any mining, excavation, landfill, or land disturbance; and any nonagricultural use or extension of the use of land. It includes redevelopment.

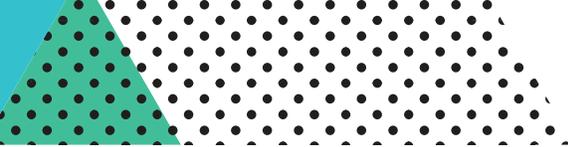
Development Order: Any order granting, denying, or granting with conditions, an application for a development permit.

Development Permit: Formal permission to erect, construct, alter, raze, move, remove, or otherwise develop land within the City. This includes, but is not limited to, building permits, sign permits, tree removal permits, etc.

Drainage Basin: The area defined by topographic boundaries which contributes stormwater to a drainage system, estuarine waters, or oceanic waters, including all areas artificially added to the basin.

Dredge and Fill: The process of excavation or deposition of ground materials by any means, in local, State or regional jurisdictional waters (including wetlands), or the excavation or deposition of ground materials to create an artificial waterway that is to be connected to jurisdictional waters or wetlands (excluding stormwater treatment facilities).

Dwelling Unit (DU): A room or group of rooms forming a single independent habitable unit used for or intended to be used for living, sleeping, sanitation, cooking and eating purposes by one family only; for owner occupancy or for rental, lease, or other occupancy on a weekly or longer basis; and containing independent kitchen, sanitary and sleeping facilities.



E

Ecological Community: A specific and distinct interrelationship between plant and animal life and their surrounding environment.

Economic Development: Any business creation, expansion, relocation, or investment, which results in an addition to the economic activities of an area.

Emergency Management Plan: A citywide strategy which establishes the framework through which the City may prepare for, respond to, recover from, and mitigate the impacts of a wide range of disasters. This plan is intended to advance planning to prepare for and/or react to natural or man-made emergency situations with potential for disastrous consequences.

Enclave: A geographical area in the County that is surrounded totally by the City of Dunedin.

Endangered Species: Species in danger of extinction if the deleterious factors affecting their populations continue to operate, since their numbers have already declined to such a critically low level or their habitats have been so seriously reduced or degraded that without active assistance their survival in Florida is questionable. Includes those species listed as “endangered” by the U.S. Department of the Interior, Florida Game and Freshwater Fish Commission and the Florida Department of Agriculture and Consumer Services.

Environmentally Sensitive Areas: Lands that, by virtue of some qualifying environmental characteristic (e.g., wildlife habitat), are regulated by either the Florida Department of Environmental Protection, the Southwest Florida Water Management District, or any other governmental agency empowered by law for such regulation.

Evacuation Routes: Routes designated by county civil defense authorities or the regional evacuation plan, for the movement of persons to safety in the event of a hurricane.

Evacuation Zones: Based on hurricane storm surge zones vulnerable to different levels of flooding determined by the National Hurricane Center. Storm surge is produced by water being pushed towards the shore by the force of winds moving around the storm. Each evacuation zone is coded to designate the potential surge height likely to occur (see also Map 5-2):

- a) Evacuation Level “A” Zone- Up to 11 feet; evacuate red areas and all mobile homes;
- b) Evacuation Level “B” Zone- Up to 15 feet; evacuate red and orange areas and all mobile homes;
- c) Evacuation Level “C” Zone- Up to 20 feet; evacuate red, orange and yellow areas and all mobile homes;
- d) Evacuation Level “D” Zone- Up to 28 feet; evacuate red, orange, yellow and green areas and all mobile homes;
- e) Evacuation Level “E” Zone- Up to 35 feet; evacuate red, orange, yellow, green and purple areas and all mobile homes.

Existing Urban Service Area: Built-up areas where public facilities and services such as wastewater treatment systems, roads, schools and recreation areas are already in place.

F

Financial Feasibility: Per F.S. §163.3180(15)(c), “A determination of financial feasibility shall be based upon currently available funding or funding sources that could reasonably be expected to become available over the planning period.”

Floodplain: An area inundated during a 100-year flood event or identified by the National Flood Insurance Program as an A-zone or V-zone on Flood Insurance Rate Maps or Flood Hazard Boundary Maps.

Floor Area Ratio (FAR): Measurement of non-residential development including all buildings, structures or similar as compared the net land area of the property or parcel on which it is located. A ratio measured in square feet to the area of a parcel of land, excluding any bonus or transferred floor area.

Florida Department of Environmental Protection (FDEP): The Florida Department of Environmental Protection is the State’s lead agency for environmental management and stewardship, protecting our air, water, and land.

Florida Department of Transportation (FDOT): The State agency responsible for administering transportation services and improvements, as well as the coordinating agency between Federal and local agencies on transportation planning and implementation initiatives.

Florida Green Building Coalition (FGBC): A nonprofit Florida corporation dedicated to providing Statewide green building program that defines, promotes and encourages sustainable efforts with environmental and economic benefits.

Florida State Implementation Plan (SIP): A plan prepared by State government and approved by the United States Environmental Protection Agency (USEPA), outlining how a company and/or community plans will reduce emissions during a specific time period, often three years, so the concentration of certain materials measured in the air are within standards set by the USEPA. The plans are mandated by an amendment to the Federal Clean Air Act and monitored by State environmental agencies.

Form: In urban design, the perceived, three-dimensional shape of topography, buildings, or landscaping.

Form-Based Code: A land development regulatory document that fosters predictable built results and a high-quality public realm by using physical form (rather than separation of uses) as the organizing principle for the code. A form-based code is a regulatory document, not a mere guideline, adopted into city, town or county law.

Forward Pinellas: A land use and transportation planning agency that addresses Countywide concerns as both the Pinellas Planning Council and Pinellas County Metropolitan Planning Organization. Forward Pinellas only provides a forum for Countywide decision-making on transportation and land use issues, but also assists local governments with technical support, regional coordination and policy advice and guidance.

Future Land Use Map: The graphic aid intended to depict the spatial distribution of various uses of the land in the City by land use category, subject to the goals, objectives and policies and the exceptions and provisions of the Future Land Use Element text and applicable development regulations.

G

Gateway: An architectural feature, hardscape, or landscaping that signifies a transition between one space and another.

Greenway: A natural corridor or area containing vegetation or natural ground and surface water conditions intended to be acquired for conservation and/or non-motorized mobility such as bicycling or pedestrian ways which generally provides connections between larger areas of open space such as conservation tracts, parks and other activity centers.

Groundwater Recharge Areas: *See recharge area.*

Group Home (Adult Congregate Living Facility): A residential facility which provides a family-living or homelike environment for compensation, including supervision and personal service necessary to meet the physical, emotional and social needs required because of the health, physical limitations or other special characteristics of the resident. The capacity of such a facility shall be at least four unrelated, but not more than sixteen, residents.

Growth Management: A method to guide development in order to minimize adverse environmental and fiscal impacts and maximize the health, safety and welfare benefits to the citizens of the community.

H

Hazardous Waste: Solid waste, or a combination of solid wastes, which because of its quantity, concentration, or infectious characteristics, may cause, or significantly contribute to, an increase in serious irreversible or incapacitating reversible illness or may pose a substantial present or potential hazard to human health or the environment when improperly transported, disposed of, stored, treated, or otherwise managed.

Hazardous Waste Management: The systematic control of the collection, source, separation, storage, transportation, recovery and disposal of hazardous wastes.



Historic Preservation: The act of conservation or recreating the remnants of past cultural systems and activities that is consistent with original or historical character. Such treatment may range from a pure “restoration” to adaptive use of the site, but its historic significance is preserved.

Historic Resources: A building, structure, district, area, site, object, or document that is of significance in American, State, or local history, architecture, archeology, or culture and is listed or eligible for listing on the Florida Master Site File, the National Register of Historic Places or designated by local ordinance.

HOME: The HOME Investment Partnerships Program which is authorized by Title II of the National Affordable Housing Act. In general, under the HOME Investment Partnerships Program, HUD (Housing and Urban Development) allocates funds by formula among eligible State and local governments to strengthen public-private partnerships to provide more affordable housing. Generally, HOME funds must be matched by nonfederal resources.

Household: Includes all the persons who occupy a group of rooms or a single room which constitutes a housing unit.

Housing: Housing is basically shelter, but it is also privacy, location, environmental amenities, and, for many, an investment. This analysis, however, is limited to the shelter aspect of housing since there is no accurate way to measure the other components.

Housing Quality Standards (HQS): HQS helps the U.S. Department of Housing and Urban Development (HUD) and local Public Housing Authorities (PHAs) accomplish that goal by defining “standard housing” and establishing the minimum quality criteria necessary for the health and safety of program participants.

Housing Stock: The aggregate of individual housing units within the City. This term is used interchangeably with housing inventory in the study.

Housing Unit: A group of rooms or a single room is regarded as a housing unit when it is occupied as a separate living quarters, that is, when the occupants do not live and eat with any other persons in the structure, and when there is either: 1) direct access from the outside of the building or a common hall, or 2) complete kitchen facilities for the exclusive use of the occupants of the household.

Housing and Urban Development (HUD), Department of: The Department of Housing and Urban Development (HUD) is a U.S. government agency created in 1965 to support the housing market and homeownership. HUD does this by improving affordable homeownership opportunities, increasing safe and affordable rental options, reducing chronic homelessness, fighting housing discrimination by ensuring equal opportunity in the rental and purchase markets and supporting vulnerable populations.

Human Scale: A combination of qualities in architecture of the landscape that provides an appropriate relationship to human size, enhancing rather than diminishing the importance of people.

Hurricane Evacuation Clearance Time: The amount of time specified by the Tampa Bay Regional Planning Council for the safe evacuation of hurricane vulnerable areas.

I

Impact Fee: A fee levied by a local government on new development so that the new development pays its proportionate share of the cost of new or expanded facilities required to service the development.

Impervious: Land surfaces which do not allow (or minimally allow) the penetration of water. An increase in the amount of impervious area will increase the rate and volume of runoff from a given drainage basin.

Impervious Surface: Surface that has been compacted or covered with a layer of material so that it is highly resistant to infiltration by water, including surfaces such as compacted sand, lime rock, shell, or clay, as well as most conventionally surfaced streets, roofs, sidewalks, parking lots and other similar structures.

Income: All income earned by each adult member of the family, including gross wages, social security, Workman's Compensation, child support, and public assistance (exclusive of any amount designated for shelter and utilities).

Industrial Uses: The activities within land areas predominantly connected with manufacturing, assembly, processing, or bulk storage of products.

Infill: Development which occurs on scattered vacant lots in a developed area. Development is not considered infill if it occurs on parcels exceeding one-half acre or more.

Infrastructure: Man-made structures which serve the common needs of the population, such as support services, traffic circulation and park facilities.

Inlet: A structure which allows stormwater to flow into a conveyance system.

Intensity: A measure of land use activity based on density, use, mass, size and impact. May be used synonymously with or measured by FAR.

Intergovernmental Coordination: A process whereby local governments and local, regional and State agencies work together to identify and achieve common goals and to resolve issues relating to development, zoning, comprehensive planning, land use and the like.

Interlocal Agreement: A formal contract between two or more local governments whereby the respective responsibilities of each government are outlined in order to achieve a common goal or purpose.

L

Land Development Regulations: Ordinances enacted by governing bodies for the regulation of any aspect of development including any local government zoning, rezoning, subdivision, building construction, and sign regulating.

Land Use: The development that has occurred on the land, the development that is proposed by a developer on the land, or the use that is permitted or permissible on the land under the adopted Comprehensive Plan or element or portion thereof, land development regulations, or a land development code, as the context may indicate.

Landfill: A disposal facility employing an engineered method of disposing of solid waste on land in a manner which minimizes environmental hazards by spreading the solid wastes in thin layers, compacting to the smallest practical volume, and applying cover material as required by State and Federal regulations.

Landscape: The totality of the built or human-influenced habitat experienced at any one place. Dominant features are topography, plant cover, buildings or other structures and their patterns.

Leadership in Energy and Environmental Design (LEED): LEED certification provides independent verification of a building or neighborhood's green features, allowing for the design, construction, operations and maintenance of resource-efficient, high-performing, healthy, cost-effective buildings.

Level of Service (LOS): A quantitative indicator of quality of service into six letter grades. LOS provides a generalized and conceptual planning measure that assesses multimodal service inside the roadway environment, the extent or degree of service provided or proposed by a facility based on and related to the operational characteristics of the facility.

Lift station: A sanitary sewer facility which pumps wastewater against the force of gravity.

Local Government: Means and refers to the City of Dunedin.

Local Planning Agency (LPA): The Planning and/or Zoning Board agency designated by the City to prepare the Comprehensive Plan required by the Local Government Comprehensive Planning and Land Development Regulation Act in Chapter 163, Florida Statutes.

Local Roadway: A roadway primarily provides direct access and traffic circulation to abutting lands within residential, commercial and industrial areas. These roadways have frequent access points and frequent intersection control such as stop signs. Trip length on local roadway is short, feeding trips to collectors and arterials.

Local Mitigation Strategy (LMS): A locally developed plan to guide the City in loss reduction initiatives before and after a natural disaster. This LMS will permit the City to pinpoint the areas most vulnerable to damage in a natural disaster, while providing the City the ability to access State and Federal funding sources to implement initiatives that reduce the City's potential for future losses. Following a natural disaster, the LMS will provide a ready list of mitigation initiatives and facilitates quick application for rebuilding funds.

Long Range Transportation Plan (LRTP): An adopted document that assesses the conditions of and performance of major transportation facilities, such as roads, airports, seaports, rail lines, bicycle and pedestrian facilities. The Plan also addresses transportation disadvantaged issues, goods movement and regional mobility. As part of the LRTP process, future demands on the County's major road network are evaluated through the Regional Transportation Analysis (RTA) model, which simulates future travel conditions based on projected population and employment growth. The LRTP is updated every three years. The LRTP is updated and administered by the Pinellas County Metropolitan Planning Organization (MPO).

Low-Impact Development (LID): A variety of practices that mimic or preserve natural drainage process to manage stormwater. LID practices typically retain rainwater and encourage it to absorb into the ground rather than allowing it to run off into ditches and storm drains where it would otherwise contribute to flooding and pollution problems.

Low-Income Household: Households whose incomes do not exceed 80 percent of the median income for the area, as determined by HUD with adjustments for smaller and larger families, except that HUD may establish income ceilings higher or lower than 80 percent of the median for the area on the basis of HUD's findings that such variations are necessary because of prevailing levels of construction costs or fair market rents, or usually high or low family incomes. (HUD income limits are updated annually and are available from local HUD offices. This term corresponds to low- and moderate-income households in the CDBG (Community Development Block Grant) Program.)

M

Marina: An establishment with a waterfront location for the dockage of watercraft with more than two wet boat slips, and/or for the refueling of watercraft used primarily for recreation and providing minor repair services for such craft. A marina may include on-shore accessory service uses, including food service establishment, laundry or sanitary facilities, sundries store and other customary accessory facilities such as boat livery.



Marine Habitat: Areas where living marine resources naturally occur, such as mangroves, seagrass beds, algal beds, salt marshes, transitional wetlands, marine wetlands, rocky shore communities, hard bottom communities, oyster bars or flats, mud flats, coral reefs, worm reefs, artificial reefs, offshore springs, near shore mineral deposits and offshore sand deposits. This definition includes, but is not necessarily limited to, those community types classified “estuarine” or “marine” by the Florida Natural Areas Inventory.

Mass Transit: Passenger services provided by public, private or nonprofit entities such as the following surface transit modes: commuter rail, rail rapid transit, light rail transit, light guideway transit, express bus and local fixed routes.

Minerals: All solid minerals, including clay, gravel, phosphate rock, lime, shells, excluding live shellfish, stone, sand, heavy minerals and any rare earths, which are contained in the soils or waters of the State.

Mitigate: To offset or avoid negative impacts through avoiding the impact altogether; minimizing the impact by limiting the degree or magnitude of the action or its implementation; rectifying the impact by repairing, rehabilitating, or restoring the affected environment; reducing the impact over time by preservation or maintenance over the life of the action; or compensating for the impact by replacing or providing substitute resources.

Mixed Use: The development of structures and communities that have a mixture of residential, business and retail uses.

Mobile Home: A structure, transportable in one or more sections, which is eight body feet or more in width and which is built on an integral chassis and designed to be used as a dwelling when connected to the required utilities and includes the plumbing, heating, air-conditioning and electrical systems contained therein.

Mode: The specific method chosen to make a trip, such as walk or rail transit. Typical modes are, walk, bicycle, motorcycle, automobile, van, taxi, bus and a variety of rail transit technologies.

Moderate Income Household: Means one or more natural persons or a family with total annual adjusted gross household income of which is less than 120 percent of the median annual adjusted gross income for households within the State, or 120 percent of the median annual adjusted gross income for households within the metropolitan statistical area (MSA) or, not within an MSA, within the county which the person or family resides, whichever is greater.





Multimodal Corridor: A contiguous, linear area designated with the Multimodal Corridor category on the Countywide Plan Map, which is governed by locally-adopted plan or code provisions that identify the area as a unified corridor, serves as a corridor of critical importance to the movement of people and goods throughout the County, and it is characterized by mixed-use development, supported by and designed to facilitate transit. These roadways include the following subcategories of the Multimodal Corridor categories:

- a) Premium Transit Corridors- Used to recognize corridors identified with existing or planned high-capacity, high-frequency transit service such as bus rapid transit or light rail;
- b) Primary Corridors- Used to recognize corridors identified as appropriate for investment in high-frequency, limited-stop transit, which may provide local or regional connectivity;
- c) Secondary Corridor- Used to recognize corridors identified as appropriate for investment in improved frequency, which may provide local or regional connectivity; and
- d) Supporting Corridor- Used to recognize those corridors that are served by local bus or trolley transit.

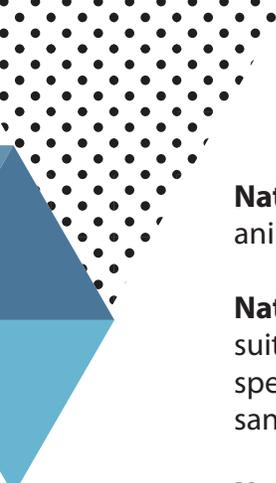
Multimodal Impact Fees: A fee levied on the developer or builder of a project by the City as compensation for otherwise unmitigated impacts the project will produce.

Multimodal Transportation System: May also be referred to as multi-modal, multi-modal system or similar; a comprehensive transportation system including, but not limited to, the following options of mode-choice: fixed-guideway transit, bus, auto, truck, motorcycle, bicycle, and pedestrian allowing the user opportunities to transfer between modes.

N

National Pollutant Discharge Elimination Discharge (NPDES) Permit: National Pollutant Discharge Elimination System Permits are issued by the State under delegation from the Federal government under the auspices of the Clean Water Act. Permits are issued to entities which may be expected to cause water pollution including the wastewater treatment facility, the Municipal Separate Storm Sewer System (MS4), certain Community Development Districts (CDDs) and construction firms. This permit requires the holder to operate their systems to either specific pollutant limitations or, in certain cases, to the maximum extent practicable.

Natural Drainage Features: The naturally occurring features of an area which accommodate the flow of stormwater, such as streams, rivers, lakes and wetlands.



Natural Resource: An existing natural element relating to land, water, air, plant, and animal life of an area or a community and the interrelationship of these elements.

Natural Shorelines: Other than those included in preservation areas, waters deemed suitable for recreation, propagation and protection of fish and wildlife as classified and specified in Chapter 62-3, FAC (Class III Waters), freshwater marshes and wet prairies, sand pine scrub, hardwood swamps, cypress swamps and significant wildlife habitat.

Net Land Area: Exclusive of any submerged land or public road right-of-way existing at the time of the most recent future land use map amendment.

O

Open Space: Undeveloped lands suitable for passive recreation, conservation or stormwater uses.

Outfall: Location where stormwater flows out of a given system. The ultimate outfall of a system is usually a receiving water.

P

Passive Recreational Use: The use of Recreation/Open Space land as a park facility without the formal park designation; “passive” also means that the construction of structures does not occur as they might in formally designated parks.

Peak Hour: In describing traffic conditions, it is the 100th highest volume hour of the year in the predominant traffic flow direction.

Pedestrian: An individual traveling on foot.

Permanent Population (resident population): Inhabitants counted in the same manner utilized by the United States Bureau of the Census, in the category of total population. Permanent population does not include seasonal population.

Person: An individual, corporation, governmental agency, business trust, estate trust, partnership, association, two or more persons having a joint or common interest, or any other legal entity.

Pinellas County Metropolitan Planning Organization (MPO): *See Forward Pinellas.*

Pinellas Planning Council (PPC): *See Forward Pinellas.*

Pinellas Suncoast Transit Authority (PSTA): The mass transit authority for Pinellas County providing residents with bus and paratransit services.



Placemaking: A multi-faceted approach to the planning, design and management of public spaces around the places that citizens view as important to community life and their daily experience based on a community needs and aspirations. Placemaking is both an overarching idea and a hands-on tool for improving a neighborhood, city or region.

Planned Redevelopment District: A contiguous area designated with the Planned Redevelopment District category on the Countywide Plan Map, which is governed by locally-adopted plan or code provisions that identify the area as a unified location, and which provides for a mix of uses, densities/intensities, and urban design that promote walking, biking and transit use.

Playground: A recreation area with play apparatus.

Policy: The way in which programs and activities are conducted to achieve an identified regional goal.

Pollution: The presence in the outdoor atmosphere, ground or water of any substances, contaminants, noise or manmade or man-induced alteration of the chemical, physical, biological, or radiological integrity of air or water, in quantities or at levels which are or may be potentially harmful or injurious to human health or welfare, animal or plant life, or property or unreasonably interfere with the enjoyment of life or property. Pollution includes, but is not necessarily limited to violations of Florida Department of Environmental Protection standards for air quality, water quality, and dredge and fill, as reflected in the Florida Administrative Code (F.A.C.) [AIR] 62-204 through 62-297, F.A.C.; [WATER] 62-503 through 62-699, F.A.C.; and [DREDGE] 62-025 and 62-312 F.A.C

Potable Water: Water which is suitable for drinking. Potable water implies that the water meets the criteria of Rules 17-3.071, 17-3.404, and/or 17-22, Florida Administrative Code.

Potable Water Facilities: A system of structures designed to collect, treat or distribute potable water including water wells, treatment plants, storage tanks, reservoirs and distribution mains.

Prime Aquifer Recharge Areas: Areas with the highest potential to recharge water into aquifers.

Public Access: The ability of the public to physically reach, enter or use recreation sites including beaches and shores.

Public Buildings and Grounds: Structures or lands that are owned, leased or operated by a government entity, such as civic and community centers, hospitals, libraries, police stations, fire stations, and government administration buildings.

Public Education Facilities: Elementary schools, special educational facilities, alternative educational facilities, middle schools, high schools and area vocational-technical schools.

Public Facilities and Services: Facilities and services which must be available concurrent with the impacts of development means those covered by Comprehensive Plan elements for with a level-of-service standards have been adopted by the City. Public facilities include roads, sanitary sewer, solid waste, stormwater, potable water, parks and open space.

Public-Private Partnership: A contractual arrangement between a public agency (Federal, State or local) and a private sector entity. Through this agreement, the skills and assets of each sector (public and private) are shared in delivering a service or facility for the use of the general public. In addition to the sharing of resources, each party shares in the risks and rewards potential in the delivery of the service and/or facility.

Public Transit: *See Mass transit.*

R

Rain Dishes: Dishes that fits under the manhole lid and help prevent rainwater from flowing into the sanitary sewer system.

Recharge or Recharge Area: Shall mean and refer to aquifer recharge area.

Reclaimed Water: Highly treated wastewater and can be used for irrigation and other uses to extend our water supplies.

Recreation: The pursuit of leisure time activities occurring in an indoor or outdoor setting.

Recycling: The process of converting waste materials into new materials and objects.

Redevelopment: The reuse, demolition and reconstruction or substantial renovation of existing buildings or infrastructure within urban infill areas, existing urban service areas or community redevelopment areas.

Rehabilitation (Historic Resources): The act or process of returning a property to a state of utility through repair or alteration which make possible an efficient contemporary use while preserving those portions or features of the property which are significant to its historical, architectural, and cultural values (Secretary of the Interior's Standards).

Renovation: Modernization of an old or historic building that may produce inappropriate alterations or elimination of important features and details. When proposed renovation, activities fall within the definition of "rehabilitation" for historic structures, they are considered to be appropriate treatments.

Residential Uses: Activities within land areas used predominantly for housing.

Resource Recovery: The process by which materials, excluding those under control of the Atomic Energy Commission, which still have useful physical or chemical properties after serving a specific purpose and are reused or recycled for the same or other purposes, including use as an energy source.

Retention: To prevent the discharge of a given volume of stormwater into surface waters through complete on-site storage. See also detention.

Retrofitting: To improve or reconstruct an existing facility with the intent of bringing it into compliance (or, where that is not feasible, more nearly into compliance) with modern standards for such facilities. This term is most frequently used in reference to upgrade of wastewater treatment plants to more advanced treatment standards, or improvement of existing drainage systems to include stormwater pollutant control facilities where such facilities are either nonexistent or insufficient.

Reuse (of building): A use for an existing building or parcel of land other than that for which it was originally intended.

Reuse (of water): The practice of using water which is no longer fit for a particular use for a different application which can tolerate water of lesser quality. For example, capturing the “graywater” from sinks and tubs within homes and using it for lawn irrigation and car washing.

Reverse Osmosis: A process of “desalinization” whereby water is forced, under pressure, through a semi-permeable membrane, removing salt and producing potable water.

Right-of-way: Land in which the state, a county, or a municipality owns the fee simple title or has an easement dedicated or required for a transportation or utility use.

Roadway Level of Service (LOS): Based primarily on travel speed on a scale of “A” through “F.” Roads operating at peak hour LOS “E” and “F” are also referred as “deficient” in the Comprehensive Plan.

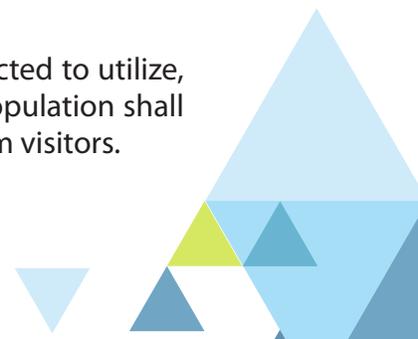
S

Saltwater Intrusion: The process by which saline water contaminates fresh groundwater supplies, making them unusable for use as drinking water. Intrusion may be “lateral,” from the ocean, or “vertical” from the relict seawater which underlies the Floridan aquifer.

Scale: Generally, refers to relative size or extent. Scale is determined by a building or other object relating to its surroundings, by the width of adjacent streets and by buildings as they relate to these streets. Scale may be reflective of Human Scale or Context Sensitive Design.

Scenic / Noncommercial Corridor: A Countywide significant corridor designated to guide the preservation and enhancement of scenic qualities, to ensure the integrity of the Countywide Plan Map, and to maintain and enhance the traffic operation of these significant roadway corridors in Pinellas County.

Seasonal Population: Part time inhabitants who utilize, or may be expected to utilize, public facilities or services, but are not permanent residents. Seasonal population shall include tourists, migrant farmworkers, and other short-term and long-term visitors.



Septic Tank: An on-site sewage treatment facility whereby wastes are piped to underground tanks directly from a building, bacteria in the wastes decompose the organic wastes, and the sludge settles on the bottom of the tank.

Shoreline: Interface of land and water in oceanic and estuarine conditions which follows the general configuration of the mean high-water line (tidal water) and the ordinary high water mark (fresh water).

Single-family Dwelling: A structure containing a single-family unit occupying the building from ground to roof.

Site: Any tract, lot of parcel of land or combination of tracts, lots, or parcels of land which are under one ownership, or are contiguous and in diverse ownership where development is to be performed as part of a unit, subdivision, or project.

Sludge: The accumulated solid residues of the sewage treatment process.

Smart Growth: An urban planning approach to development that encourages a mix of building types and uses, diverse housing and transportation options, development within existing neighborhoods, and community engagement.

Solid Waste: Sludge from a waste treatment works, water supply treatment plant, or air pollution control facility or garbage, rubbish, refuse, or other discarded material, including solid, liquid, semi-solid, or contained gaseous material resulting from domestic, industrial, commercial, mining, agricultural, or governmental operations.

Southwest Florida Water Management District (SWFWMD): A regional agency that manages the water resources, supplies and protects water quality and preserves natural systems that serve important water-related functions for west-central Florida as directed by State law.

Special Flood Hazard Area (SFHA): An area in the floodplain subject to a one percent or greater chance of flooding in any given year. Special flood hazard areas are shown on FIRMs (flood insurance rate maps) as Zone A, AO, A1-A30, AE, A99, AH, V1-V30, VE or V. (Also defined in FBC, B Section 202.)

State Housing Initiatives Partnership Program (SHIP): The State Housing Initiatives Partnership Program is created for the purpose of providing funds to local governments as an incentive for the creation of partnerships to produce and preserve affordable housing.

State Land Planning Agency: The Florida Department of Economic Opportunity (DEO).

Storage: When used in conjunction with hazardous waste, means the containment or holding of a hazardous waste, either on a temporary basis or for a period of years, in such a manner as not to constitute disposal of such hazardous waste.

Stormwater: Flow of water which results from and which occurs immediately after a rainfall event.

Stormwater Management System: A system which is designed and constructed or implemented to control stormwater, incorporating methods to collect, convey, store, absorb, inhibit, treat, use, or reuse stormwater to prevent or reduce flooding, over-drainage, environmental degradation and water pollution or otherwise affect the quantity and quality of discharges from the system.

Stormwater Retention: To store stormwater to provide treatment before discharge into receiving waters or to provide storage facility for stormwater where no outfall is available.

Stormwater Runoff: That portion of precipitation that flows off the land surface during, and for short durations following, a rainfall event.

Streetscape: The term streetscape refers to that general aggregation of all street-side elements of the urban environment perceived by the pedestrian or motorist. This street-side environment includes such things as street, alleys, parks, sidewalks and parking lots. Streetscape elements include lighting, paving, traffic safety and control, signage, shelters, recreation and play equipment, street furniture and other miscellaneous items.

Strip Commercial Development: Commercial or retail development, usually one-store deep, that fronts on a major street.

Structure: Anything constructed, installed or portable, the use of which requires a location on a parcel of land. It includes a movable structure while it is located on land. Structures may be used for housing, business, commercial, agricultural or office purposes, either temporarily or permanently. "Structure" also includes sheds, garages, fences, billboards, swimming pools and advertising signs.

Subdivisions: The process of laying out parcel of land into lots, parcels, tracts or other divisions of land as defined in applicable State Statutes and local land development regulations.

Submerged Land: The area situated below the mean high water line or the ordinary high water line of a standing body of water, including ocean, estuary, lake, pond, river, stream, or existing natural and man-made drainage detention areas. For the purpose of this definition, submerged lands created as a function of development that are recorded on an approved final site plan or other authorized development order action, and wetlands landward of the mean and/or ordinary high water line, shall not be considered submerged land, and thus may be included in the computation of net land area for the purpose of determining permitted density/intensity.

Suburban: Generally, refers to development on the periphery of urban areas, predominantly residential in nature with many urban services available. Intensity of development is usually lower than in urban areas.

Support Documents: Any surveys, studies, inventory maps, data, inventories, listings or analyses used as bases for or in developing the local Comprehensive Plan.

Support Service: Sanitary sewer, potable water, drainage, solid waste and natural groundwater aquifer recharge systems which service development.

Surficial Aquifer: An unconfined, non-artesian body of water stored in the soil. The top of the surficial aquifer may form the groundwater table. *Also see prime aquifer recharge area.*

T

Tampa Bay Area Regional Transit Agency (TBARTA): A State agency created for the purpose of improving mobility and expanding multi-modal transportation options for passengers and freight throughout the five county region made up of Hernando, Hillsborough, Manatee, Pasco, and Pinellas counties.

Tampa Bay Regional Planning Council (TBRPC): An association of local governments and gubernatorial representatives that brings together governments to coordinate planning for the community's future and provide an opportunity for sharing solutions among the local government jurisdictions in the six-county Tampa Bay region.

Target Employment: High-wage, primary employment, including but not limited to, the fields of aviation/aerospace, financial services, high tech industries, information technology, marine science, medical technology, microelectronics, modeling/simulation, optics/photonics, homeland security and defense, research/development and wireless technology.

Tiny Home: A house with a square footage that is between 100 and 400 square feet which is large enough to provide a sleeping area, bathroom, kitchen, storage, and living area.

Traffic Calming: A concept fundamentally concerned with reducing the adverse impact of motor vehicles on built up areas. Usually involves reducing vehicle speeds, providing more space for pedestrians and cyclists, and improving the local environment.

Transit-Oriented Development (TOD): Moderate and high-density housing concentrated in mixed-use developments located along transit routes. The location, design and mix of uses in a TOD emphasize pedestrian oriented environments and encourage the use of public transportation.

Transportation Demand Management (TDM) Strategies: Policies, activities, or programs which reduce the total number of vehicles traveling to an area. Transportation demand management measures include improved alternatives to driving alone, such as pooling and biking; incentives to shift modes, such as subsidizing transit fares and van pooling costs; disincentives to driving, such as higher parking fees and reduced parking supply; and work hours management programs, such as flexible work hours and compressed work weeks.

Transportation Disadvantaged: Persons who, because of physical or mental disability, income status, or age are unable to transport themselves or purchase transportation.

Transportation Improvement Program (TIP): A consolidated State, County and municipal five-year capital improvement program for transportation projects. The purpose of the TIP is to coordinate and prioritize transportation projects with the County, in accordance with Federal and State law. The TIP is updated annually by the MPO.

Transportation Management Initiatives (TMI): Organization formed to encourage and coordinate the participation of local businesses in transportation demand management activities. These agencies are also known as transportation management organizations (TMOs).

Transportation System: The sum of all forms or modes of transportation which, taken together, provide for the movement of people and goods. The system includes all forms of air, water and ground transportation.

Treatment: When used in connection with hazardous waste, means any method, technique or process, including neutralization, designed to change the physical, chemical, or biological character or composition of any hazardous waste so as to neutralize it or render it non-hazardous, safe for transport, amenable to recovery, amenable to storage or disposal, or reduced in volume or concentration. The term includes any activity or processing designed to change the physical form or chemical composition of hazardous waste to render it non-hazardous.

U

Universal Design: The process of creating buildings, products and environments that are usable by people of all ages and physical capabilities, making it possible for all to have access to and fully enjoy their homes, neighborhoods, work places and other community destination. Universal design is based on the recognition that all people, regardless of ability, should be valued equally. Universal Design is highly relevant to planning officials, planning directors and planners seeking to improve the quality of life in their communities by providing residents with better choices of where to work, live and play.

Urban: Generally, refers to an area having the characteristics of a city, with intense development and a full or extensive range of public facilities and services.

Urban Design: A process by which the built environment may shape and regulate the physical form of our cities and towns in response to our human needs.

Urban Form: The integration of all the physical elements of a city into a three-dimensional whole.

Urban Sprawl: Uncontrolled and untimely expansion and spreading out of an urban community into the outlying non-urban areas.

Urban Trail Corridor: A multi-use public path that creates an active transportation corridor through a built environment for uses such as: bicycling, walking, running, in-line skating, stroller and wheelchair use in which people travel.

V

Vegetative Communities: Ecological communities, such as coastal strands, oak hammocks, and cypress swamps, which are classified based on the presence of certain soils, vegetation and animals.

W

Wastewater: The spent water of the community comprising the liquid and water-carried wastes from residences, commercial buildings, industries, and institutions, together with minor quantities of ground and surface waters that are not admitted intentionally.

Wastewater Treatment Plant: A facility which functions to remove solid and organic materials from sewage.

Water-Dependent Uses: Activities which can be carried out only on, in, or adjacent to water areas because the use requires access to the water body for waterborne transportation including ports or marinas, recreation, electrical generating facilities, or water supply.

Water Recharge: The process by which water falls on the land surface percolates into the soil and is stored in an underground aquifer. Recharge may be to a surficial aquifer or (under special conditions) to the artesian Floridian aquifer.

Water Reclamation: Water recovered after its application for one use and then diverting it to another use which requires water of lesser quality. For example, wastewater can be treated to advanced standards and then reused for irrigation.

Water Wells: Wells excavated, drilled, dug or driven for the supply of industrial, agricultural or potable water for general public consumption.

Watershed: A region or area bounded peripherally by a water parting, dividing or ridge line and draining ultimately to a watercourse or body of water.

Wellfield: An area containing wells that are excavated, drilled, dug, or driven for the supply of industrial, agricultural or potable water for general public consumption.

Wellhead Protection Area: An area designated by local government to provide land use protection for the groundwater source for potable water wellfield, including the surface and subsurface area surrounding the wellfield. Differing levels of protection may be established within the wellhead protection area commensurate with the capacity of the well and an evaluation of the risk to human health and the environment. Wellhead protection areas shall be delineated using professionally accepted methodologies based on the best available data and considering any zones of contribution described in existing data.

Wetlands: Land that is inundated or saturated by surface or groundwater at a frequency and duration sufficient to support, and that under normal circumstances does support, a prevalence of vegetation typically adapted for life in saturated soil conditions.

Wildlife: Any member of the plant and animal kingdoms, with the exception of man, including but not limited to any mammal, fish, bird, amphibian, reptile, mollusk, crustacean, arthropod or other invertebrate and excluding domestic animals.

Workforce Housing: Housing that is affordable to household of low, moderate and above-moderate income in the range of 60 percent to 120 percent of the area median income.

X

Xeriscape: A method of landscaping using native vegetation and other drought-resistant plants designed for low maintenance and water conservation.

Z

Zoning: The dividing of a municipality into districts and the establishment of regulations governing land use, population density, and building size.





Acronyms

AARP	American Association of Retired Persons
ACLF	Adult Congregate Living Facility
ADA	Americans with Disabilities Act of 1990
ADU	Accessory Dwelling Unit
AHCA	Agency for Health Care Administration
AV	Autonomous Vehicle
A2O	Anaerobic/Anoxic/Oxic
BMP	Best Management Practices
BRT	Bus Rapid Transit
CCCL	Coastal Construction Control Line
C&D	Construction & Debris
CDBG	Community Development Block Grant
CHHA	Coastal High Hazard Area
CIP	Capital Improvements Program
CLT	Community Land Trust
CMP	Congestion Management Process
CRA	Community Redevelopment Agency
CRD	Community Redevelopment District
CRS	Community Rating System
DCA	Department of Community Affairs
DEO	Department of Economic Opportunity
DRC	Development Review Committee
DU	Dwelling Unit
DU/AC	Dwelling Unit per Acre
EAR	Evaluation and Appraisal Review
EPA/USEPA	United States Environmental Protection Agency
FAR	Floor Area Ratio
FDEP	Florida Department of Environmental Protection
FDOT	Florida Department of Transportation
FEMA	Federal Emergency Management Agency
FGBC	Florida Green Building Coalition
FRPA	Florida Recreation and Park Association
F.S.	Florida Statutes
FSA	Florida Stormwater Association
FSDN	Florida Sustainability Director's Network
FTE	Full-time Equivalent
FWC	Florida Fish and Wildlife Conservation Commission
GIS	Geographic Information System
GPCPD	Gallons per Capita per Day
HOME	Home Investment Partnerships Program
HQS	Housing Quality Standards
HUD	Department of Housing Urban Development

ICLEI	International Council for Local Environmental Initiatives
I/I	Infiltration/Inflow
ISBA	Interlocal Service Boundary Agreement
ISR	Impervious Surface Ratio
LDC	Land Development Code
LEED	Leadership in Energy and Environmental Design
LID	Low-Impact Development
LMS	Local Mitigation Strategy
LOS	Level of Service
LPA	Local Planning Agency
L RTP	Long Range Transportation Plan
MGD	Million Gallons per Day
MPO	Metropolitan Planning Organization
MRF	Materials Recovery Facility
MSW	Municipal Solid Waste
MS4	Municipal Separate Storm Sewer System
NFIP	National Flood Insurance Program
NPDES	National Pollutant Discharge Elimination System
NRPA	National Recreation and Park Association
P	Preservation
PPC	Pinellas Planning Council
PCSB	Pinellas County School Board
PET	Polyethylene Terephthalate
P/SP	Public/Semi-Public
PSTA	Pinellas Suncoast Transit Authority
PV	Photovoltaic
RFP	Request for Proposal
RFT	Recycle Florida Today
RH	Residential High
RL	Residential Low
RLM	Residential Low Medium
RM	Residential Medium
RO	Reverse Osmosis
R/OS	Recreation/Open Space
RU	Residential Urban
SAP	Special Area Plan
SFHA	Special Flood Hazard Area
SHIP	State Housing Initiatives Partnership
SIP	Florida State Implementation Plan
SR	State Road
SSDN	Southeast Sustainability Director's Network
SWANA	Solid Waste Association of North America
SWFWMD	Southwest Florida Water Management District
TBARTA	Tampa Bay Area Regional Transit Agency
TBRPC	Tampa Bay Regional Planning Council

TCC	Technical Coordinating Committee
TDM	Transportation Demand Management
TIP	Transportation Improvement Program
TMDL	Total Maximum Daily Load
TMI	Transportation Management Initiatives
UBC	Used Beverage Containers
USF	University South Florida
USFWS	United States Fish and Wildlife Services
USGBC	United States Green Building Council
VTC	Volume-to-Capacity
WTE	Waste-to-Energy



APPENDIX

REFERENCED
DOCUMENTS,
PROGRAMS,
AGENCIES &
ORGANIZATIONS



Referenced Documents

Documents referenced in all Elements-

City of Dunedin Municipal Business Plan and Capital Improvements Plan
City of Dunedin Code of Ordinances
City of Dunedin Land Development Code

Future Land Use

Forward Pinellas Countywide Plan (Countywide Rules, Countywide Plan Map and Countywide Plan Strategies)
Forward Pinellas 2045 Long Range Transportation Plan (LRTP)
Forward Pinellas Bicycle Pedestrian Master Plan Facilities Element
Forward Pinellas Countywide Trends & Conditions Report
Pinellas by Design
City of Dunedin 2017 Vision Update
City of Dunedin Corridor Studies:
 Douglas Ave
 Patricia Ave
 Causeway Blvd
 SR 580
City of Dunedin Community Redevelopment Agency Downtown Master Plan 2033
City of Dunedin Bicycle and Pedestrian Master Plan
City of Dunedin Economic Development Master Plan

Transportation

FDOT 2060 Florida Transportation Plan
FDOT Five Year Work Program
Florida State Implementation Plan (SIP)
PSTA Transit Development Plan
Forward Pinellas Countywide Plan (Countywide Rules, Land Use Plan Map and Strategies)
Forward Pinellas 2045 Long Range Transportation Plan (LRTP)
Forward Pinellas Transportation Improvement Program (TIP)
Forward Pinellas Congestion Management Process (CMP) Policies and Procedures Manual
Forward Pinellas Bicycle Pedestrian Master Plan Facilities Element
Forward Pinellas Countywide Trends & Conditions Report
City of Dunedin 2017 Vision Update
City of Dunedin Corridor Studies:
 Douglas Ave
 Patricia Ave
 Causeway Blvd
 SR 580
City of Dunedin Community Redevelopment Agency Downtown Master Plan 2033
City of Dunedin Bicycle and Pedestrian Master Plan
City of Dunedin Citywide Multimodal Transportation Master Plan

Housing

City of Dunedin Affordable Housing Needs Assessment 2019
University of South Florida Community Sustainability Partnership Program- Affordable Housing Plan for the City of Dunedin

Conservation

Florida State Implementation Plan (SIP)
SWFWMD Regional Water Supply Plan
Pinellas County Utilities Potable Water Supply Program
SWFWMD Water Use Permit (City of Dunedin Water Supply Facilities Work Plan)
City of Dunedin Stormwater Master Plan
University of South Florida Community Sustainability Partnership Program- Carbon Footprint Study
Dunedin's Resilient Environmental Action Master Plan (DREAM)

Coastal Management

Pinellas County Local Mitigation Strategy Plan
City of Dunedin Stormwater Master Plan
University of South Florida Community Sustainability Partnership Program- Peril of Flood Act Comprehensive Plan Amendment and Sea Level Rise Study

Recreation & Open Space

City of Dunedin Parks & Recreation Strategic Plan

Support Services

SWFMWD Regional Water Supply Plan
Pinellas County Utilities' Potable Water Supply Facilities Work Plan
Pinellas County Solid Waste 30 Year Master Plan
SWFWMD Water Use Permit (City of Dunedin Water Supply Facilities Work Plan)
City of Dunedin Stormwater Master Plan

Capital Improvement

FDOT Five Year Work Program
Forward Pinellas 2045 Long Range Transportation Plan (LRTP)
Forward Pinellas Transportation Improvement Program (TIP)
Pinellas County School Board Five Year Work Program
City of Dunedin Community Redevelopment Agency Downtown Master Plan 2033
City of Dunedin Corridor Studies:
Douglas Ave
Patricia Ave
Causeway Blvd
SR 580

Referenced Programs

Future Land Use

AARP Livable Communities
FDOT Complete Streets Implementation

Transportation

AARP Livable Communities
FDOT Complete Streets Implementation
Forward Pinellas Complete Streets Program
Pinellas County Transportation Disadvantaged Program
Pinellas County Transportation Management Initiatives

Housing

Pinellas County Down Payment Assistance Program
Pinellas County Home Improvement No Interest Loan Program
Pinellas County Independent Living Program
Pinellas County Housing Finance Authority First-time Homebuyer Program
Pinellas County Services for Seniors
Pinellas County Housing Authority Public Housing Program
Pinellas County Housing Authority Housing Choice Voucher Program
Pinellas County Housing Finance Authority Target Area Homebuyer Programs
Pinellas County Housing Finance Authority Development of Mixed-Income Rental Housing
Pinellas County State Housing Initiative Partnership (SHIP)
Pinellas County Community Development Block Grant (CDBG) Program
Pinellas County HOME Investment Partnership Program (HOME)
Pinellas County Community Housing Trust Fund
Pinellas County Neighborhood Stabilization Program
Pinellas County Homelessness Prevention and Rapid Re-Housing Program
Pinellas County Emergency Solutions Grant Program

Conservation

Leadership in Energy and Environmental Design Certification Program (LEED)
Green Globes Certification Program
Ready for 100: Commitment to 100 Percent Renewable Energy
Mission Blue Hope Spot Initiative
Duke Energy Park & Plug Electric Vehicle Charging Stations
Florida Green Building Coalition Certification Designation
Earth Day Celebrations
Tree City USA
Recycling Education
Florida Water Star Program

Coastal Management

National Flood Insurance Program (NFIP)
Community Rating System (CRS)

Recreation & Open Space

National Parks and Recreation Month
LiveWell Dunedin
Arbor Day Celebration and Tree Give-Away
Tree City USA
Dr. Martin Luther King, Jr. Unity Celebration
Senior Awareness Day
Intergenerational Week
Every Child A Swimmer
Dunedin for Youth Scholarship Fund
Purple Heart Day Ceremony
Before & After School Programs
Summer Camps

Support Services

Florida Water Star Program
Clean Water Act
National Pollutant Discharge Elimination System (NPDES)
National Flood Insurance Program (NFIP)
Community Rating System (CRS)
Earth Day Celebrations
Recycling Education



Referenced Agencies & Organizations

Future Land Use

American Association of Retired Persons (AARP)
Smart Growth America
Urban Land Institute
National Register of Historic Places
Florida Department of State
Florida Department of Transportation (FDOT)
Southwest Florida Water Management District (SWFWMD)
Tampa Bay Regional Planning Council (TBRPC)
Forward Pinellas (PPC/MPO)
Pinellas County

Transportation

Smart Growth America
Florida Department of Environmental Protection (FDEP)
Florida Department of Transportation (FDOT)
Tampa Bay Area Regional Transportation Authority (TBARTA)
Tampa Bay Regional Planning Council (TBRPC)
Forward Pinellas (PPC/MPO)
Pinellas County
Pinellas County School Board (PCSB)
Pinellas Suncoast Transit Authority (PSTA)

Housing

American Association of Retired Persons (AARP)
Florida Housing Coalition
University of South Florida (USF)
Pinellas County
Pinellas County Community Development Department
Pinellas County Homeless Leadership Board
Pinellas County Housing Authority
Pinellas County Housing Finance Authority
Pinellas County Human Services
Habitat for Humanity of Pinellas County
Bright Community Trust
Dunedin Housing Authority

Conservation

U.S. Environmental Protection Agency (USEPA)
U.S. Fish and Wildlife Services (USFWS)
U.S. Green Building Council (USGBC)
Green Building Initiative
Florida Department of Environmental Protection (FDEP)
Florida Fish and Wildlife Conservation Commission (FWC)
Florida Green Building Coalition (FGBC)
Southwest Florida Water Management District (SWFWMD)
Tampa Bay Regional Planning Council (TBRPC)
Tampa Bay Estuary Program
Duke Energy
Pinellas County Extension
Pinellas County
Keep Pinellas Beautiful
Southeast Sustainability Director's Network (SSDN)
Florida Sustainability Director's Network (FSDN)
Recycle Florida Today (RFT)
International Council for Local Environmental Initiatives (ICLEI)
University of Florida Patel College of Sustainability (USF)
Ocean Allies
Mission Blue

Coastal Management

Federal Emergency Management Agency (FEMA)
U.S. Environmental Protection Agency (USEPA)
U.S. Fish and Wildlife Services (USFWS)
U.S. Army Corps of Engineers
Florida Department of Environmental Protection (FDEP)
Florida Department of Transportation (FDOT)
Florida Division of Emergency Management
Florida Fish and Wildlife Conservation Commission (FWC)
Southwest Florida Water Management District (SWFWMD)
Tampa Bay Regional Planning Council (TBRPC)
Tampa Bay Area Regional Transportation Authority (TBARTA)
Pinellas County
Pinellas County Local Mitigation Strategy Working Group (LMS)
Pinellas Suncoast Transit Authority (PSTA)
Pinellas County Extension
Tampa Bay Watch



Recreation & Open Space

Florida Recreation and Park Association (FRPA)
National Recreation and Park Association (NRPA)
Friends of the Hammock, Inc.
Kiwanis Club of Dunedin
Rotary Clubs of Dunedin
Dunedin Youth Guild
VFW Post 2550
Special Olympics
Dunedin Scottish Arts Foundation
Dunedin Youth Soccer
Dunedin Little League
Dunedin Golf Club
Dunedin Fine Art Center
Dunedin Youth Sailing
Dunedin Boat Club
Dunedin Windlasses
Toronto Blue Jays
Achieva Credit Union
Dunedin Chamber of Commerce
Downtown Dunedin Merchants Association
Pinellas County
Pinellas County School Board
Florida Department of Environmental Protection (FDEP)
Florida State Parks
Florida Fish and Wildlife Conservation Commission (FWC)

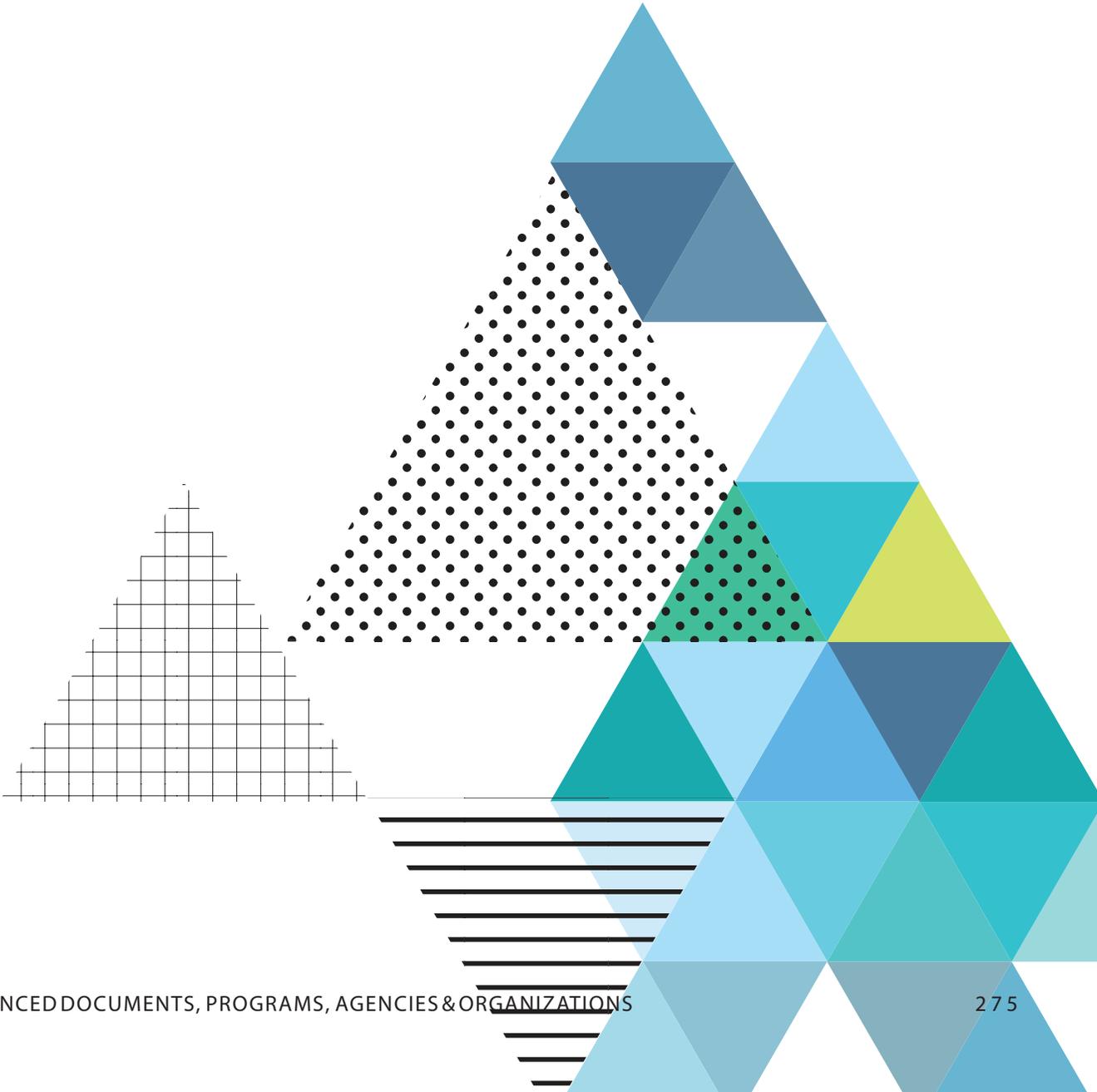
Support Services

U.S. Environmental Protection Agency (USEPA)
U.S. Army Corps of Engineers
U.S. Environmental Protection Agency Waste Wise
Solid Waste Association of North America (SWANA)
Florida Department of Environmental Protection (FDEP)
Florida Department of Transportation (FDOT)
Florida Stormwater Association (FSA)
Recycle Florida Today (RFT)
Tampa Bay Regional Planning Council (TBRPC)
Southwest Florida Water Management District (SWFWMD)
Tampa Bay Water
Pinellas County
Keep Pinellas Beautiful



Capital Improvement

Florida Department of Transportation (FDOT)
Southwest Florida Water Management District (SWFWMD)
Tampa Bay Regional Planning Council (TBRPC)
Forward Pinellas (PPC/MPO)
Pinellas County School Board (PCSB)
Pinellas County



APPENDIX

**FY 2020 CAPITAL
IMPROVEMENTS
PLAN BY FUND**

FY 2020 - 2025 Capital Improvements Plan

Project	Project Status	Lead Department	Fund	FY19 Carry-Forward	FY20
Citywide Exterior Facility Painting	Revised	Public Services	General	-	25,000
Citywide HVAC Replacements	Revised	Public Services	General	-	136,000
Citywide Roof Replacements	Revised	Public Services	General	-	302,000
Citywide Security Camera System Replacements	Revised	IT Services	General	-	45,000
Court Resurfacing	Revised	Parks & Rec	General	-	-
Electrical Distribution (Power Grid) Assessment	New	City Manager	General	-	-
Fleet Purchase: Parks Pick-up Truck	New	Parks & Rec	General	-	30,000
Fleet Purchase: Planning & Development Golf Cart	New	Plng & Dev	General	-	9,000
Lightning Detection System Replacement	Existing	Parks & Rec	General	-	-
Marina Master Plan	New	Parks & Rec	General	-	-
Microfilm to Digital Format Conversion	New	City Clerk	General	-	20,300
Park Pavilion Replacement	Revised	Parks & Rec	General	-	-
Patricia Corridor Enhancement Project	Revised	Eco & Hsg Dev	General	-	50,000
Rotary Pavilion Renovations	Revised	Parks & Rec	General	65,000	50,000
S.R. 580 Mast Arm Repainting	Revised	Public Services	General	-	-
SCBA Air Pack Replacements	Existing	Fire	General	-	-
Sindoon Stage Awning Replacement	Revised	Parks & Rec	General	-	50,000
Weybridge Woods Bridge Removal	Revised	Public Works	General	-	-
GENERAL FUND TOTAL				65,000	717,300
Stadium & Englebert Reconstruction	Revised	Parks & Rec	Stadium	39,431,900	41,899,900
STADIUM FUND TOTAL				39,431,900	41,899,900
Fire Training Facility & Emergency Operations Center (EOC)	Existing	Fire	Impact	-	200,000
Pedestrian Safety Improvements- Alt 19 & Main	Revised	Public Works	Impact	-	70,000
IMPACT FEE FUND TOTAL				-	270,000
Pavement Management Program	Revised	Public Services	CGT	-	310,000
COUNTY GAS TAX FUND TOTAL				-	310,000
Athletic Field Renovation	Revised	Parks & Rec	Penny	-	125,000
Bridges & Boardwalks	Revised	Parks & Rec	Penny	81,000	60,000
Citywide Parking Lot Resurfacing - Library	Revised	Public Services	Penny	-	72,000
Community Center Parking Lot	Revised	Parks & Rec	Penny	-	30,000
Court Resurfacing	Revised	Parks & Rec	Penny	-	25,000
Dog Park	Existing	Parks & Rec	Penny	-	150,000
Downtown Parking Structure	Revised	Eco & Hsg Dev	Penny	-	1,800,000
Fire Training Facility & Emergency Operations Center (EOC)	Existing	Fire	Penny	1,839,000	1,796,000
Jerry Lake Parking Lot Renovation	New	Parks & Rec	Penny	-	-
New Aquatics Center	Existing	Parks & Rec	Penny	-	-
New City Hall	Existing	City Manager	Penny	1,050,000	7,200,000
Parks Maintenance Facility	Revised	Parks & Rec	Penny	75,400	1,124,600
Parks Trail Renovation	Revised	Parks & Rec	Penny	-	300,000
Pavement Management Program	Revised	Public Services	Penny	-	690,000
Playground Equipment Replacement	Revised	Parks & Rec	Penny	-	90,000
Skinner Boulevard Improvements	Revised	Eco & Hsg Dev	Penny	-	-
St. Catherine Soil Roadway Stabilization	Revised	Public Works	Penny	-	50,000
PENNY FUND TOTAL				3,045,400	13,512,600
Artistic Benches for Downtown	New	Eco & Hsg Dev	CRA	-	4,000
Downtown East End Plan (DEEP)	Existing	Eco & Hsg Dev	CRA	-	30,000
Downtown Landscaping Project	New	Eco & Hsg Dev	CRA	-	10,000
Downtown Median Removal	New	Eco & Hsg Dev	CRA	-	10,000
Downtown Parking Structure	Revised	Eco & Hsg Dev	CRA	-	1,000,000
Downtown Pavers, Walkability & Enhancements	Revised	Eco & Hsg Dev	CRA	-	250,000
Downtown Undergrounding	New	Eco & Hsg Dev	CRA	-	-
East End Public Restrooms	New	Eco & Hsg Dev	CRA	-	-
Enhance Welcome Signs Downtown	New	Eco & Hsg Dev	CRA	-	10,000
Existing City Hall Adaptive Re-use	New	Eco & Hsg Dev	CRA	-	-
Highland/Louden/Virginia Streetscape	New	Eco & Hsg Dev	CRA	-	-
John L Lawrence Pioneer Park Enhancements & Impr.	Revised	Eco & Hsg Dev	CRA	95,000	50,000
Skinner Boulevard Improvements	Revised	Eco & Hsg Dev	CRA	-	100,000
Skinner/New York Entry Way Median	New	Eco & Hsg Dev	CRA	-	-
Underground Utilities on Douglas Ave S	New	Eco & Hsg Dev	CRA	-	100,000
COMMUNITY REDEVELOPMENT AGENCY (CRA) FUND TOTAL				95,000	1,564,000

FY 2020 - 2025 Capital Improvements Plan					
Project	Project Status	Lead Department	Fund	FY19 Carry-Forward	FY20
Citywide Exterior Facility Painting - Solid Waste	Revised	Public Services	Solid Waste	-	7,000
Citywide HVAC Replacements - Solid Waste	Revised	Public Services	Solid Waste	-	-
Citywide Roof Replacements - Solid Waste	Revised	Public Services	Solid Waste	-	-
Fleet Replacements	Revised	Solid Waste	Solid Waste	-	489,000
SOLID WASTE FUND TOTAL				-	496,000
Armour Drive & Mangrum Drive Water Main Replacement	New	Public Works	Water/WW	-	500,000
Citywide HVAC Replacements - Water Admin	Revised	Public Services	Water/WW	-	26,000
Curlew Reclaimed Tank Repainting	Revised	Public Works	Water/WW	-	350,000
Curlew Road Water Main Replacement	Revised	Public Works	Water/WW	100,000	200,000
Friendly Lane Water & Sewer Upgrade	Revised	Public Works	Water/WW	-	150,000
Lift Station #20 Repair/Replacement	Revised	Public Works	Water/WW	-	400,000
Lift Station #32 Repair/Replacement	Revised	Public Works	Water/WW	-	150,000
Lift Station Evaluation	New	Public Works	Water/WW	-	100,000
Lofty Pine Estates Septic to Sewer Project	New	Public Works	Water/WW	-	850,000
Ranchwood Dr S & Hitching Post Lane Water Main Repl.	Revised	Public Works	Water/WW	-	-
Reclaimed Water Distribution System Master Plan	New	Public Works	Water/WW	-	100,000
Water Production Well Facilities	Revised	Public Works	Water/WW	-	-
WTP Design-Build	Existing	Public Works	Water/WW	13,986,000	5,220,000
WW Lift Station Force Main Replacement	Existing	Public Works	Water/WW	-	600,000
WWTP Electrical System Upgrade	Revised	Public Works	Water/WW	-	7,150,000
WWTP Facility 8, Filter Building Noise Attenuation Project	Existing	Public Works	Water/WW	-	50,000
WWTP Outfall Piping Repair	Revised	Public Works	Water/WW	-	100,000
WATER/WASTEWATER FUND TOTAL				14,086,000	15,946,000
Downtown Wayfinding	Revised	Eco & Hsg Dev	Parking	-	5,000
PARKING FUND TOTAL				-	5,000
Armour Drive & Mangrum Drive Water Main Replacement	New	Public Works	Stormwater	-	250,000
Brady Box Culvert	Existing	Public Works	Stormwater	80,000	240,000
Cedarwood & Lyndhurst CMP Design Replacement	Revised	Public Works	Stormwater	25,000	375,000
Gabion Repair & Replacement Program	Existing	Public Works	Stormwater	-	700,000
Patricia Beltrees Treatment Facility	Existing	Public Works	Stormwater	75,000	75,000
Stormwater Pipe Lining	Revised	Public Works	Stormwater	-	425,000
Underdrain Repair & Replacement	Existing	Public Works	Stormwater	-	45,000
STORMWATER FUND TOTAL				180,000	2,110,000
Citywide Parking Lot Resurfacing - Marina	Revised	Public Services	Marina	-	-
Dock A Repair & Replacement	New	Parks & Rec	Marina	-	350,000
Harbormaster Building Replacement	Existing	Parks & Rec	Marina	-	-
Marina Dredging	Existing	Parks & Rec	Marina	-	1,500,000
MARINA FUND TOTAL				-	1,850,000
Citywide Exterior Facility Painting - Fleet Bldg.	Revised	Public Services	Fleet	-	20,000
Citywide HVAC Replacements - Fleet Bldg.	Revised	Public Services	Fleet	-	10,000
Citywide Security Camera System Repl. - Fleet Building	Revised	IT Services	Fleet	-	35,000
Fleet Replacements	Revised	Public Works	Fleet	-	1,135,500
FLEET FUND TOTAL				-	1,200,500
Citywide HVAC Replacements - Public Services Bldg.	Revised	Public Services	Facilities	-	-
FACILITIES MAINTENANCE FUND TOTAL				-	-
Citywide Computer Replacements	Revised	IT Services	IT Fund	-	123,800
Data Backup and Disaster Recovery System	New	IT Services	IT Fund	-	120,000
Enterprise Resource Program (ERP) Equipment	Revised	IT Services	IT Fund	-	18,000
ERP Phases 5 & 6 Installation	New	IT Services	IT Fund	-	185,000
Fiber Cable Audit & Survey	Revised	IT Services	IT Fund	-	-
Fleet Purchase: IT Nissan Leaf	New	IT Services	IT Fund	-	30,000
Network Infrastructure Upgrades	Revised	IT Services	IT Fund	-	85,000
IT SERVICES FUND TOTAL				-	561,800
TOTAL CAPITAL IMPROVEMENTS PROJECTS EXPENDITURES				\$ 56,903,300	\$ 80,443,100

FY 2020 - 2025 Capital Improvements Plan

FY21	FY22	FY23	FY24	FY25	Six Year Planning Period	GOAL / TYPE
-	15,000	120,000	-	-	160,000	CIP
124,000	20,000	35,000	25,000	25,000	365,000	CIP
500,000	210,000	-	-	-	1,012,000	CIP
-	-	-	-	-	45,000	CIP
25,000	25,000	30,000	-	-	80,000	CIP
100,000	-	-	-	-	100,000	CIP
-	-	-	-	-	30,000	CIP
-	-	-	-	-	9,000	CIP
-	-	35,000	-	-	35,000	CIP
100,000	-	-	-	-	100,000	CIP
-	-	-	-	-	20,300	CIP
-	-	65,000	65,000	65,000	195,000	CIP
50,000	50,000	50,000	50,000	-	250,000	CIP
-	-	-	-	-	115,000	CIP
-	-	110,000	-	-	110,000	CIP
209,500	-	-	-	-	209,500	CIP
-	-	-	-	-	50,000	CIP
-	-	-	10,000	-	10,000	CIP
1,108,500	320,000	445,000	150,000	90,000	2,895,800	
-	500,000	-	-	-	81,831,800	CIP
-	500,000	-	-	-	81,831,800	
-	-	-	-	-	200,000	CIP
-	-	-	-	-	70,000	CIP
-	-	-	-	-	270,000	
310,000	310,000	290,000	270,000	250,000	1,740,000	CIP
310,000	310,000	290,000	270,000	250,000	1,740,000	
100,000	100,000	100,000	-	-	425,000	CIP
-	-	-	-	-	141,000	CIP
-	-	66,000	-	-	138,000	CIP
150,000	-	-	-	-	180,000	CIP
-	-	-	-	-	25,000	CIP
-	-	-	-	-	150,000	CIP
2,700,000	-	-	-	-	4,500,000	CIP
-	-	-	-	-	3,635,000	CIP
125,000	-	-	-	-	125,000	CIP
600,000	6,400,000	-	-	-	7,000,000	CIP
9,750,000	-	-	-	-	18,000,000	CIP
-	-	-	-	-	1,200,000	CIP
-	-	-	-	-	300,000	CIP
690,000	690,000	690,000	710,000	730,000	4,200,000	CIP
75,000	80,000	300,000	90,000	-	635,000	CIP
-	-	-	1,000,000	-	1,000,000	CIP
200,000	-	-	-	-	250,000	CIP
14,390,000	7,270,000	1,156,000	1,800,000	730,000	41,904,000	
-	-	-	-	-	4,000	CIP
-	-	-	-	-	30,000	CIP
-	-	-	-	-	10,000	CIP
-	-	-	-	-	10,000	CIP
1,500,000	-	-	-	-	2,500,000	CIP
-	100,000	350,000	400,000	300,000	1,400,000	CIP
-	-	-	-	400,000	400,000	CIP
-	-	100,000	-	-	100,000	CIP
-	-	-	-	-	10,000	CIP
-	200,000	200,000	-	-	400,000	CIP
-	-	-	400,000	-	400,000	CIP
315,000	-	-	-	-	460,000	CIP
-	-	-	3,700,000	-	3,800,000	CIP
-	-	-	-	150,000	150,000	CIP
-	-	-	-	-	100,000	CIP
1,815,000	300,000	650,000	4,500,000	850,000	9,774,000	

FY 2020 - 2025 Capital Improvements Plan							GOAL / TYPE
FY21	FY22	FY23	FY24	FY25	Six Year Planning Period		
-	-	-	-	-	7,000	CIP	
		30,000			30,000	CIP	
30,000	-	-	-	-	30,000	CIP	
318,200	641,800	908,100	951,100	272,356	3,580,556	CIP	
348,200	641,800	938,100	951,100	272,356	3,647,556		
-	-	-	-	-	500,000	CIP	
-	-	-	-	-	26,000	CIP	
-	-	-	-	-	350,000	CIP	
-	-	-	-	-	300,000	CIP	
-	-	-	-	-	150,000	CIP	
900,000	-	-	-	-	1,300,000	CIP	
600,000	-	-	-	-	750,000	CIP	
-	-	-	-	-	100,000	CIP	
-	-	-	-	-	850,000	CIP	
325,000	-	-	-	-	325,000	CIP	
-	-	-	-	-	100,000	CIP	
-	300,000	300,000	150,000	-	750,000	CIP	
1,176,000	-	-	-	-	20,382,000	CIP	
125,000	300,000	850,000	-	-	1,875,000	CIP	
-	-	-	-	-	7,150,000	CIP	
-	-	-	-	-	50,000	CIP	
-	-	-	-	-	100,000	CIP	
3,126,000	600,000	1,150,000	150,000	-	35,058,000		
-	-	-	-	-	5,000	CIP	
-	-	-	-	-	5,000		
-	-	-	-	-	250,000	CIP	
1,680,000	-	-	-	-	2,000,000	CIP	
-	-	-	-	-	400,000	CIP	
-	-	500,000	-	-	1,200,000	CIP	
150,000	-	-	-	-	300,000	CIP	
420,000	420,000	400,000	100,000	100,000	1,865,000	CIP	
45,000	45,000	45,000	45,000	45,000	270,000	CIP	
2,295,000	465,000	945,000	145,000	145,000	6,285,000		
-	-	44,000	-	-	44,000	CIP	
-	-	-	-	-	350,000	CIP	
-	-	-	-	400,000	400,000	CIP	
-	-	-	-	-	1,500,000	CIP	
-	-	44,000	-	400,000	2,294,000		
-	-	-	-	-	20,000	CIP	
-	-	-	-	-	10,000	CIP	
-	-	-	-	-	35,000	CIP	
280,800	226,800	605,700	1,180,600	953,919	4,383,319	CIP	
280,800	226,800	605,700	1,180,600	953,919	4,448,319		
-	40,000	-	-	-	40,000	CIP	
-	40,000	-	-	-	40,000		
125,500	125,500	145,500	145,500	145,500	811,300	CIP	
-	-	-	-	-	120,000	CIP	
-	-	-	-	-	18,000	CIP	
-	-	-	-	-	185,000	CIP	
50,000	-	-	-	-	50,000	CIP	
-	-	-	-	-	30,000	CIP	
-	-	-	-	-	85,000	CIP	
175,500	125,500	145,500	145,500	145,500	1,299,300		
\$ 23,849,000	\$ 10,799,100	\$ 6,369,300	\$ 9,292,200	\$ 3,836,775	\$ 191,492,775		

