



City Commission Workshop

January 10, 2012



Workshop Agenda

- **Where we're at in the process.**
- **Brief review of the 4-day corridor charrette.**
- **Introduce corridor form-based overlays.**
- **What's next?**

- **Other regulations**
 - Sound
 - Landscaping



PRELIMINARY ANALYSIS

- Existing Conditions Analysis –
- Market Conditions Assessment
- Listening Sessions

DESIGN CHARRETTE

- Planning & Design Studies
- Preliminary Improvement Plans
- Zoning Code Outline

THIS WEEK

FINAL PLANNING & CODING

- Revisions to Charrette Plans & Designs
- Draft Framework Plan & Code
- Final Framework Plan & Code

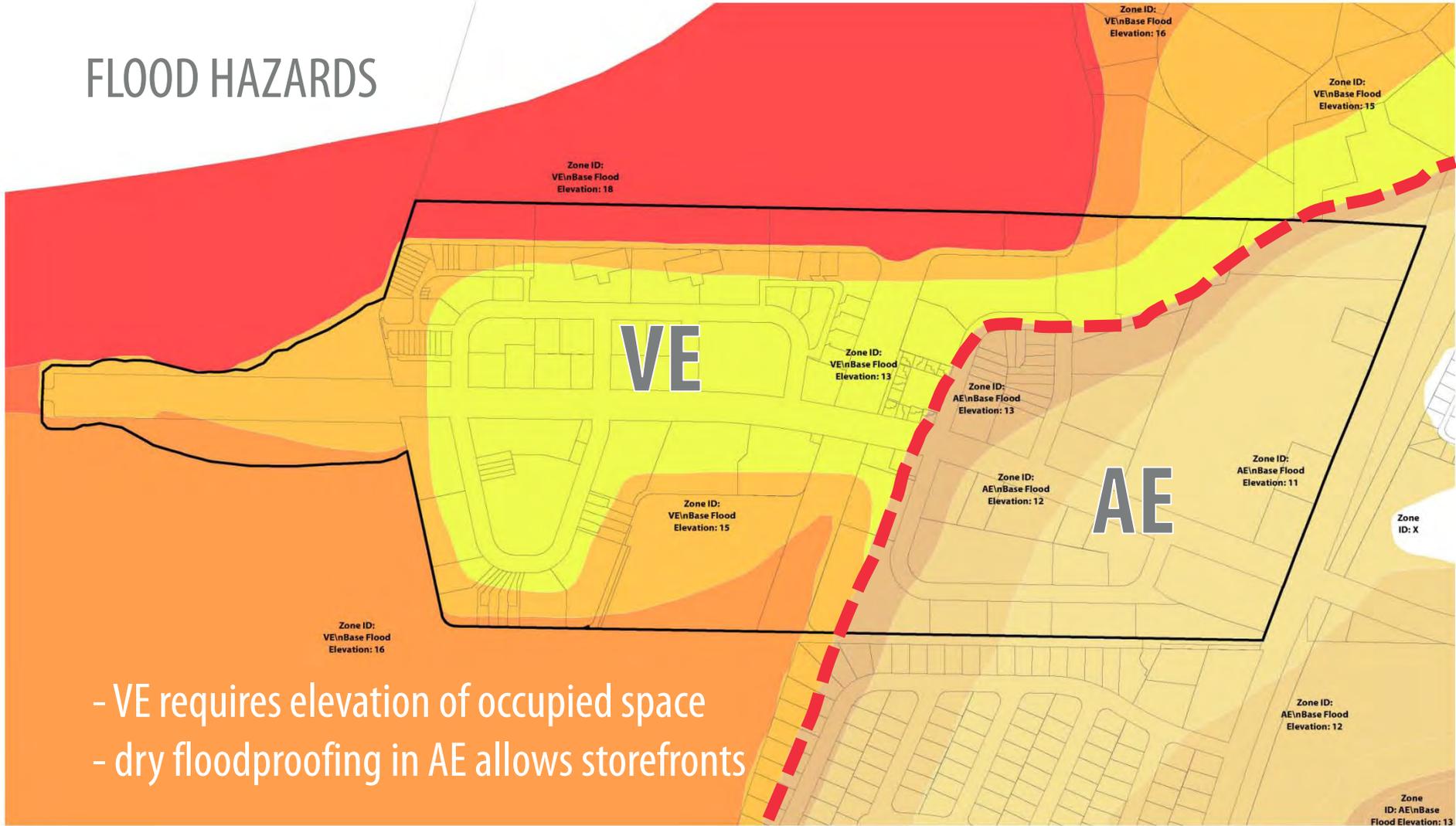


STUDY AREA





FLOOD HAZARDS

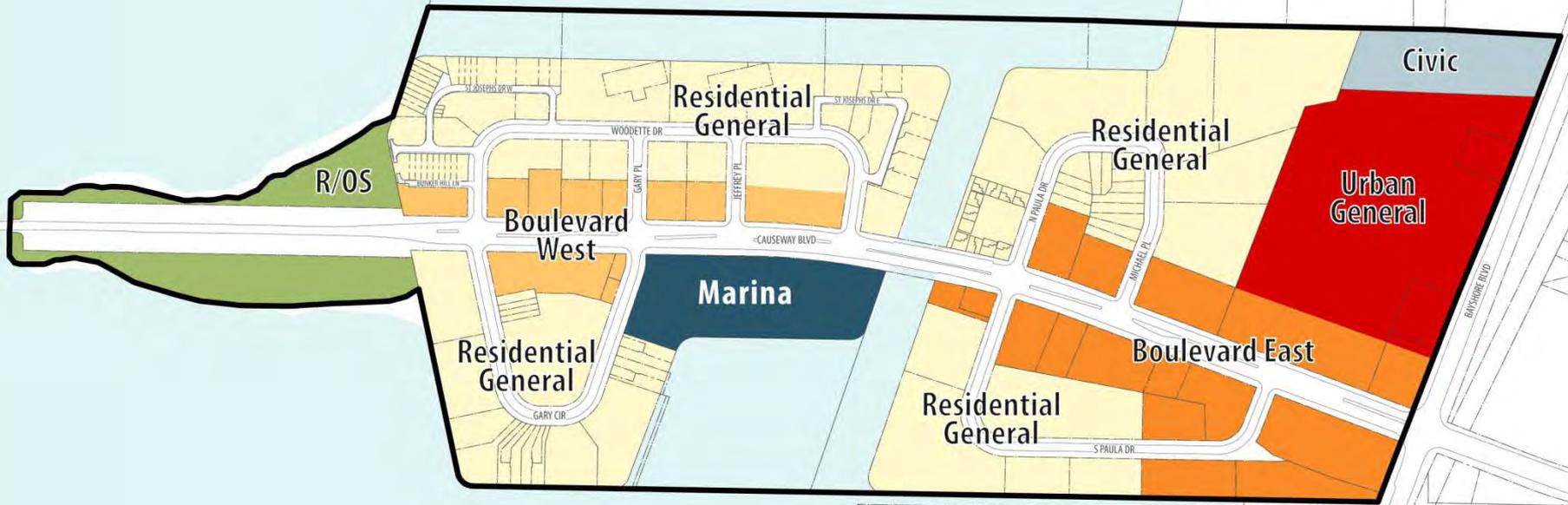


- VE requires elevation of occupied space
- dry floodproofing in AE allows storefronts





REGULATING PLAN



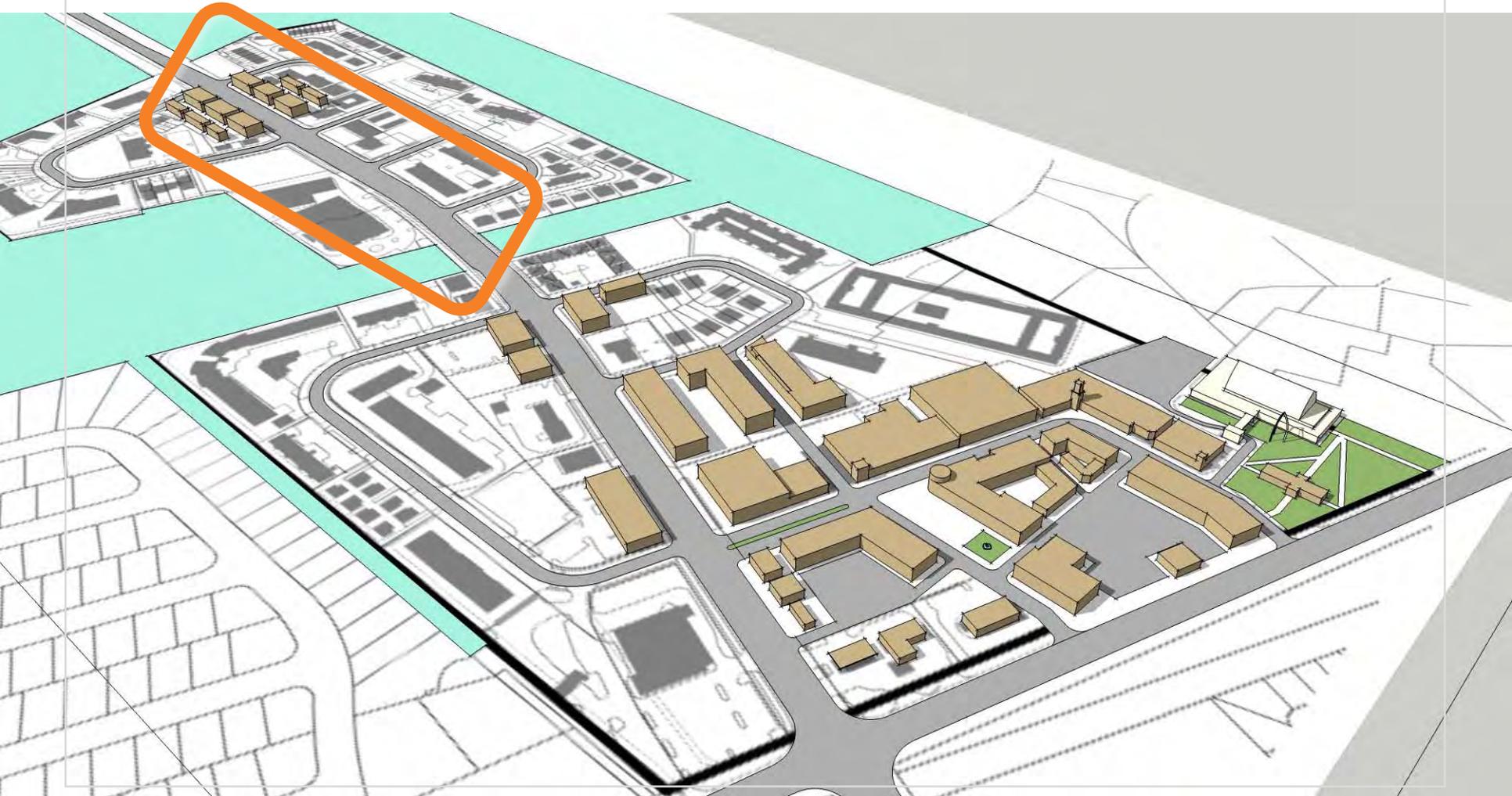


> **BOULEVARD WEST & EAST DISTRICTS**

- pedestrian-friendly frontage
- limits on auto-oriented land uses
- east: ground floor retail with café space and parking in rear
- west: elevated with screened parking



BOULEVARD WEST



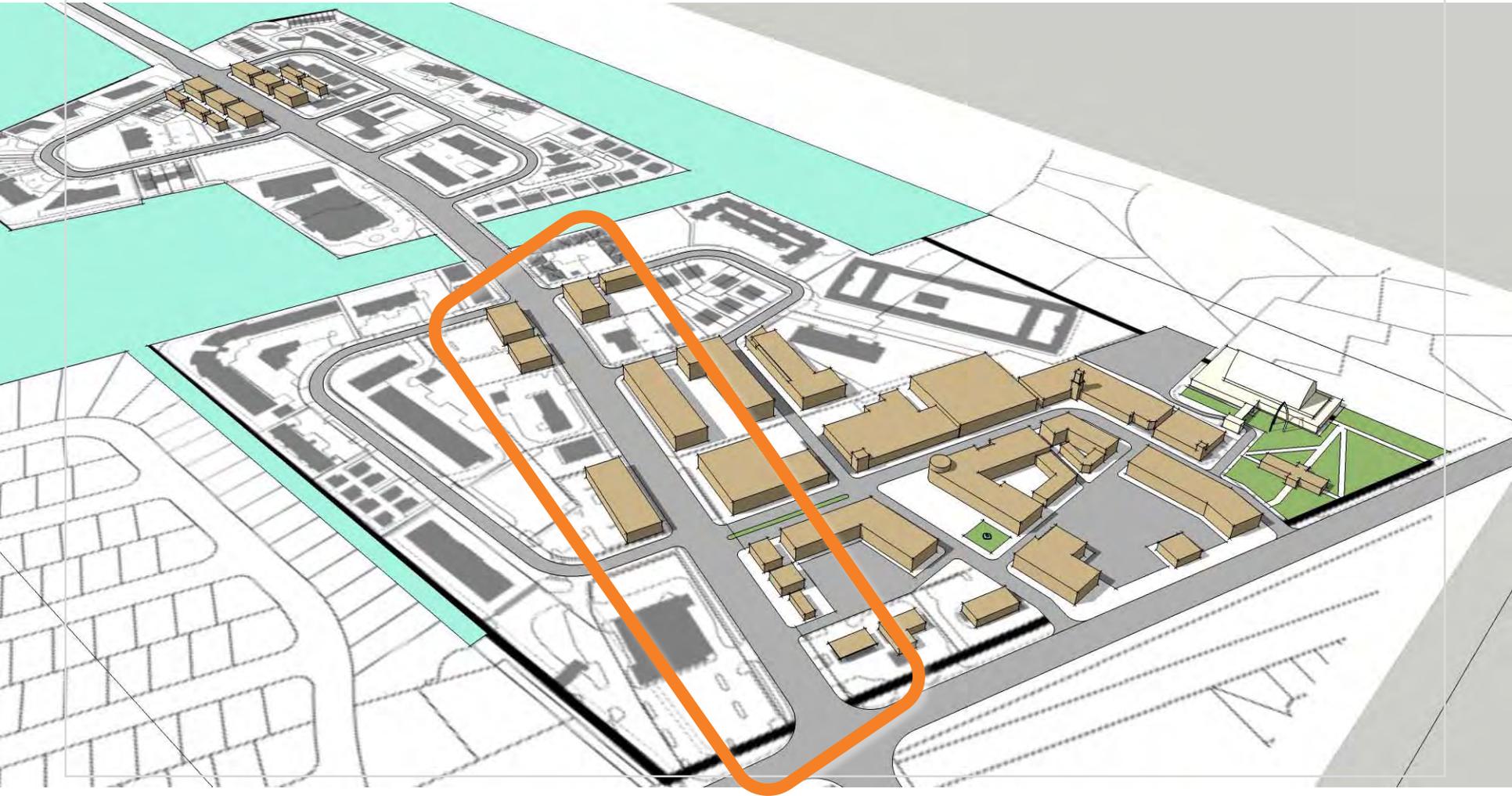


BOULEVARD WEST





BOULEVARD EAST





CAUSEWAY EAST



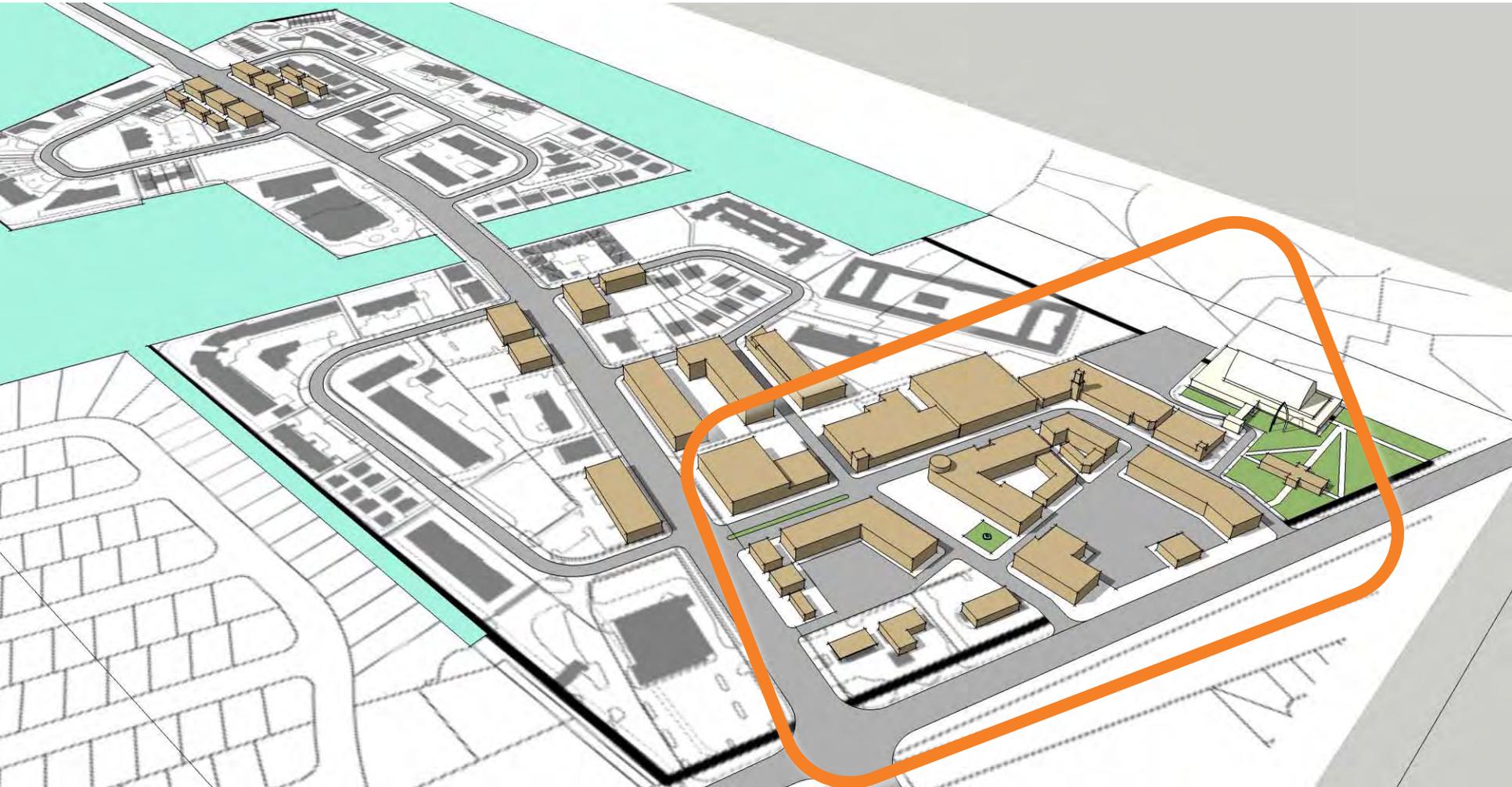


> **URBAN GENERAL (Shopping Center)**

- plan for phased redevelopment
- mixed uses including residential, commercial and possibly hotel
- buildings aligned along walkable streets



SHOPPING CENTER REDEVELOPMENT





URBAN GENERAL





> RESIDENTIAL GENERAL

- post-disaster or market-driven change
- elevated buildings with screened parking
- low-rise but similar densities
- carriage house units along internal streets



RESIDENTIAL GENERAL





RESIDENTIAL GENERAL





RESIDENTIAL GENERAL





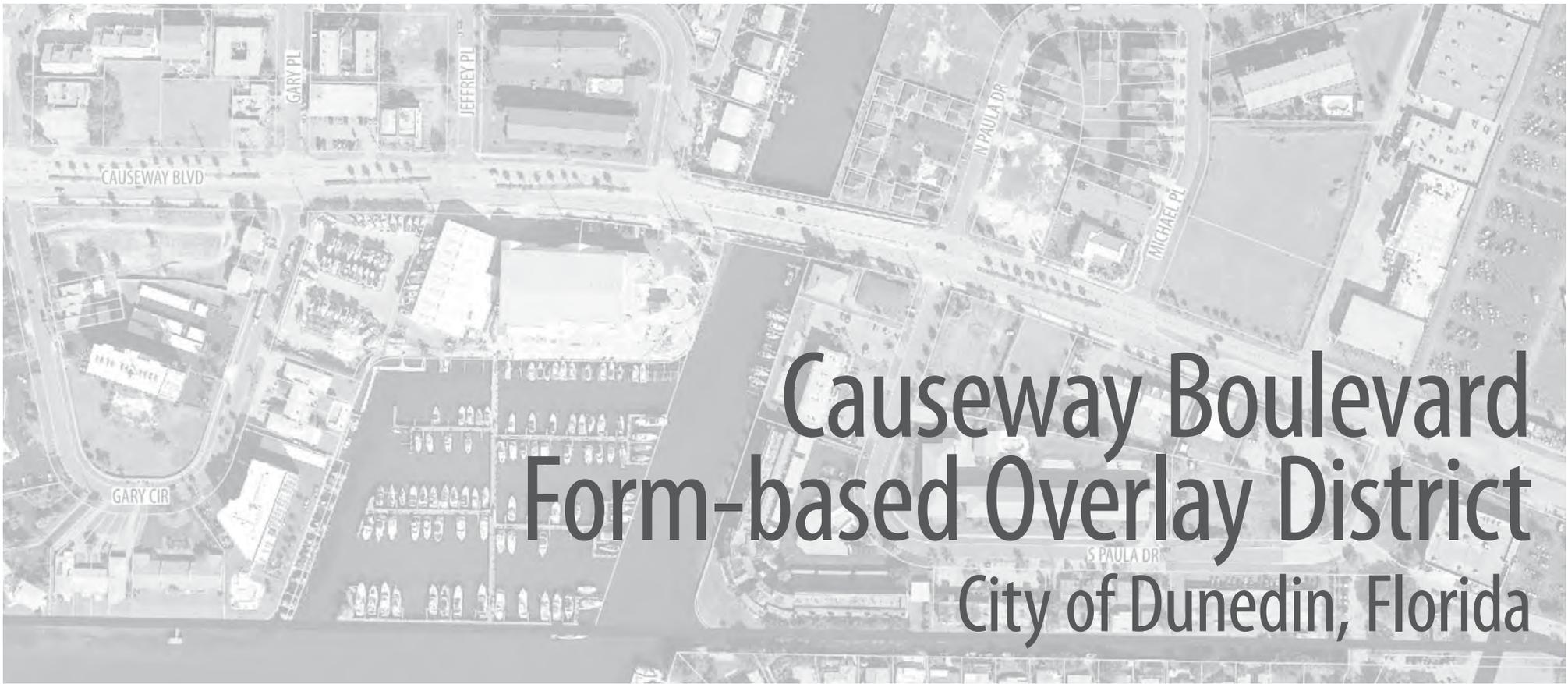
Form-Based Zoning Overlays

Boulevard East

Boulevard West

Residential General

Urban General



Causeway Boulevard Form-based Overlay District City of Dunedin, Florida



Seth Harry & Associates
WTL Associates

Prepared by HDR, Inc. for the City of Dunedin, Florida | December 29, 2011



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Form-Based Overlay District

Purpose & Intent

The Causeway Boulevard Form-Based Overlay District provides standards for the development and redevelopment of property along the Causeway Boulevard Corridor. The form-based standards, which supplement standards in the underlying zone districts, are designed to ensure individual projects contribute to the creation of a safe, attractive, and pedestrian-friendly destination for local residents and visitors.

As called for in the standards, buildings should be aligned along public streets, entries should open onto sidewalks or outdoor patio and café spaces, and parking and service areas should be in rear yard and mid-block locations and screened from public view. To reinforce the corridor's pedestrian-friendly character, auto-oriented uses such as service stations, drive-through facilities, and auto repair businesses are prohibited from fronting directly on Causeway Boulevard.

The Causeway Boulevard Form-Based Overlay District provides building and site design standards applicable to development and redevelopment projects in each of three related subdistricts. Figure 2. Overlay District and Subdistrict Boundaries shows the limits of the district and the boundaries of each subdistrict.

Unless otherwise addressed in the subdistrict standards, land uses, densities, intensities, and other development conditions are controlled by the underlying zoning category.

Figure 1. Illustrative Example of Buildings in Boulevard East Subdistrict

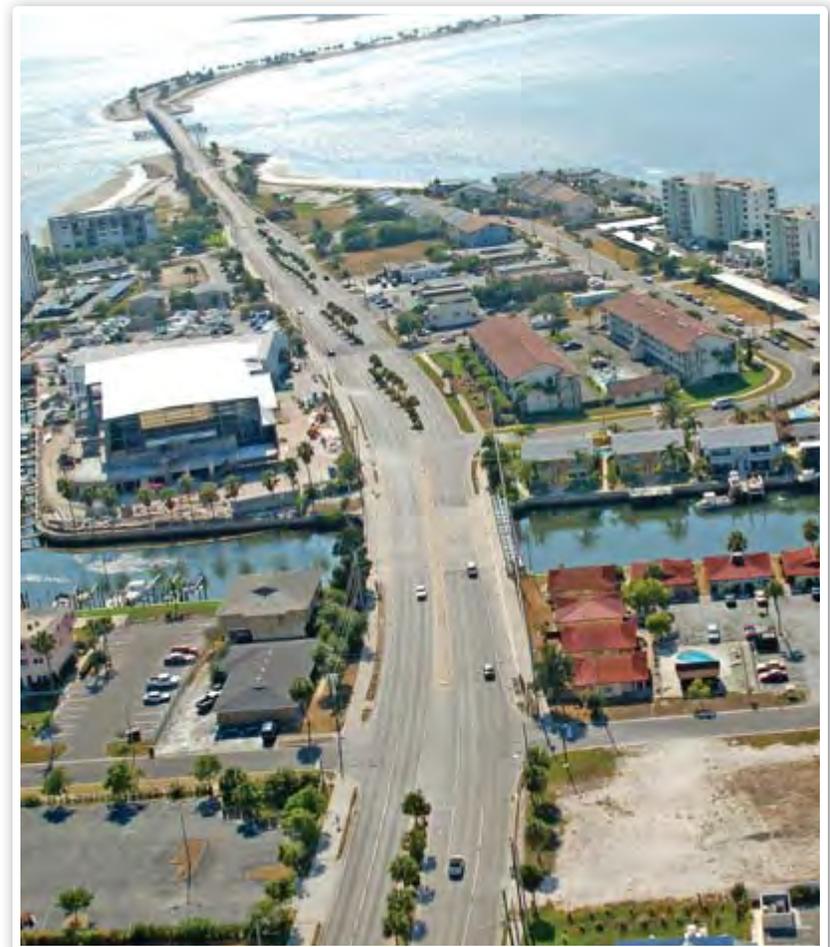
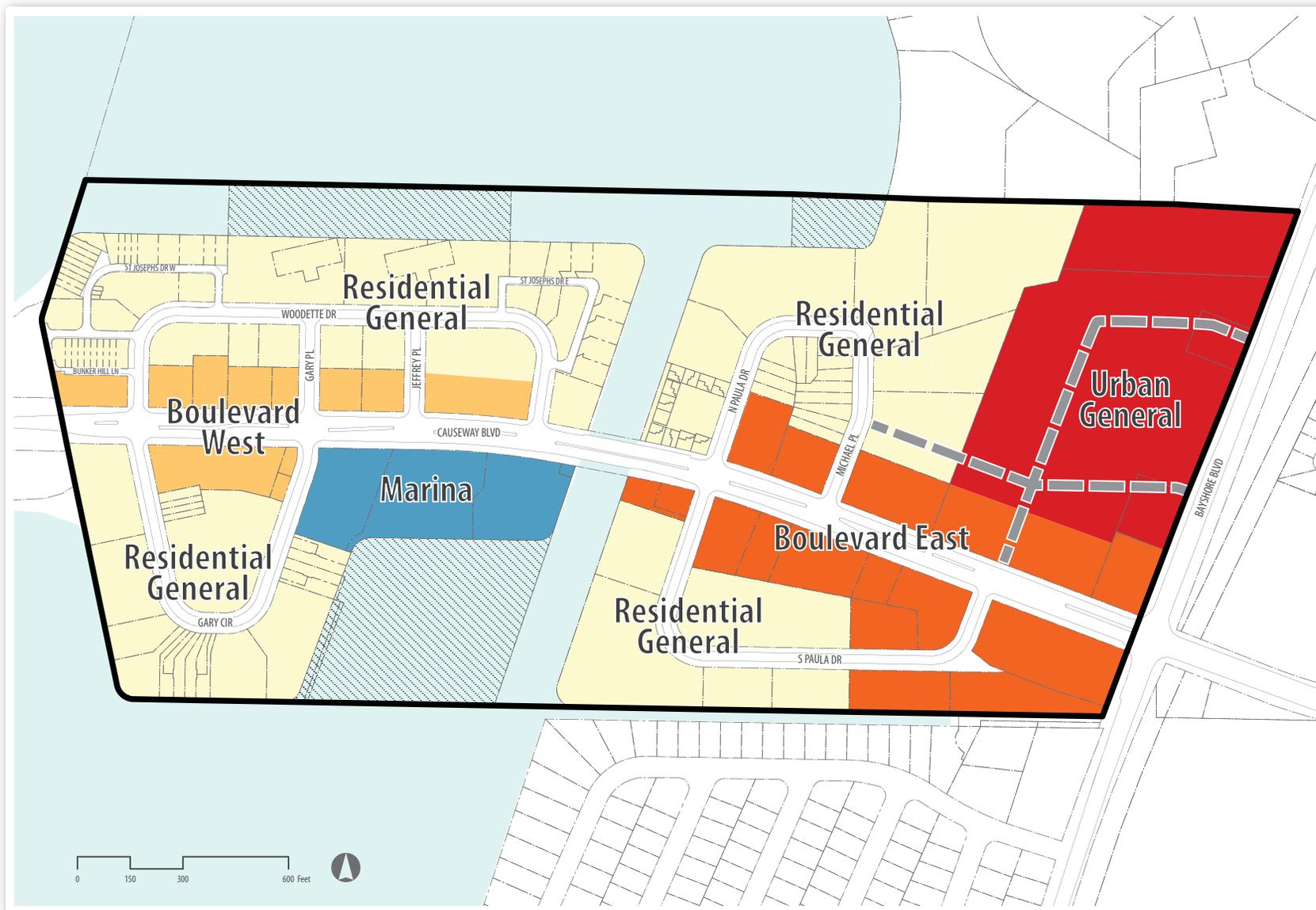


Figure 2. Overlay District & Subdistrict Boundaries



Boulevard East Subdistrict

Overview

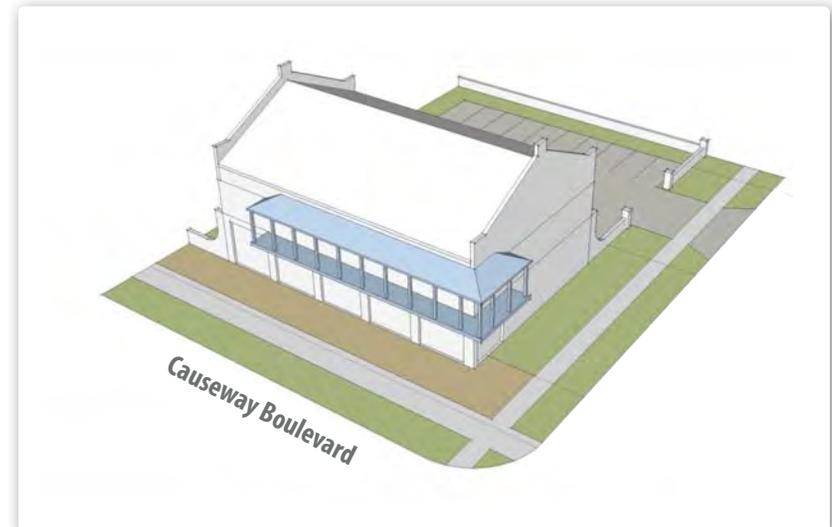
The Boulevard East subdistrict is intended for mixed use development with active ground-level uses oriented to Causeway Boulevard and strict limits on future auto-oriented land uses. Subdistrict standards call for buildings with mostly continuous storefronts and modest front yard setbacks to accommodate sidewalk extensions, patios, and café space. Parking and service areas are to be located to the rear of buildings.

Building Use

Within the Boulevard East subdistrict, permitted uses shall be those allowed in the underlying zone districts, with the exception that auto-oriented uses such as auto service station, automotive repair, drive-in restaurant, rental of passenger cars and utility trailers, and other uses with the potential to detract from the subdistrict's pedestrian-orientation, are prohibited.

Retail, restaurant, and entertainment uses, where permitted in underlying zone districts, shall be located in the first story of buildings with storefronts and entries at ground level, and aligned along adjacent public sidewalks. Where non-residential uses are not permitted in underlying zone districts, projects shall be designed so that individual dwelling units have front façades and entries oriented to Causeway Boulevard.

Figure 3. Illustrative Example of Buildings in Boulevard East Subdistrict



Boulevard East Subdistrict (cont.)

Figure 4. Boulevard East Subdistrict Character Images



Boulevard East Subdistrict (cont.)

Building Placement

Primary Buildings

Primary buildings shall be placed to the front of lots and aligned along setback lines in compliance with the following requirements as shown in Figure 5. Primary Building Placement.

- Front Setback - Causeway Boulevard (A): 6' min., 12' max.
- Front Setback - Secondary Street (B): 6' min., 12' max.
- Side Setback (C): 7.5' min.
- Rear Setback (D): 20' min.

Accessory Buildings

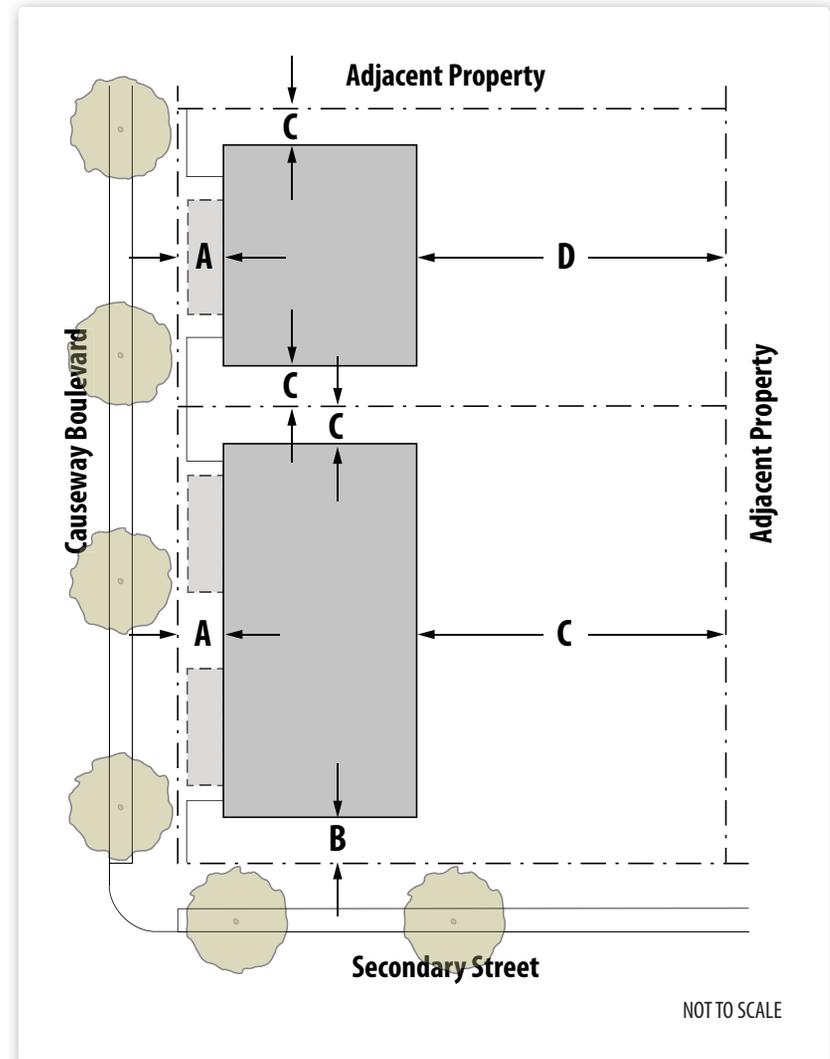
Accessory buildings shall be placed to the rear of lots with façades aligned parallel to setback lines in compliance with the following requirements.

- Front Setback - Causeway Boulevard: 50' min.
- Front Setback - Secondary Street: Aligned with secondary street façade of primary building.
- Side Setback: 7.5' min.
- Rear Setback - 1-Story Buildings (D): 5' min.
- Rear Setback - 2- or 3-Story Buildings (D): Same as for primary building.

Continuous Frontage

Along Causeway Boulevard, primary buildings shall occupy a minimum of 70% of the lot width along the required front setback. The percentage may be reduced to accommodate driveways on narrow lots without secondary street, cross-parcel, or alley access to rear yard parking.

Figure 5. Primary Building Placement



Boulevard East Subdistrict (cont.)

Building Form

The form of buildings shall comply with the following requirements as shown in Figure 6. Building Form. Where non-residential uses are not permitted in the underlying zone district, building form standards provided for the Boulevard West subdistrict shall apply.

Building Height (E)

Building heights shall be controlled by the underlying zone district.

Building Step Back (F)

Above the 3rd story, all front façades shall step back 12' from lower-story façades. As an alternative to a step back, building space above the 3rd story may be located within a pitched roof with dormer windows projecting to the plane of the façade.

Building Projections (J)

The following building elements may project beyond building façades into front setbacks as follows:

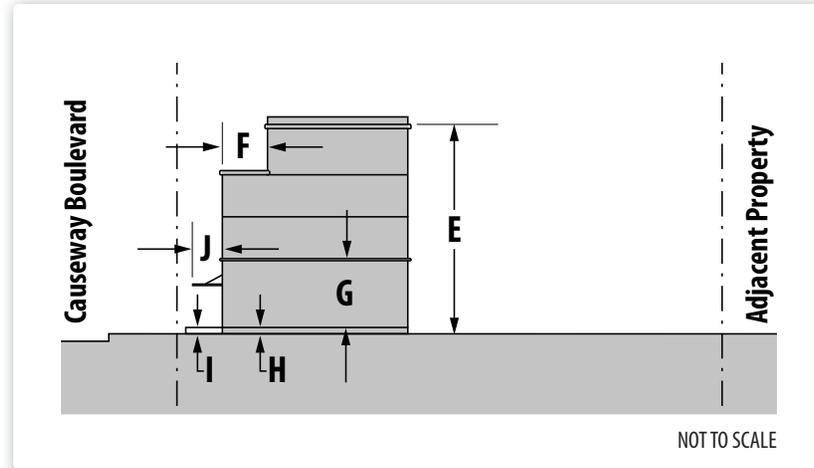
- Bay Windows, Roof Overhangs, and Eaves: 2' max.
- Balconies: 6' max.
- Porches, Patios, and Decks: 10' max., but not closer than 2' from lot line.
- Awnings and Canopies: 10' max.

Permitted projections may not extend beyond lot lines.

First-Story Clear Ceiling Height (G)

16' min. measured from finished floor to finished floor.

Figure 6. Building Form



First-Story Finished Floor Elevation (H)

First-story building space for non-residential use shall not be elevated higher than 1.5' above the average finished grade at the front setback line and shall be designed to meet flood proofing requirements in Section 105-22.8 of the Dunedin Code of Ordinances.

Porch, Patio, & Deck Elevation (I)

Porches, patios, and decks shall not be elevated higher than 1' above the average finished grade at the front setback line and shall be designed to meet applicable accessibility requirements.

Boulevard East Subdistrict (cont.)

Parking, Service Areas & Vehicular Access

Parking, service areas, and mechanical equipment shall be located and designed in compliance with the following standards as shown in Figure 7, Parking & Service Area Placement.

Surface Parking Lots & Service Areas

- Front Setback - Causeway Blvd (K): 50' min.
- Secondary Streets (L): 15' min.
- Side Setback (M): 7.5' min.
- Rear Setback (N): 5' min.

Side and rear parking setbacks may be reduced to 0' where parking abuts and is accessible from a surface parking lot on an adjacent property. Tuck-under parking (O) enclosed within a building envelope shall be located to the rear of the building, accessed from a drive aisle or alley, and be set back at least 20' from the front building façade along Causeway Boulevard.

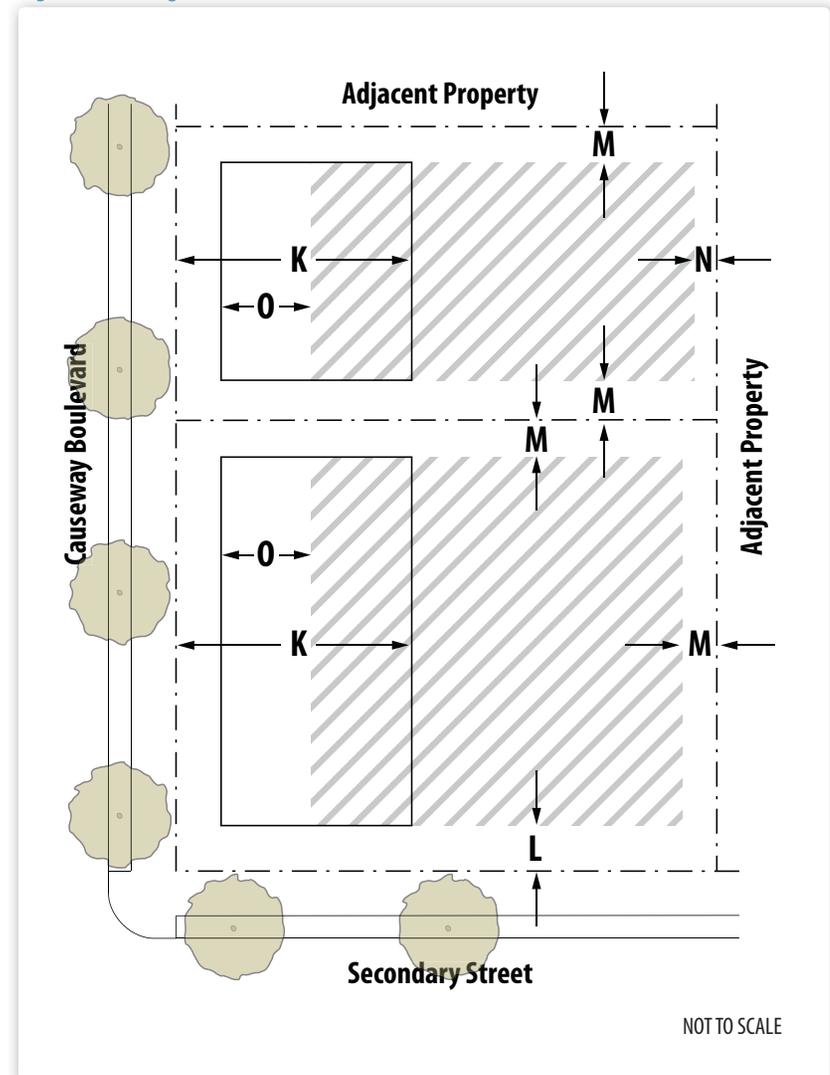
Vehicular Access

Parking and service areas shall be accessed from a rear alley, a shared access driveway, or a driveway from a secondary street where no alley or shared access driveway is present. To limit curb cuts and driveway aprons, vehicular access directly from Causeway Boulevard is permitted only where no practical alternative exists.

Screening

Parking, service areas, and mechanical equipment shall be enclosed within buildings or screened from public view by finished masonry walls, landscaping, or other means. Screening shall be designed as an extension of the architectural and landscape design of the project.

Figure 7. Parking & Service Area Placement



Boulevard East Subdistrict (cont.)

Frontage Conditions

The front façades of primary buildings and improvements within required front setbacks shall comply with the following requirements. Where non-residential uses are not permitted in the underlying zone district, building frontage conditions standards for the Boulevard West subdistrict shall apply.

Building Façades

The front façades of buildings shall be aligned parallel to front lot lines and designed with ground-level storefronts featuring high levels of transparency and generous display windows. Storefront window and door glazing shall be no less than 65% of the total area of the first-story façade and shall be in clear glass. Storefronts shall include canopies, awnings, arcades, galleries, or other open-air architectural features intended to shade patio, sidewalk, or other outdoor space. Buildings on corner lots shall be designed with two façades of equal architectural expression.

Building Entries

Primary entries to first-story building space shall be located along Causeway Boulevard at or near sidewalk grade. Entries to upper stories shall be provided through a separate ground-level entry or common foyer.

Front Setback Area

The area between the right-of-way and front façade of buildings along Causeway Boulevard shall be improved as outdoor café or patio space. Between 50% and 80% of the setback area shall be paved or otherwise improved as patio, deck, or porch space. Placement of vehicular parking, driveways, stormwater management facilities, or mechanical equipment within the front setback area is prohibited.

Sidewalk Extension

A minimum 2' wide paved extension of the public sidewalk is required along Causeway Boulevard.

Boulevard West Subdistrict

Overview

The Boulevard West subdistrict is intended primarily for moderate density residential uses in buildings with front façades, porches, and entry stairs oriented to Causeway Boulevard. In compliance with floodplain elevation requirements, the first story of buildings is elevated above ground level and accessed by internal elevators or internal/external stairs. Modest front yard setbacks along Causeway Boulevard accommodate porch projections, stairs, and ground-level patios and landscape areas. Parking and service areas are located below or to the rear of buildings and screened from view from public rights-of-way.

Building Use

Residential is the primary land use within the subdistrict although limited non-residential use is allowed where permitted in underlying zone districts. Regardless of use, development and redevelopment projects shall comply with subdistrict standards.

Figure 8. Illustrations of New Buildings in Boulevard West Subdistrict



Boulevard West Subdistrict (cont.)

Figure 9. Boulevard West Subdistrict Character Images



Boulevard West Subdistrict (cont.)

Building Placement

Primary Buildings

Primary buildings shall be placed to the front of lots and aligned along setback lines in compliance with the following requirements as shown in Figure 10. Primary Building Placement.

- Front Setback - Causeway Blvd (A): 16' min., 20' max.
- Front Setback - Secondary Street (B): 12' min., 20' max.
- Side Setback (C): 7.5' min.
- Rear Setback (D): 20' min.

Accessory Buildings

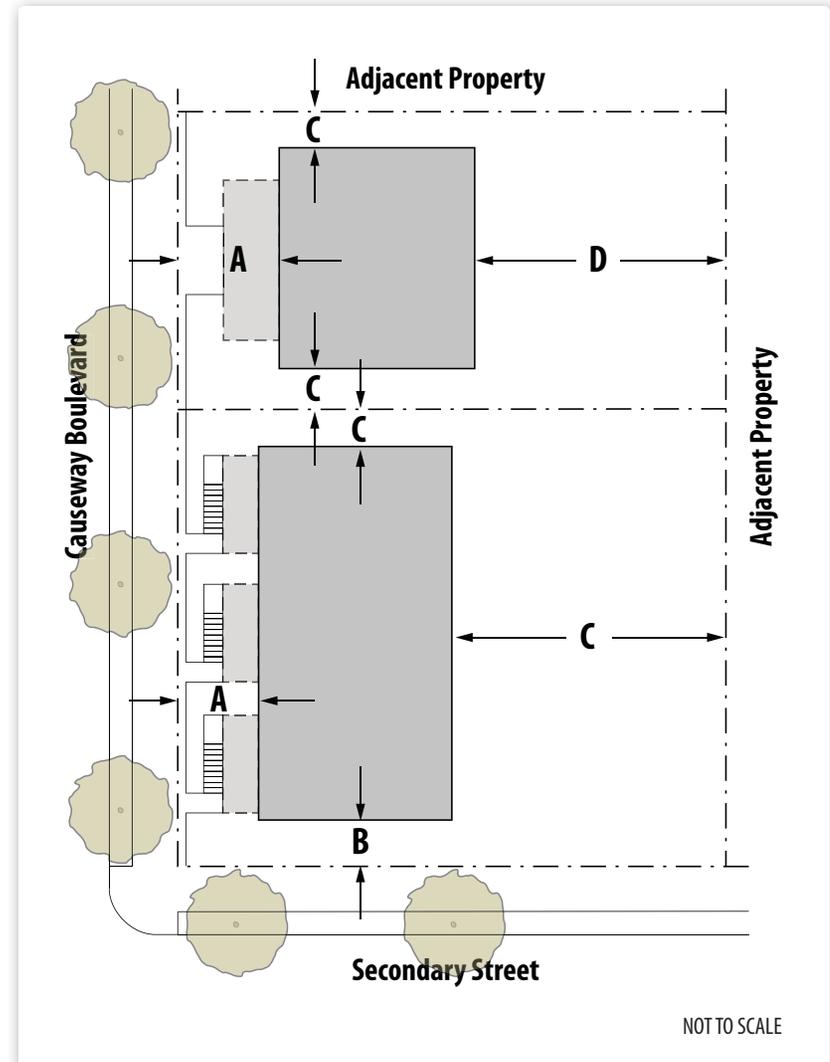
Accessory buildings shall be placed to the rear of lots with façades aligned parallel to setback lines in compliance with the following requirements.

- Front Setback - Causeway Blvd: 50' min.
- Front Setback - Secondary Street: Aligned with secondary street façade of primary building.
- Side Setback: 7.5' min.
- Rear Setback - 1 Story Buildings (D): 5' min.
- Rear Setback - 2- or 3- Story Buildings (D): Same as for primary building.

Continuous Frontage

Along Causeway Boulevard, buildings shall occupy a minimum of 60% of the lot width along the required front setback. The percentage may be reduced to accommodate driveways on narrow lots without secondary street, cross-parcel, or alley access to rear yard parking.

Figure 10. Primary Building Placement



Boulevard West Subdistrict (cont.)

Building Form

The form of buildings shall comply with the following requirements as generally shown in Figure 11. Building Form.

Building Height (E)

Building heights shall be limited to 4 stories with maximum height in feet controlled by the underlying zone district.

Building Step Back (F)

Above the 2nd story, all front façades of buildings shall step back 12' from lower-story façades. As an alternative to a step back, building space above the 2nd story may be located within a pitched roof with dormer windows projecting to the plane of the façade.

Building Projections (J)

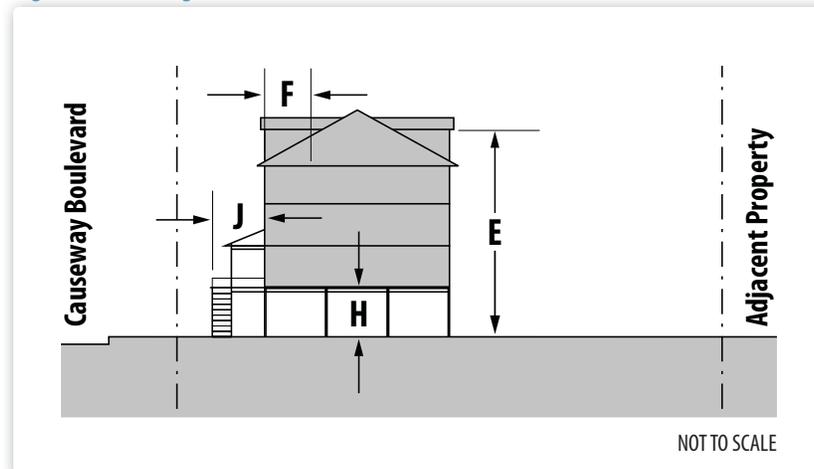
The following building elements may project beyond building façades into front setbacks as follows:

- Bay Windows, Roof Overhangs, and Eaves: 2' max.
- Balconies: 6' max.
- Covered Porches: 6' min, 10' max., but not closer than 6' from lot line. Porch projections may be up to 2 levels in height and shall extend across at least 60% of the front building façade.
- Stairs: 6' max., but not closer than 6' from lot line.

First-Story Finished Floor Elevation (H)

The finished floor elevation of first-story building space shall be elevated to meet floodplain requirements in Section 105-22.8 of the Dunedin Code of Ordinances.

Figure 11. Building Form



Boulevard West Subdistrict (cont.)

Parking, Service Areas, & Vehicular Access

Parking, service areas, and mechanical equipment shall be located and designed in compliance with the following standards as shown in Figure 12. Parking & Service Area Placement.

Surface Parking Lots & Service Areas

- Front Setback-Causeway Boulevard (K): 50' min.
- Front Setback-Secondary Streets (L): 15' min.
- Side Setback (M): 7.5' min.
- Rear Setback (N): 5' min.

Side and rear parking setbacks may be reduced to 0' where parking abuts and is accessible from a surface parking lot on an adjacent property. Tuck-under parking (O) enclosed within a building envelope shall be located to the rear of the building, accessed from a drive aisle or alley, and be set back at least 2' from the front facade.

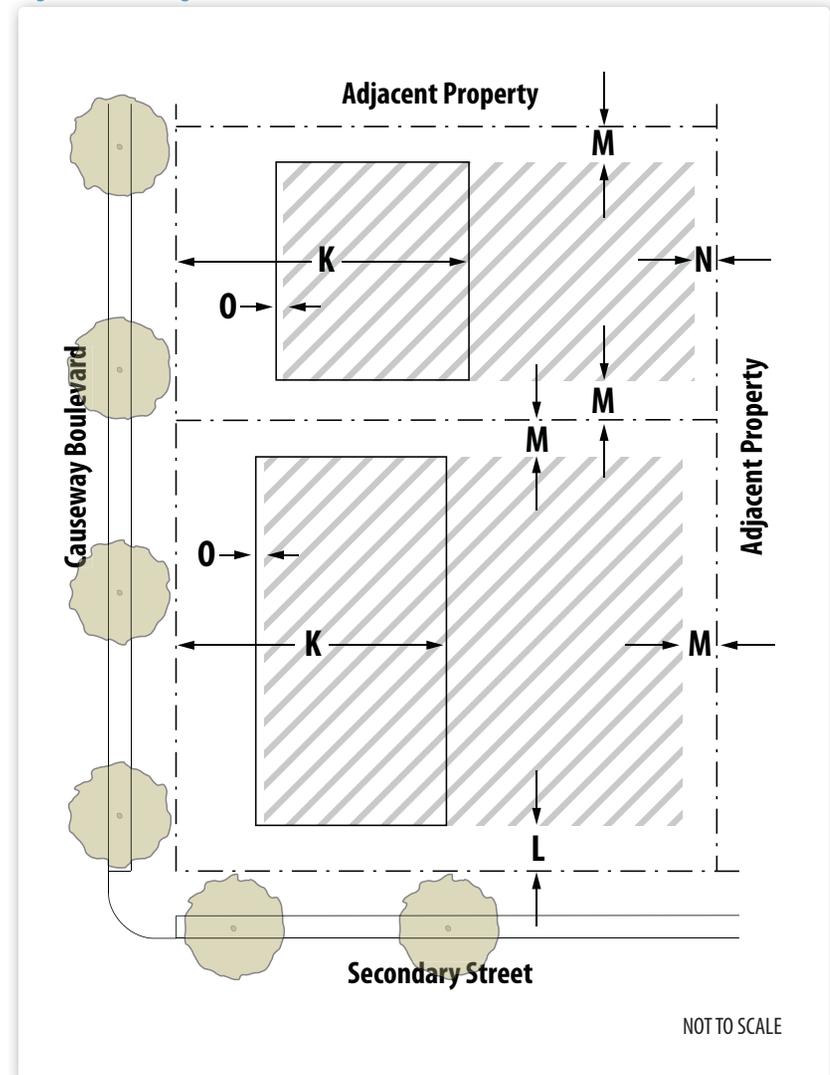
Vehicular Access

Parking and service areas shall be accessed from a rear alley, a shared access driveway, or a driveway from a secondary street where no alley or shared access driveway is present. To limit curb cuts and driveway aprons, vehicular access directly from Causeway Boulevard is permitted only where no practical alternative exists.

Screening

Parking, service areas, and mechanical equipment shall be enclosed within buildings or screened from public view by finished masonry walls, landscaping, or other means. Screening shall be designed as an extension of the architectural and landscape design of the project.

Figure 12. Parking & Service Area Placement



Boulevard West Subdistrict (cont.)

Frontage Conditions

The front façades of primary buildings and improvements within required front setbacks shall comply with the following requirements.

Building Façades

The front façades of buildings shall be aligned along front setback lines. Ground-level building elements of elevated buildings, including stairs, architectural screen walls, columns, posts, and others, shall be designed as integral parts of the building with treatments, material, and details matching those on first-story and upper-story façades. Open foundations with piers or piles supporting buildings should be fitted with non-structural, architectural screening panels or breakaway walls to match the overall architectural design of the building. Piers or piles should be painted or wrapped with architectural molding or trim.

Building Entries

Primary entries to first-story building space shall be located along Causeway Boulevard. First-story building space shall be accessed by external stairs or ground-level entries to internal elevators or stairways. Entries to upper stories shall be provided through a separate ground-level entry or common foyer.

Front Setback Area

At least 50% of the area between the right-of-way and front façade of buildings along Causeway Boulevard shall be landscaped. Placement of vehicular parking, driveways, stormwater management facilities, or mechanical equipment within the front setback area is prohibited.

Urban General Subdistrict

Overview

The Urban General subdistrict provides for mixed use, pedestrian-oriented development on the site of the Causeway Plaza shopping center. Subdistrict standards require active ground-level uses along Bayshore Boulevard and new internal streets, mixed use buildings with continuous storefronts along sidewalks, and parking and service areas located to the rear of buildings.

Building Use

Within the Urban General subdistrict, permitted uses shall be those allowed in the underlying zone districts with the exception that auto-oriented uses such as auto service stations, automotive repair, restaurants with drive-through facilities, and other uses with the potential to detract from the subdistrict's pedestrian-orientation are prohibited. Retail, restaurant, and entertainment uses shall be located in the first story of buildings with entries at ground level opening onto adjacent sidewalks.

Block & Lot Configuration

Projects within the Urban General subdistrict shall be organized around interconnected networks of internal streets and development blocks, scaled to accommodate perimeter buildings and mid-block parking and service areas. The perimeter of development blocks shall be between 1,600 - 2,000 linear feet with allowance for different block sizes so long as site access, vehicular circulation, and pedestrian safety are not compromised. Internal streets, whether maintained for public access by dedication or by easement, shall connect to the existing street network. Conceptual street alignments for the Urban Center subdistrict are shown on Figures 13 and 14.

Figure 13. Illustrations of New Buildings in the Urban General Subdistrict

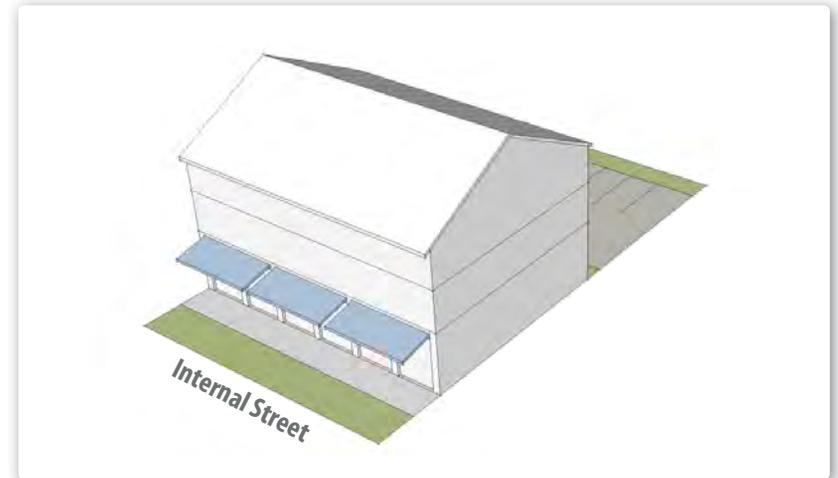


Figure 14. Shopping Center Redevelopment Concept Sketch



Urban General Subdistrict (cont.)

Figure 15. Urban General Subdistrict Character Images



Urban General Subdistrict (cont.)

Building Placement

Primary Buildings

Primary Buildings shall be located along block perimeters and placed in compliance with the following requirements as shown in Figure 16. Primary Building Placement.

- Front Setback - Internal Street (A): 0' min., 6' max.
- Front Setback - Bayshore Boulevard (B): 6' min., 12' max.
- Side Setback (C): 0' if attached building, 7.5' min. if not attached.
- Rear Setback (D): 20' min.

Along internal streets without right-of-way dedication, buildings shall be no closer than 12' from the face of an adjacent curb line.

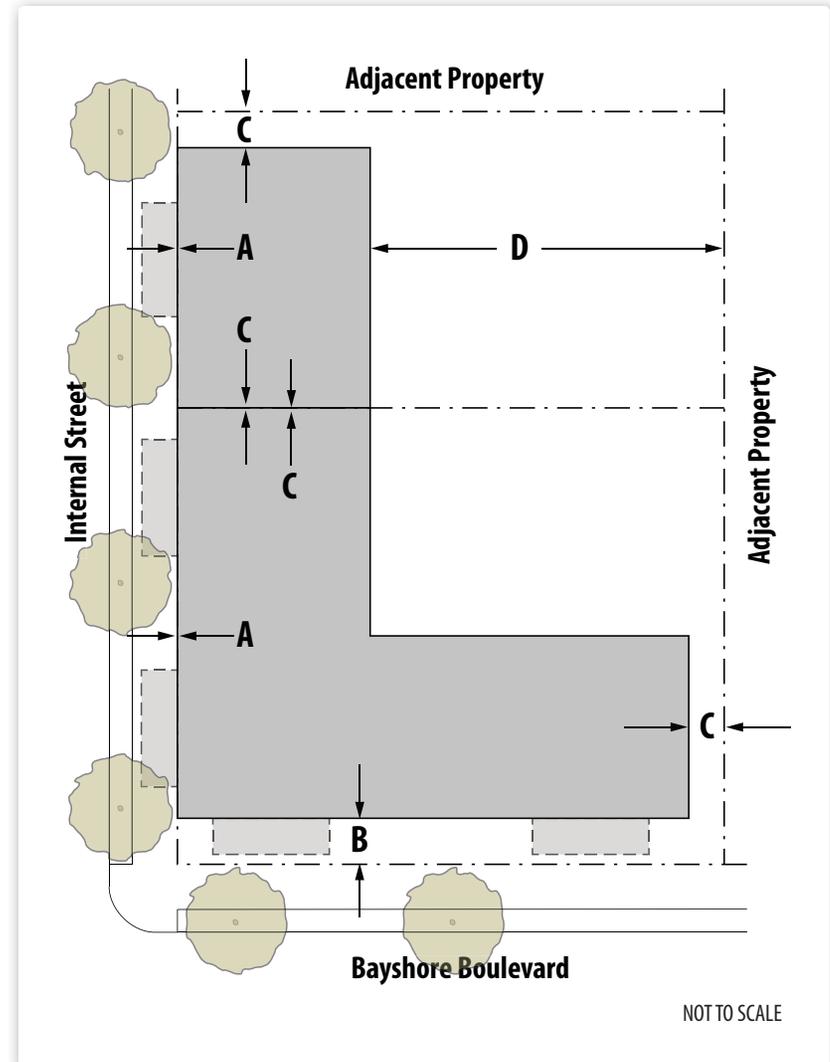
Accessory Buildings

Accessory Buildings shall meet requirements of underlying zone districts.

Continuous Frontage

Along Bayshore Boulevard, buildings shall occupy a minimum of 30% of the lot width along the required front setback. Along internal streets, buildings shall occupy a minimum of 70% of the block frontage. The percentage may be reduced to accommodate driveways on narrow lots without secondary street, cross-parcel, or alley access to rear yard parking.

Figure 16. Primary Building Placement



Urban General Subdistrict (cont.)

Building Form

The form of buildings shall comply with the following requirements as shown in Figure 17. Building Form.

Building Height (E)

Building heights shall be controlled by the underlying zone district with the exception that the provision for additional setbacks for heights above 30' shall not apply to buildings adjacent to residential uses within the Urban General or Boulevard East subdistricts.

Building Step Back (F)

Above the 3rd story, all front façades shall step back 12' from lower-story façades. As an alternative to a step back, building space above the 3rd story may be located within a pitched roof with dormer windows projecting to the plane of the façade.

Building Projections (J)

The following building elements may project beyond building façades as follows:

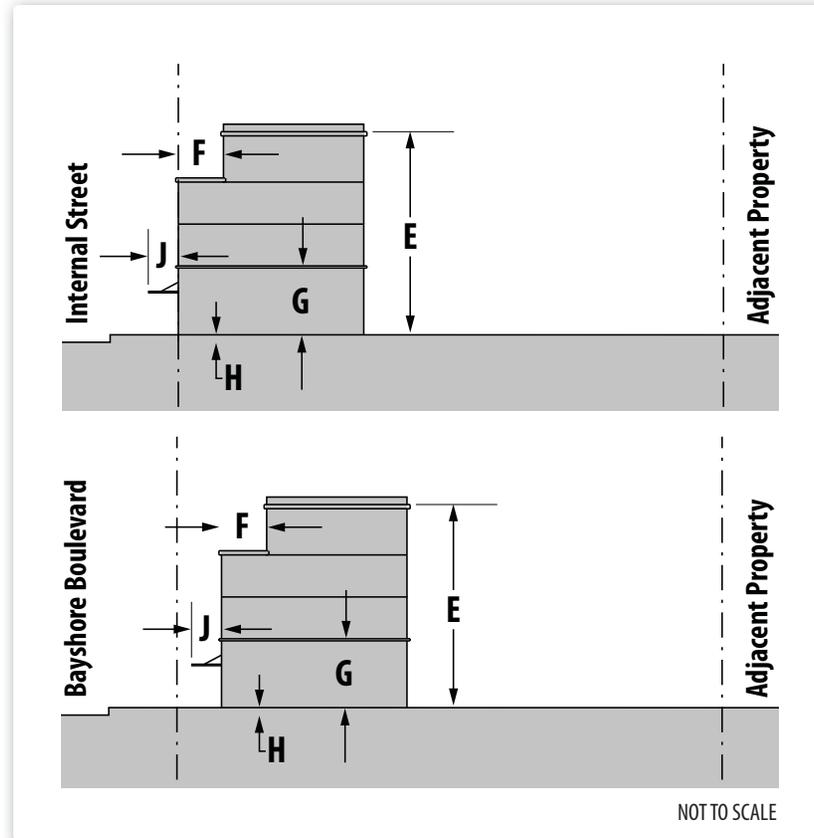
- Bay Windows, Roof Overhangs, and Eaves: 2' max.
- Balconies: 6' max.
- Awnings and Canopies: 10' max.

Permitted projections along Bayshore Boulevard may not extend beyond lot lines.

First-Story Clear Ceiling Height (G)

16' min. measured from finished floor to finished floor.

Figure 17. Building Form



First-Story Finished Floor Elevation (H)

First-story building space for non-residential use shall not be elevated higher than 1' above the average finished grade at the front setback line and shall be designed to meet flood proofing requirements in Section 105-22.8 of the Dunedin Code of Ordinances.

Urban General Subdistrict (cont.)

Parking, Service Areas & Vehicular Access

Parking, service areas, and mechanical equipment shall be located and designed in compliance with the following standards as shown in Figure 18. Parking & Service Area Placement.

Surface Parking Lots, Carports & Service Areas

- Front Setback - Internal Streets (K): 20' min.
- Front Setback - Bayshore Boulevard (L): 12' min.
- Side Setback (M): 7.5' min.
- Rear Setback (N): 5' min.

Side and rear parking setbacks may be reduced to 0' where parking abuts and is accessible from a surface parking lot on an adjacent property. Tuck-under parking (O) enclosed within a building envelope shall be located to the rear of the building, accessed from a drive aisle or alley, and be set back at least 20' from the front building façade.

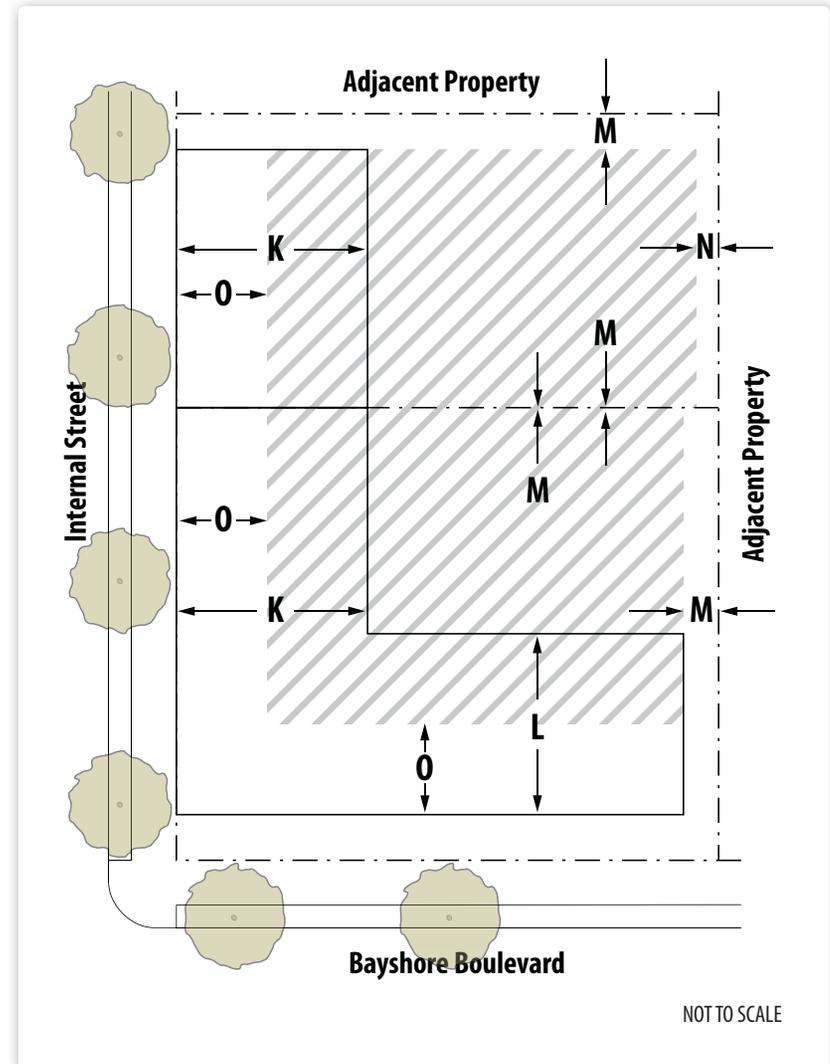
Vehicular Access

Parking and service areas shall be accessed from a rear alley, a shared access driveway, or a driveway from a secondary street where no alley or shared access driveway is present. To limit curb cuts and driveway aprons, vehicular access directly from Bayshore Boulevard is permitted only where no practical alternative exists.

Screening

Parking, service areas, and mechanical equipment shall be enclosed within buildings or screened from public view by finished masonry walls, landscaping, or other means. Screening shall be designed as an extension of the architectural and landscape design of the project.

Figure 18. Parking & Service Area Placement



Urban General Subdistrict (cont.)

Frontage Conditions

The front façades of primary buildings and improvements within required front setbacks shall comply with the following requirements.

Building Façades

The front façades of buildings shall be aligned parallel to front lot lines or block frontages and designed with ground-level storefronts with high levels of transparency and generous display windows. Storefront window and door glazing shall be no less than 65% of the total area of the first-story façade and shall be in clear glass. Storefronts shall include canopies, awnings, arcades, galleries, or other open-air, architectural features intended to shade patio, sidewalk, or other outdoor space. Buildings on corner lots shall be designed with two façades of equal architectural expression.

Building Entries

Primary entries to first-story building space shall be located along internal streets and Bayshore Boulevard at or near sidewalk grade. Entries to upper stories shall be provided through a separate ground-level entry or common foyer.

Front Setback Area

Along internal streets, the space between the building façade and curbline shall be improved as a publicly-accessible sidewalk with space for an outdoor cafe. Along Bayshore Boulevard, at least 50% of the area between the right-of-way and front façade of buildings along Bayshore Boulevard shall be landscaped. Placement of vehicular parking, driveways, stormwater management facilities, or mechanical equipment within this setback area is prohibited.

Residential General Subdistrict

Overview

The Residential General subdistrict provides standards for residential projects along secondary streets connecting to Causeway Boulevard. In compliance with floodplain elevation requirements, the first story of buildings is raised above ground level and accessed by internal elevators or internal/external stairs. Modest front yard setbacks accommodate porch projections, stairs, and lawn and landscape areas.

To create safe, attractive streetscapes and minimize pedestrian-vehicular conflicts, curb cuts are restricted and parking is enclosed within buildings or located in parking courts accessed from shared driveways.

Building Use

Land uses within the Residential General subdistrict are limited to single and multi-family residential with the exception that lodging uses are permitted consistent with use allowances in the underlying zone districts.

Figure 19. Concepts for New Buildings in the Residential General Subdistrict



Residential General Subdistrict (cont.)

Figure 20. Residential General Subdistrict Character Images



Residential General Subdistrict (cont.)

Building Placement

Primary Buildings

Primary buildings, including multiple buildings on a single lot, shall be placed in compliance with the following requirements as shown in Figure 21. Primary Building Placement.

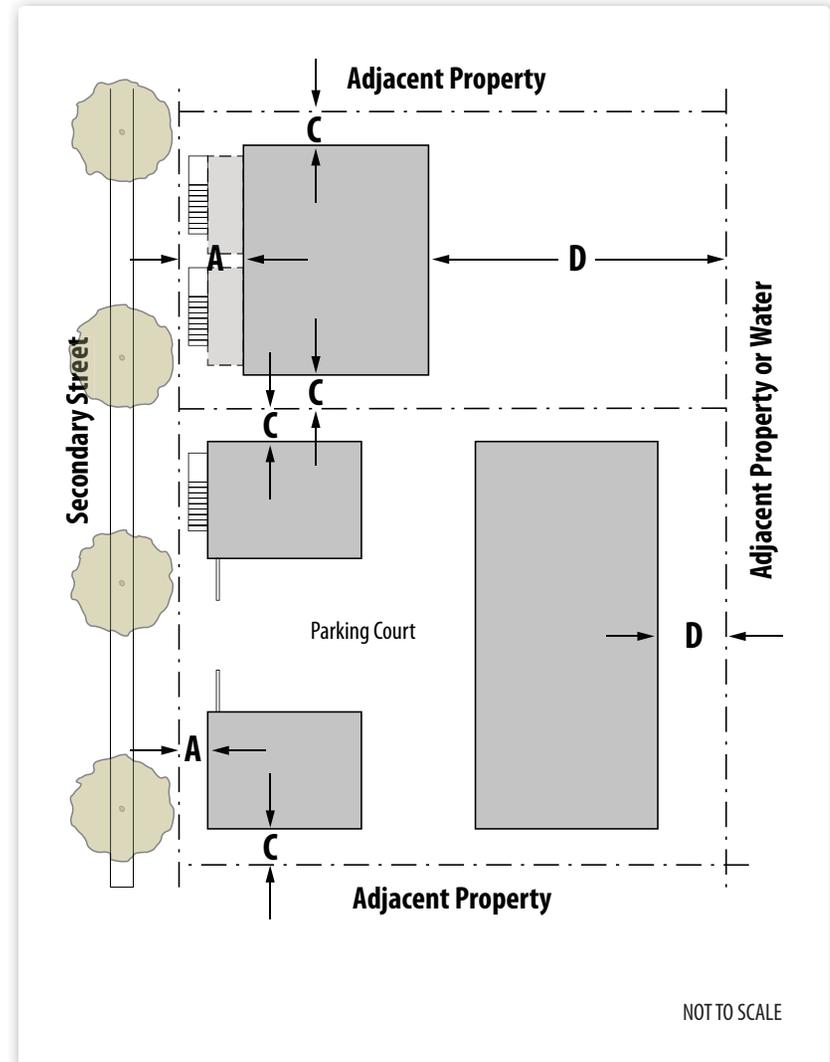
- Front Setback (A): 6' min., 20' max.
- Side Setback (C): 7.5' min.
- Rear Setback (D): 20' min. For waterfront lots, waterfront setbacks shall be controlled by the underlying zone district.

For waterfront lots where parking to the rear of buildings is not feasible, the front yard setback may be greater than 20'.

Accessory Buildings

Setbacks for accessory buildings shall be controlled by the underlying zone district. However, for waterfront lots, accessory buildings may be placed along the front setback line if the street facing facade extends to the ground plane, ground-level space under the building is fully screened from public view, and the façade is designed and constructed to match the material, finish, and architectural detail of the primary building.

Figure 21. Primary Building Placement



Residential General Subdistrict (cont.)

Building Form

The form of buildings shall comply with the following requirements as shown in Figure 22. Building Form.

Building Height (E)

Building heights shall be limited to 4 stories with maximum height in feet controlled by the underlying zone district.

Building Projections (J)

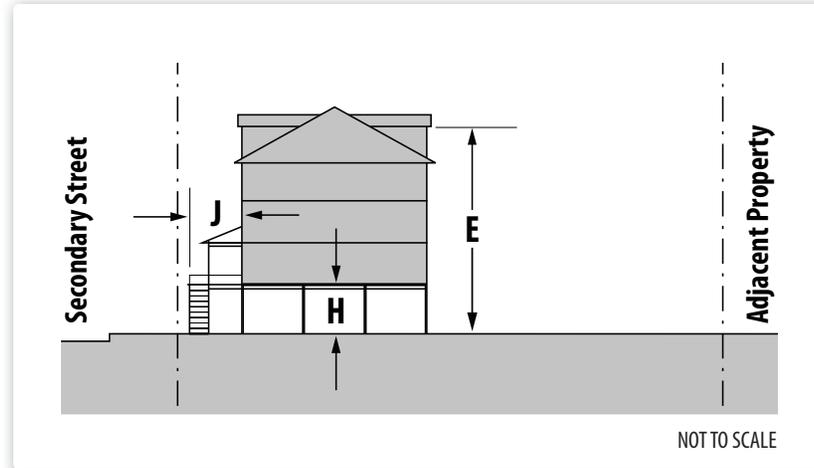
The following building elements may project beyond building façades into front setbacks as follows:

- Bay Windows, Roof Overhangs, and Eaves: 2' max.
- Balconies: 6' max.
- Covered Porches: 6' min, 10' max., but not closer than 2' from lot line. Porch projections may be up to 2 levels in height and shall extend across at least 60% of the front building façade.
- Stairs: 6' max., but not closer than 2' from lot line.

First-Story Finished Floor Elevation (H)

The finished floor elevation of first-story building space shall be elevated to meet floodplain requirements in Section 105-22.8 of the Dunedin Code of Ordinances.

Figure 22. Building Form



Residential General Subdistrict (cont.)

Parking, Service Areas, & Vehicular Access

Parking, service areas, and mechanical equipment shall be located and designed in compliance with the following standards as shown in Figure 23. Parking & Service Area Placement.

Surface Parking Lots & Service Areas

- Front Setback (K): 2' min. from front building facade
- Side Setback (M): 7.5' min.
- Rear Setback (N): 5' min. For waterfront lots, rear setbacks for parking shall be controlled by the underlying zone district.

For waterfront sites where rear yard parking is not feasible, parking may be located 8' from the front lot line and screened by a 2.5-4' high architecturally-finished masonry wall aligned parallel to the front lot line. Tuck-under parking (O) enclosed within a building envelope shall be located to the rear of buildings, accessed from a drive aisle or parking court, and set back at least 2' from the front facade.

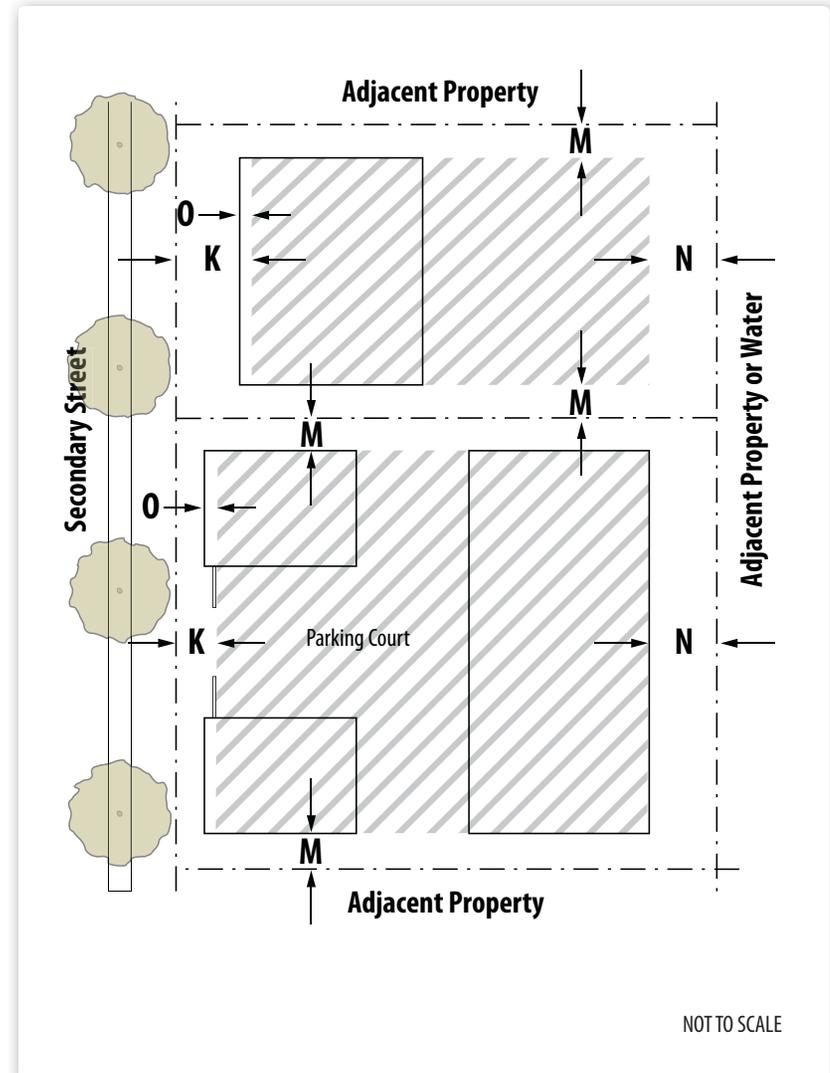
Vehicular Access

Parking and service areas shall be accessed from a shared access driveway. To limit curb cuts and driveway aprons, direct access from a street to an individual garage or driveway is permitted only where no practical alternative exists.

Screening

Parking, service areas, and mechanical equipment shall be enclosed within buildings or screened from public view by finished masonry walls, landscaping, or other means. Screening shall be designed as an extension of the architectural and landscape design of the project.

Figure 23. Parking & Service Area Placement



Residential General Subdistrict (cont.)

Frontage Conditions

The front façades of primary buildings and improvements within required front setbacks shall comply with the following requirements.

Building Façades

The front façades of buildings shall be aligned along front setback lines. Ground-level building elements of elevated buildings, including stairs, architectural screen walls, columns, posts, and others, shall be designed as integral parts of the building with treatments, material, and details matching those on first-story and upper-story façades. Open foundations with piers or piles supporting buildings should be fitted with non-structural, architectural screening panels or breakaway walls to match the overall architectural design of the building. All piers or piles should be painted or wrapped with architectural molding or trim.

Building Entries

Primary entries to first-story building space shall be located along a public street. First-story building space shall be accessed by external stairs or ground-level entries to internal elevators or stairways. Entries to upper stories shall be provided through a separate ground-level entry or common foyer.

Front Setback Area

At least 75% of the area between the right-of-way and front façade of buildings shall be landscaped. Placement of vehicular parking, driveways, stormwater management facilities, or mechanical equipment within the front setback area is prohibited.





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